



METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

**JOHN COOPER
MAYOR**

**NASHVILLE DEPARTMENT OF TRANSPORTATION
AND MULTIMODAL INFRASTRUCTURE**

INSTRUCTIONAL BULLETIN NO. 2022-3 (Revised)
Multimodal Access Closure Policy Update

Instructional Bulletin No. 2022-3 is hereby revised effective on 9/20/2022. These revisions are being made to clarify the previous policy bulletin as a result of questions and to address internal Department operations and workflow. The following list outlines the substantive portion of this revision:

- The intent of this policy is to prohibit the continuous closure of multimodal pathways in the public rights of way for a period of seven (7) days or more. The word “continuous” was added to clarify that this policy does not apply to permit requests for discrete locations where the total amount of time the closure will be in place is less than seven (7) days.
- The terminology describing the process in which those seeking an exemption from this policy was changed from “variance” to “exception”. This revision was made to prevent confusion and/or false assumption between this and other processes referred to as variances.
- An additional paragraph was added to provide more guidance to those seeking a permit for work less than seven (7) days. These permit request will require traffic control plans, however only those submitted through the exception process will require plans stamped and signed by a licensed engineer.
- The body established by the Director to support the review of those seeking an exemption for closures seven (7) days or more is being clarified as an advisory committee. All policy exceptions are at the sole discretion of the Director. The advisory committee will provide technical support and industry feedback.

Effective immediately, all newly permitted construction activities closing a multimodal pathway of travel in Metro Nashville’s right-of-way for a continuous period of seven (7) days or more will be explicitly prohibited. All ongoing currently permitted work will be under review by the Nashville Department of Transportation & Multimodal Infrastructure (NDOT).

To provide for the safety, health, and welfare of the citizens of Davidson County, this policy update seeks to ensure all modes of transportation including traffic lanes, sidewalks, bikeways, pedestrian crossing and bridges, and bus stops remain open and in good condition. NDOT will limit right-of-way closures to keep its infrastructure in a state free from obstruction and in good working order.

For work that impacts multimodal pathway travel for a period of less than 7 days, a traffic control plan must be submitted at the time of the permit application. This plan should outline the layout of required the MUTCD traffic control signage and an alternate travel pathway that meets ADA standards. Traffic control plans for closures less than 7 days are not required to be stamped by a licensed engineer.

The Nashville Department of Transportation and Multimodal Infrastructure (NDOT) has the discretion to move the effective date to the end of the year after review on a case-by-case basis. If the applicant believes that the inability to utilize Metro right-of-way for more than seven days will constitute an undue burden to completion of a construction project, the applicant may submit a request for an exception. Under the authority and discretion of the Director, NDOT may grant exceptions on a case-by-case basis. The Director will create an advisory committee to support providing industry feedback during the exception process. Please refer to the enclosed exception request guidelines and application template for further process details.

A handwritten signature in black ink, appearing to read "Brad Freeze", is written over a horizontal line.

Brad Freeze, PE

Chief Engineer/Assistant Director

FAQ

Who does this new policy apply to?

The Multimodal Access Closure Policy applies to all new permit applicants seeking to close multimodal paths of travel in the public right-of-way for a continuous period of seven (7) days or more. The policy also applies to current permit holders looking to renew right-of-way permits for seven (7) days or more. For example, if a current permit holder has a 30 day sidewalk closure permit, once that permit expires, the permit holder will work with NDOT to assess closure needs and will either receive a permit for less than 7 days, or if necessary, may request an exception as outlined in this policy.

How does the policy affect construction projects that have been permitted but have not yet begun?

Permits that have been issued prior to August 19, 2022 will be honored for the permitted amount of time. Once the permit has expired, closures will be subject to the new policy.

How does this policy affect construction projects that are seeking a permit for future construction?

This policy will impact future contractors seeking to obtain a construction easement permit in that they would not be allowed to close a multimodal path of travel without the approval of an exception.

What recourse exists if a construction firm maintains they need to use the public right-of-way to implement a project?

Any entity maintaining a need to close multimodal access in the public right-of-way for a continuous period of seven (7) days or more may apply for an exception through the process described herein. The exception will be considered by NDOT and approved or denied by the Director on a case-by-case basis.

Guidelines for Submitting a Multimodal Access Closure Exception For Public Right-of-Way

Section 1 – General Notes

Prior to any permit approval that requires temporary right-of-way closures impacting multimodal access facilities within Davidson County, the project owner shall submit a construction package to the Nashville Department of Transportation and Multimodal Infrastructure (NDOT) for review. If any parts listed below are missing and/or incomplete, the package will be immediately rejected. It is therefore recommended that the owner prepare the exception requirements at the conceptual stage of the project. This will allow both NDOT and the owner to understand all requirements needed for the project before any permitting is granted.

NDOT will have the authority to reject any exception application for any reason deemed necessary. Under such circumstances, the fee will not be reimbursed, and a second, and possible subsequent, submittal will be required until all requirements are met. Applying for an exception does not guarantee that the proposed project closures will be approved by NDOT.

See Section 2 for fees and the following Application Form and Checklist.

Section 2 – Fee Requirements

1st Submittal: \$500

2nd and Subsequent Submittals: \$250

MULTIMODAL ACCESS CLOSURE EXCEPTION APPLICATION FORM AND CHECKLIST

Submittal Date: 2/1/24 New Submittal Re-Submittal No: _____

Related Building Permit No: 2/1/24

Project Name: Alta Rochelle

Street Name Location: 1020 Southside Court, Nashville, TN 37203

Between: 12th Ave. South And: South Street

Applicant Name: Wood Partners

Address: 15 Music Square West, Nashville, TN 37203

Phone: (615)246-6208 Fax: NA Contact: Michael Stock

Email: michael.stock@woodpartners.com

Project Description: 355 unit multifamily project with a wrapped parking garage

Start Date: 2/19/24 End Date: 2/19/26 Project Length: 24 months

Describe Type of Closure: Full sidewalk and parking lane closure on South Street for the
Sidewalk and bike lane closure on 12th Ave. South for short periods only as needed.

Provide Reasons why Project cannot be completed without closures and what other options were considered (attach documents as needed): There is no room to bring
deliveries to the jobsite due to the proximity of the new buildings to the street. We
considered the bike lane and sidewalk along 12th Ave. South, but were advised the less
busy South Street would be preferable.

PROJECT INFORMATION CHECKLIST:

Included Not Applicable

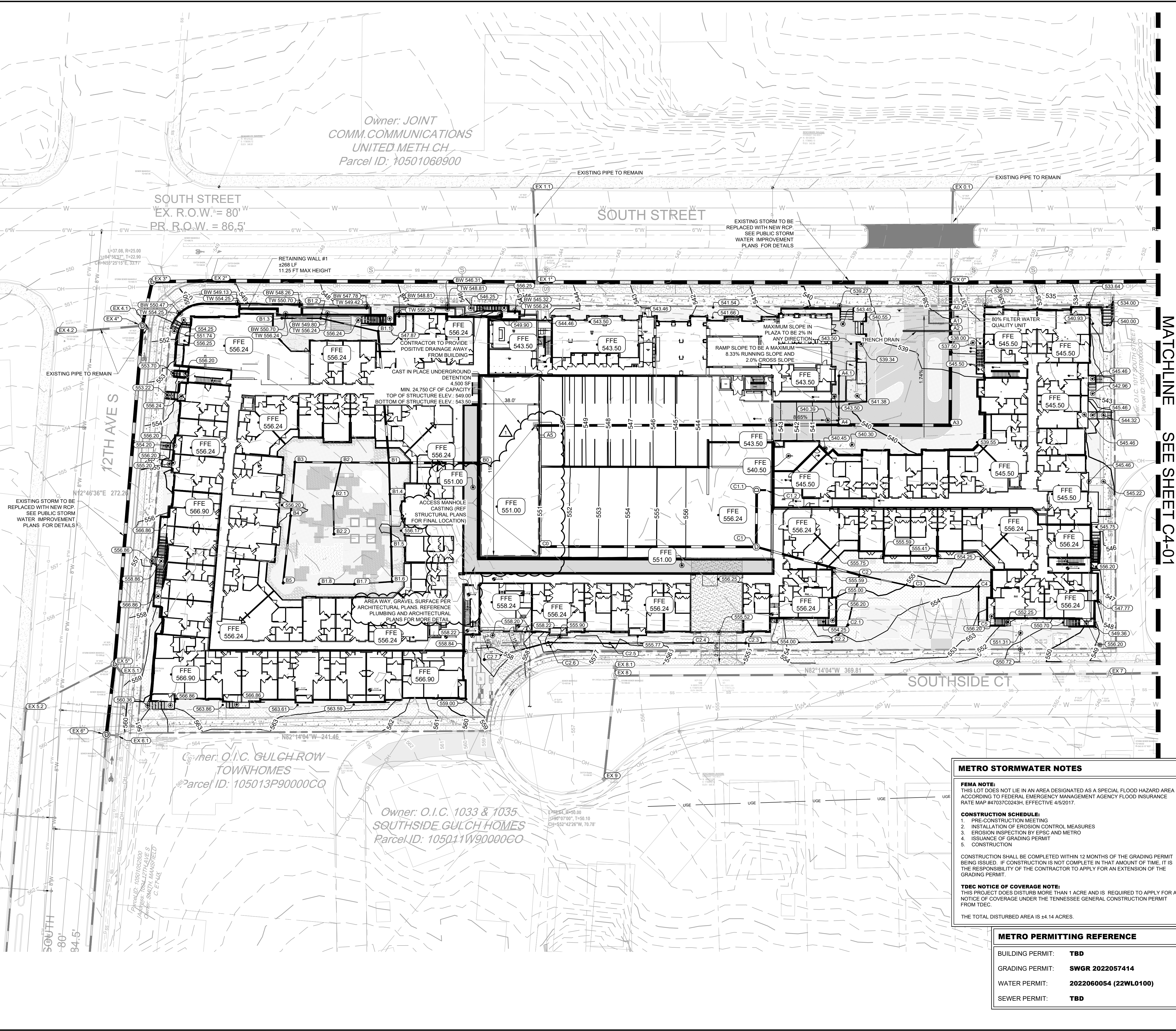
- | | | |
|--------------------------|--------------------------|--|
| <input type="checkbox"/> | <input type="checkbox"/> | Project Vicinity Map with Project Area shown, street names, property information, existing pavement and striping, gutter and building locations, north arrow, and scale. |
| <input type="checkbox"/> | <input type="checkbox"/> | Planned work hours included. |
| <input type="checkbox"/> | <input type="checkbox"/> | Exact location and dimensions of the construction work zone shown. |
| <input type="checkbox"/> | <input type="checkbox"/> | If multiple phases are necessary, include perimeter impact of each phase, phase number, anticipated work hours and phase duration. |
| <input type="checkbox"/> | <input type="checkbox"/> | Details on construction activity and equipment being used as part of construction included for each phase. |
| <input type="checkbox"/> | <input type="checkbox"/> | Specify if any on-street parking, and/or metered parking, is to be restricted and if bus zone will need to be relocated. |
| <input type="checkbox"/> | <input type="checkbox"/> | Specify if trash pickup will be impacted. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide information on all utility work and utility connections. |
| <input type="checkbox"/> | <input type="checkbox"/> | List all affected residents, businesses, agencies, and schools and any conversations/agreements taken place. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show ongoing construction projects within vicinity of proposed project impact. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide plan to address conflicts with other nearby projects. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide traffic control plan for each phase of construction (see traffic control checklist for more information). |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide information on work vehicle parking locations. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show construction trucks ingress/egress to project location. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide information on any traffic signals, traffic signal loops, and traffic signal cabinets in close proximity to project. |

TRAFFIC CONTROL PLAN CHECKLIST:

Included Not Applicable

- | | | |
|--------------------------|--------------------------|---|
| <input type="checkbox"/> | <input type="checkbox"/> | All temporary traffic control plans shall be designed in accordance with the most recent ADA regulations and requirements of the Manual of Uniform Traffic Control Devices. |
| <input type="checkbox"/> | <input type="checkbox"/> | Clearly show the locations of all existing signs (including speed limit) as well as the proposed signs for each construction phase. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show the location of all existing pedestrian paths and pedestrian detour route of each stage of construction. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show dimensions of travel lane width, shoulder width, sidewalk of each phase, and overall roadway width along the length of affected area. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show all existing striping and markings to remain, to be removed, and all proposed striping and markings for each construction stage. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide detour plan clearly showing detour route for any roadway or pedestrian/bike path closures. |
| <input type="checkbox"/> | <input type="checkbox"/> | Specify placement of all temporary traffic control devices. |
| <input type="checkbox"/> | <input type="checkbox"/> | Specify spacing of all temporary traffic control devices. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show all existing traffic signals and streetlights in the work zone location. |
| <input type="checkbox"/> | <input type="checkbox"/> | Lighting provided for all pedestrian detour routes. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide minimum eleven (11) foot travel lanes at all times. |
| <input type="checkbox"/> | <input type="checkbox"/> | Show size, height, and location of all channelizing devices, warning lights, flag trees, barriers, etc. |
| <input type="checkbox"/> | <input type="checkbox"/> | Label all taper lengths and widths. |
| <input type="checkbox"/> | <input type="checkbox"/> | Provide locations of police officers for each phase as needed. |
| <input type="checkbox"/> | <input type="checkbox"/> | Temporary Traffic Control Plan has been stamped and signed by a TN licensed Civil Engineer. |

This document, together with the concepts and designs presented herein, is an instrument of service, and an instrument of record, and shall be without liability to Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc. in the event of any error or omission in the design or construction of the project.



Owner: JOINT
COMMUNICATIONS
UNITED METH CH
Parcel ID: 10501060900

SOUTH STREET
EX. R.O.W. = 80'
PR. R.O.W. = 86.5'

SOUTH STREET

SOUTHSIDE CT

Owner: O.I.C. GULCH ROW
TOWNHOMES
Parcel ID: 105013P90000C0

Owner: O.I.C. 1033 & 1035
SOUTHSIDE GULCH HOMES
Parcel ID: 105011W90000C0

MATCHLINE SEE SHEET C4-01

- ### GRADING NOTES
- CONTRACTOR RESPONSIBLE FOR VERIFYING LOCATION, SIZE, AND ELEVATIONS OF EXISTING UTILITIES AT CONNECTION POINTS PRIOR TO GRADING OR INSTALLATION OF ANY PROPOSED UTILITIES. CONTRACTOR TO IMMEDIATELY NOTIFY OWNERS REPRESENTATIVE IF DISCREPANCIES ARE FOUND.
 - AREAS FOR CONSTRUCTION THAT REQUIRE DE-WATERING FOR EXCAVATION WILL BE DONE AT NO ADDITIONAL COST TO THE OWNER AND IN ACCORDANCE WITH SECTION 4.14 OF THE TENNESSEE GENERAL NPDES PERMIT FOR DISCHARGES OF STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
 - ADDITIONAL EROSION CONTROL DEVICES TO BE USED AS REQUIRED BY LOCAL INSPECTOR.
 - DISTURBED AREAS LEFT IDLE FOR FIVE DAYS, AND NOT TO FINAL GRADE, WILL BE ESTABLISHED TO TEMPORARY VEGETATION, MULCH, TEMPORARY VEGETATION OR PERMANENT VEGETATION SHALL BE COMPLETED ON ALL EXPOSED AREAS WITHIN 14 DAYS AFTER DISTURBANCE. ALL AREAS TO FINAL GRADE WILL BE ESTABLISHED TO PERMANENT VEGETATION UPON COMPLETION.
 - WHEN HAND PLANTING, MULCH (HAY OR STRAW) SHOULD BE UNIFORMLY SPREAD OVER SEED AREA WITHIN 24 HOURS OF SEEDING. IF UNABLE TO ACCOMPLISH, MULCH SHALL BE USED AS A TEMPORARY COVER. CONCENTRATED FLOW AREAS AND ALL SLOPES STEEPER THAN 2.5:1 AND WITH A HEIGHT OF TEN FEET OR GREATER (DOES NOT APPLY TO RETAINING WALLS), AND CUTS AND FILLS WITHIN BUFFERS, SHALL BE STABILIZED WITH THE APPROPRIATE EROSION CONTROL MATTING OR BLANKETS.
 - THE GRADING PERMIT MUST BE DISPLAYED ON SITE AT ALL TIMES DURING CONSTRUCTION AND IN PLAIN VIEW FROM A PUBLIC ROAD OR STREET.
 - EROSION AND SEDIMENT CONTROL DEVICES MUST BE DISPLAYED AND INSPECTED PRIOR TO ANY GRADING ON SITE. THE CONTRACTOR MUST CALL FOR AN INSPECTION TO OBTAIN A PERMIT TO GRADE. PLEASE CALL WITH ENOUGH LEAD-TIME FOR AN INSPECTION TO MEET YOUR SCHEDULE.
 - SEDIMENTATION AND EROSION CONTROL DEVICES MUST BE INSPECTED ACCORDING TO LOCAL AND STATE REQUIREMENTS AND AS STIPULATED IN THE STORMWATER POLLUTION PREVENTION PLAN. EACH DEVICE IS TO BE MAINTAINED OR REPLACED IF SEDIMENT ACCUMULATION HAS REACHED ONE HALF THE CAPACITY OF THE DEVICE. ADDITIONAL DEVICES MAY BE NECESSARY AS THE PROJECT PROGRESSES AND NEW CHANNELS HAVE DEVELOPED.
 - THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO, OR CONCURRENT WITH, LAND-DISTURBING ACTIVITIES.
 - EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION CONTROL AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
 - CONTRACTOR SHALL REVIEW SITE GEOTECHNICAL REPORT BEFORE COMMENCING GRADING OPERATIONS.
 - SEED ALL DISTURBED AREAS UNLESS OTHERWISE NOTED AS PART OF THIS CONTRACT. REFER TO LANDSCAPING PLANS FOR AREAS TO RECEIVE SOD.
 - INSTALL SOD OR RIPRAP IN SWALES AS INDICATED ON GRADING PLANS AND LANDSCAPING CONTROL PLANS.
 - TOPSOIL ON SITE TO BE STRIPPED AND STOCKPILED FOR USE IN LAWN AREAS.
 - ADAPTED DRAINAGE, EROSION AND SEDIMENT CONTROL MEASURES, BEST MANAGEMENT PRACTICES, AND/OR OTHER WATER QUALITY MANAGEMENT FACILITIES SHALL BE PROVIDED AND MAINTAINED AT ALL TIMES DURING CONSTRUCTION. DAMAGES TO ADJACENT PROPERTY AND/OR THE CONSTRUCTION SITE CAUSED BY THE CONTRACTORS OR PROPERTY OWNERS FAILURE TO PROPERLY MAINTAIN ADEQUATE DRAINAGE AND EROSION/SEDIMENT CONTROL FOR THE CONSTRUCTION AREA SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER AND/OR CONTRACTOR.
 - UNDEGROUND UTILITIES HAVE NOT BEEN VERIFIED BY THE OWNER, DESIGNER, OR THEIR REPRESENTATIVES. BEFORE YOU DIG CALL ONE CALL-811 OR 1-800-752-6007.
 - THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT RESULT FROM THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY UNDERGROUND UTILITIES TO REMAIN.
 - PROPOSED EXCAVATION WILL NOT UTILIZE BLASTING.

- ### RETAINING WALL NOTES
- ALL SITE WALLS SHALL BE DESIGN / BUILD UNDER THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL SITE RETAINING WALLS INDICATED ON THESE PLANS ARE SHOWN FOR GENERAL LOCATION ONLY. CONTRACTOR SHALL DETERMINE WALL TYPE (UNLESS SPECIFICALLY NOTED ON PLANS) AND SHALL VERIFY PROPOSED WALL LOCATIONS AND HEIGHTS.
 - CONTRACTOR SHALL SUBMIT DESIGN DRAWINGS AND CALCULATIONS SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF TENNESSEE TO THE CIVIL ENGINEER OF RECORD AND THE CODES AND ENGINEERING DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO ORDERING MATERIALS OR CONSTRUCTION.
 - ALL WALLS OF HEIGHTS EQUAL TO OR GREATER THAN 30 INCHES (MEASURED FROM TOP OF WALL TO FINISHED GRADE AT BOTTOM OF WALL) SHALL INCLUDE PEDESTRIAN GUARDRAIL OR FENCE. GUARDRAIL SHALL BE IN COMPLIANCE WITH LOCAL CODES AND IBC REQUIREMENTS.

- ### AS-BUILT NOTE
- IN ACCORDANCE WITH THE METRO STORMWATER MANAGEMENT MANUAL, VOLUME 1, SECTION 3.9, AS-BUILT CERTIFICATIONS, MWS STORMWATER DIVISION MUST APPROVE THE FOLLOWING AS-BUILTS PRIOR TO ISSUANCE OF THE USE & OCCUPANCY PERMIT:
- UNDERGROUND DETENTION AND WATER QUALITY INFRASTRUCTURE
 - ABOVE-GROUND DETENTION AND WATER QUALITY INFRASTRUCTURE
 - MUNICIPAL STORM SEWER INFRASTRUCTURE
 - CUT & FILL IN THE FLOODPLAIN
 - SINK HOLE ALTERATIONS
- THE ENGINEER SHALL CONTACT STORMWATER DEVELOPMENT REVIEW STAFF FOR SUBMITTAL REQUIREMENTS.

- AS PART OF THE FEBRUARY 2016 REGULATIONS UPDATE, PLEASE NOTE THAT THE FOLLOWING ARE REQUIRED AS A PART OF THE AS-BUILT PLAN:
- A CERTIFICATION LETTER FROM TN REGISTERED P.E. STATING THAT THE SITE HAS BEEN INSPECTED AND THAT THE STORMWATER MANAGEMENT SYSTEM AND STORMWATER CONTROL MEASURES (BOTH STRUCTURAL AND NON-STRUCTURAL) ARE COMPLETE AND FUNCTIONAL IN ACCORDANCE WITH THE PLANS APPROVED BY MWS.
 - AN AS-BUILT LID SPREADSHEET.
 - HYDROLOGIC AND HYDRAULIC CALCULATIONS FOR AS-BUILT CONDITIONS, AS REQUIRED.
 - AS-BUILT DRAWINGS SHOWING FINAL TOPOGRAPHIC FEATURES OF ALL THESE FACILITIES. THIS SHALL INCLUDE INVERT ELEVATIONS OF OUTLET CONTROL STRUCTURES.
 - ANY DEVIATIONS FROM THE APPROVED PLANS SHALL BE NOTED ON AS-BUILT DRAWINGS SUBMITTED.
 - COPY OF AS-BUILT PLAN CAD FILE ON A CD AND SHOULD BE REGISTERED TO THE TN STATE PLANE COORDINATE SYSTEM, NORTH AMERICAN DATUM 1983 (NAD83). DATA SHOULD BE PLACED IN SEPARATE LAYERS AND SHOULD BE LABELED / NAMED FOR EASY IDENTIFICATION.
 - CUT AND FILL BALANCE CERTIFICATION FOR FLOODPLAIN AND SINKHOLE ALTERATIONS.
 - WATER QUALITY BUFFERS SHALL BE SURVEYED AND INCLUDED WITH THE AS-BUILT SUBMITTAL.
 - ANY PUBLIC (TO BECOME THE RESPONSIBILITY OF METRO TO MAINTAIN) STORMWATER INFRASTRUCTURE SHALL BE VIDEO-INSPECTED TO VERIFY PROPER INSTALLATION WITH THE VIDEO RECORDING AND ANY ASSOCIATED INSPECTION REPORT SUBMITTED AS PART OF AS-BUILT RECORD.
 - ADDITIONAL TESTING MAY BE REQUIRED AS / IF WARRANTED BY VIDEO INSPECTION.
- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AS-BUILT DOCUMENTATION IN ACCORDANCE WITH METRO STORMWATER TO KIMLEY-HORN FOR REVIEW AND APPROVAL. THE AS-BUILT INFORMATION SHALL BE COMPLETE AND PROVIDED TO THE ENGINEER A MINIMUM OF SIX WEEKS BEFORE C.O. IS REQUIRED FOR THE PROJECT.

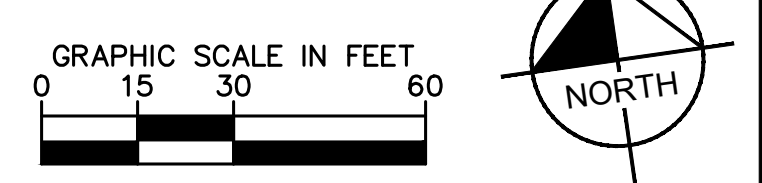
- ### METRO STORMWATER NOTES
- FEMA NOTE:**
THIS LOT DOES NOT LIE IN AN AREA DESIGNATED AS A SPECIAL FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP #47037C0243H, EFFECTIVE 4/5/2017.
- CONSTRUCTION SCHEDULE:**
- PRE-CONSTRUCTION MEETING
 - INSTALLATION OF EROSION CONTROL MEASURES
 - EROSION INSPECTION BY EPSC AND METRO
 - ISSUANCE OF GRADING PERMIT
 - CONSTRUCTION
- CONSTRUCTION SHALL BE COMPLETED WITHIN 12 MONTHS OF THE GRADING PERMIT BEING ISSUED. IF CONSTRUCTION IS NOT COMPLETE IN THAT AMOUNT OF TIME, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO APPLY FOR AN EXTENSION OF THE GRADING PERMIT.
- TDEC NOTICE OF COVERAGE NOTE:**
THIS PROJECT DOES NOT DISTURB MORE THAN 1 ACRE AND IS REQUIRED TO APPLY FOR A NOTICE OF COVERAGE UNDER THE TENNESSEE GENERAL CONSTRUCTION PERMIT FROM TDEC.
- THE TOTAL DISTURBED AREA IS ±4.14 ACRES.

METRO PERMITTING REFERENCE

BUILDING PERMIT:	TBD
GRADING PERMIT:	SWGR 2022057414
WATER PERMIT:	2022060054 (22WL0100)
SEWER PERMIT:	TBD

- ### STORM PIPE NOTES
- ALL STORM PIPES TO BE ROUTED UNDER THE BUILDING SHALL BE WATER TIGHT.

- ### GRADING PLAN LEGEND
- 476.17 SPOT ELEVATION
 - HP HIGH POINT
 - TC TOP OF CURB
 - BC BOTTOM OF CURB / OUTER LINE
 - EW TOP OF EXPOSED WALL
 - IC BOTTOM OF EXPOSED WALL
 - INV INVERT
 - LP LOW POINT
 - IFC INVERT FINISH CURB / OUTER LINE
 - RM TOP OF GRATE / COVER
 - FFE FINISH FLOOR ELEVATION
 - 500 EXISTING CONTOUR
 - 500 PROPOSED CONTOUR



10 Lea Avenue, Suite 400, Nashville, TN 37210
Main: 615.564.2701 | www.kimley-horn.com
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ALTA ROCHELLE
WP SOUTH
ACQUISITIONS, LLC
NASHVILLE, TN

SWGR 2022057414

NO.	DATE	BY	REVISIONS
1	07/09/2022		SCHEMATIC DESIGN
2	08/12/2022		50% DESIGN DEVELOPMENT
3	08/19/2022		GRADING PERMIT SUBMITTAL
4	09/19/2022		100% DESIGN DEVELOPMENT
5	09/19/2022		100% DESIGN DEVELOPMENT
6	07/28/2023		50% GMP SET
7	09/08/2023		OFFSITE STORM UPDATES
8	09/15/2023		100% GMP
9	09/29/2023		100% GMP - ADDENDUM 1

DESIGNED BY: _____
DRAWN BY: _____
CHECKED BY: _____
DATE: 08/12/2022
KIMLEY-HORN PROJECT NO. 012095036

GRADING AND DRAINAGE PLAN - OVERALL

SHEET NUMBER
C4-00

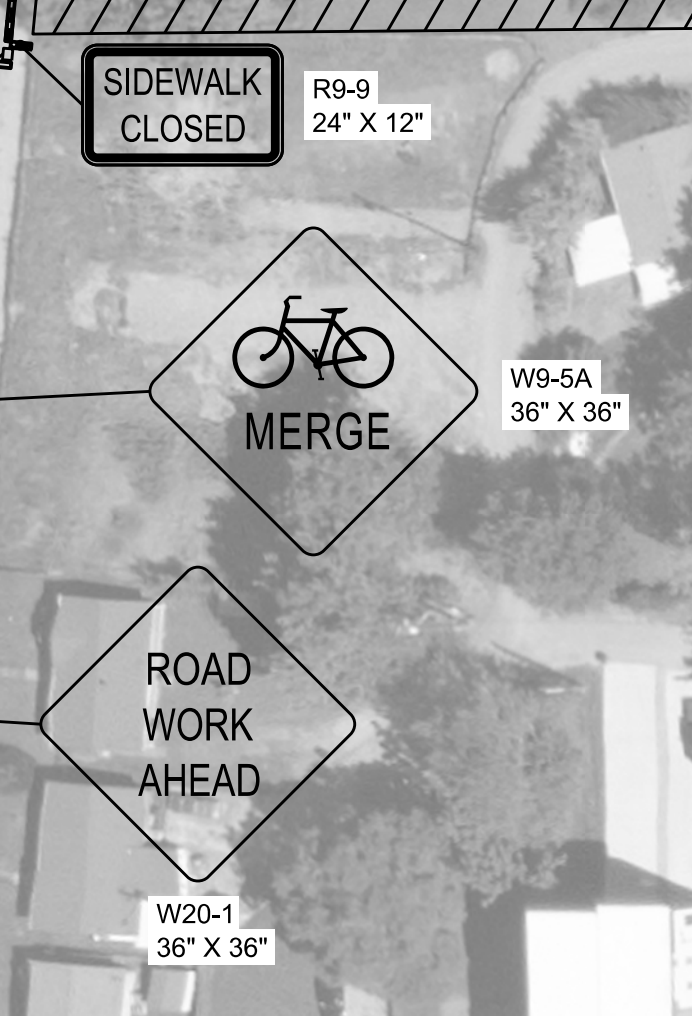


PLASTIC WATER-FILLED BARRIERS (TYP.)

PLASTIC WATER-FILLED BARRIERS W/ FENCE (TYP.)

PED. WILL UTILIZE THE EXISTING BIKE LANE FOR A PED. PATH

PED. PATH ENTRANCE/EXIT W/ RAMP FROM ROAD TO SIDEWALK AT NO MORE THAN A 2% GRADE

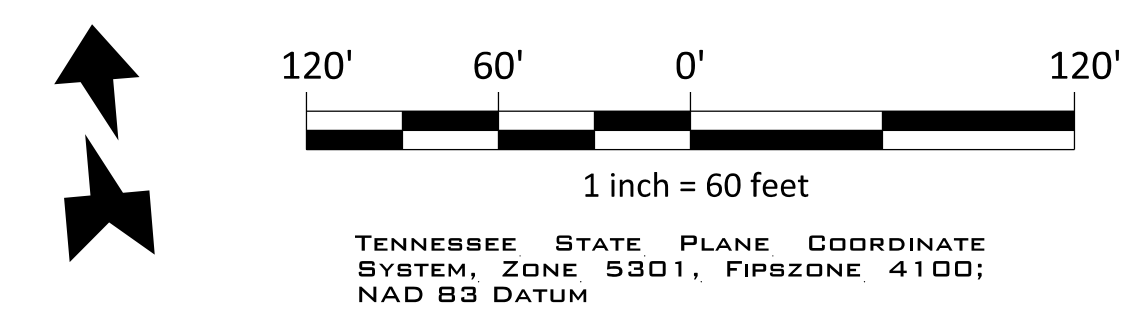


DESCRIPTION:

NORTHBOUND BIKE LANE AND SIDEWALK CLOSURE ALONG 12TH AVENUE S IN NASHVILLE, TN.

TRAFFIC CONTROL NOTES:

1. NORTHBOUND BIKE LANE AND SIDEWALK CLOSURE TO OCCUR ALONG 12TH AVENUE S BETWEEN ARCHER STREET AND SOUTH STREET FOR THE DURATION OF THE PROJECT.
2. THIS CLOSURE WILL BE ACTIVE FOR NO LONGER THAN NINE (9) WEEKS FOR THE DURATION OF THE UTILITY WORK ALONG 12TH AVENUE S.
3. ALL TRAFFIC CONTROL DEVICES AND WARNING SIGNS SHOULD BE REMOVED OR COVERED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER NEEDED OR IN USE.
4. ALL TRAFFIC CONTROL DEVICES, BARRICADES, BALLAST SYSTEMS, AND SIGN SUPPORT STRUCTURES USED ON ANY ROADWAY OPEN TO PUBLIC TRAVEL SHALL BE CRASHWORTHY WHEN INSTALLED FACING TRAFFIC OR TURNED AWAY FROM TRAFFIC.
5. ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND.
6. ALL REGULATORY SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND AND BORDER ON A WHITE BACKGROUND.
7. ADVANCE WARNING SIGNS TO BE PLACED ALONG THE 12TH AVENUE S NORTHBOUND APPROACH APPROXIMATELY 100 FEET APART PRIOR TO SIDEWALK CLOSURE.
8. THE EXISTING NORTHBOUND BIKE LANE ALONG 12TH AVENUE S WILL BE UTILIZED FOR PEDESTRIAN TRAFFIC DURING THE DURATION OF THE SIDEWALK CLOSURE.
9. THE PEDESTRIAN PATH TO INCLUDE TEMPORARY CURB RAMPS TO TRANSITION TO/FROM THE SIDEWALK WITH A MINIMUM WIDTH OF FIVE (5) FEET AND NO MORE THAN A TWO PERCENT (2%) GRADE.
10. PORTABLE WATER-FILLED BARRIERS WILL BE UTILIZED ALONG 12TH AVENUE S TO SEPARATE THE PEDESTRIAN PATH AND THE NORTHBOUND TRAVEL LANE.
11. PORTABLE WATER-FILLED BARRIERS WITH FENCES WILL BE UTILIZED ALONG 12TH AVENUE S TO SEPARATE THE WORK ZONE FROM PEDESTRIAN PATH.
12. ALL PORTABLE WATER-FILLED BARRIERS MUST BE INTERLOCKED AND FILLED ACCORDING TO MANUFACTURER'S STANDARDS TO PROVIDE NECESSARY CRASHWORTHINESS.
13. PORTABLE WATER-FILLED BARRIERS SHALL BE SUPPLEMENTED WITH STANDARD DELINEATION TO IMPROVE VISIBILITY DURING DAYTIME AND NIGHTTIME OPERATIONS.
14. EACH PORTABLE WATER-FILLED BARRIER SECTION SHOULD BE MADE OF POLYETHYLENE PLASTIC MEASURING APPROXIMATELY 6.5 FEET IN LENGTH AND A MINIMUM OF 1.75 FEET IN WIDTH.
15. FENCES USED WITHIN THE TRAFFIC CONTROL AREA SHOULD NOT CREATE SIGHT DISTANCE RESTRICTIONS AND SHOULD BE CONSTRUCTED OF MATERIALS THAT ARE CONSIDERED CRASHWORTHY ACCORDING TO THE MUTCD.
16. THE BICYCLE TRAFFIC ALONG 12TH AVENUE S WILL MERGE INTO THE NORTHBOUND TRAVEL LANE FOR THE DURATION OF THE SIDEWALK CLOSURE.
17. TRAFFIC CONTROL DEVICES SHOULD BE PLACED AS NOT TO IMPEDE ACCESS TO DRIVEWAYS LOCATED OUTSIDE OF THE WORK ZONE.
18. TRAFFIC CONTROL DEVICES SHOULD BE PLACED AS NOT TO BLOCK VISIBILITY OF ANY REGULATORY SIGNS OUTSIDE OR WITHIN THE WORK ZONE.
19. THE LOCATIONS OF TRAFFIC SAFETY OFFICERS (IF NECESSARY) WITHIN THE WORK ZONE AREA TO BE DETERMINED BY THE SITE COORDINATOR.
20. REFER TO TDOT STANDARD DRAWINGS, TDOT WORK ZONE FIELD ZONE MANUAL, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR ADDITIONAL DETAILS RELATED TO WORK ZONE DEVICES.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION)
	PLASTIC WATER-FILLED BARRIER
	PLASTIC WATER-FILLED BARRIER WITH FENCE

NO.	DATE	REVISIONS	
		DATE	DESCRIPTION
1	01/25/24	DRAFT REVIEW	
2	01/30/24	INITIAL SUBMITTAL TO NDOT	

TRAFFIC CONTROL PLAN

12TH AVENUE S
NASHVILLE, TENNESSEE

12TH AVENUE S - SIDEWALK CLOSURE



1/30/24

PROJECT
24-0137

SHEET
T-1.2

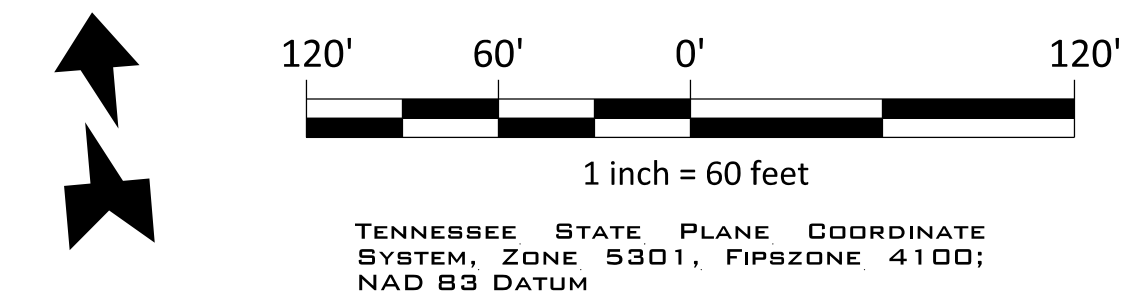


DESCRIPTION:

EASTBOUND PARKING LANE AND SIDEWALK CLOSURE ALONG SOUTH STREET IN NASHVILLE, TN.

TRAFFIC CONTROL NOTES:

- EASTBOUND PARKING LANE AND SIDEWALK CLOSURE TO OCCUR ALONG SOUTH STREET BETWEEN 12TH AVENUE S AND SOUTHSIDE AVENUE FOR THE DURATION OF THE PROJECT.
- ALL TRAFFIC CONTROL DEVICES AND WARNING SIGNS SHOULD BE REMOVED OR COVERED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER NEEDED OR IN USE.
- ALL TRAFFIC CONTROL DEVICES, BARRICADES, BALLAST SYSTEMS, AND SIGN SUPPORT STRUCTURES USED ON ANY ROADWAY OPEN TO PUBLIC TRAVEL SHALL BE CRASHWORTHY WHEN INSTALLED FACING TRAFFIC OR TURNED AWAY FROM TRAFFIC.
- ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND.
- ALL REGULATORY SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND AND BORDER ON A WHITE BACKGROUND.
- ADVANCE WARNING SIGNS TO BE PLACED ALONG THE SOUTH STREET EASTBOUND APPROACH APPROXIMATELY 100 FEET APART PRIOR TO 12TH AVENUE S.
- ADVANCE WARNING SIGN TO BE PLACED ALONG THE 12TH AVENUE S SOUTHBOUND APPROACH APPROXIMATELY 100 FEET PRIOR TO SOUTH STREET.
- ADVANCE WARNING SIGN TO BE PLACED ALONG THE 12TH AVENUE S NORTHBOUND APPROACH APPROXIMATELY 100 FEET PRIOR TO SOUTH STREET.
- THE EASTBOUND TRAVEL LANE WILL TRANSITION INTO THE EXISTING BIKE LANE WITH A PEDESTRIAN PATH ALONG SOUTH STREET FOR THE DURATION OF THE PROJECT.
- THE EASTBOUND TRAVEL LANE ALONG SOUTH STREET MUST REMAIN A MINIMUM OF TEN (10) FEET AS MEASURED FROM THE NEAR FACE OF THE TRAFFIC CONTROL DEVICES.
- THE PROPOSED PEDESTRIAN PATH WILL MUST REMAIN A MINIMUM OF FIVE (5) FEET AS MEASURED FROM THE NEAR FACE OF THE TRAFFIC CONTROL DEVICES.
- THE PEDESTRIAN PATH TO INCLUDE TEMPORARY CURB RAMPS TO TRANSITION TO/FROM THE SIDEWALK WITH A MINIMUM WIDTH OF FIVE (5) FEET AND NO MORE THAN A TWO PERCENT (2%) GRADE.
- PORTABLE WATER-FILLED BARRIERS WILL BE UTILIZED ALONG SOUTH STREET TO SEPARATE THE PEDESTRIAN PATH AND THE EASTBOUND TRAVEL LANE.
- PORTABLE WATER-FILLED BARRIERS WITH FENCES WILL BE UTILIZED ALONG SOUTH STREET TO SEPARATE THE WORK ZONE FROM PEDESTRIAN PATH.
- ALL PORTABLE WATER-FILLED BARRIERS MUST BE INTERLOCKED AND FILLED ACCORDING TO MANUFACTURER'S STANDARDS TO PROVIDE NECESSARY CRASHWORTHINESS.
- PORTABLE WATER-FILLED BARRIERS SHALL BE SUPPLEMENTED WITH STANDARD DELINEATION TO IMPROVE VISIBILITY DURING DAYTIME AND NIGHTTIME OPERATIONS.
- EACH PORTABLE WATER-FILLED BARRIER SECTION SHOULD BE MADE OF POLYETHYLENE PLASTIC MEASURING APPROXIMATELY 6.5 FEET IN LENGTH AND A MINIMUM OF 1.75 FEET IN WIDTH.
- FENCES USED WITHIN THE TRAFFIC CONTROL AREA SHOULD NOT CREATE SIGHT DISTANCE RESTRICTIONS AND SHOULD BE CONSTRUCTED OF MATERIALS THAT ARE CONSIDERED CRASHWORTHY ACCORDING TO THE MUTCD.
- THE BICYCLE TRAFFIC ALONG SOUTH STREET WILL MERGE INTO THE EASTBOUND TRAVEL LANE FOR THE DURATION OF THE PROJECT.
- TRAFFIC CONTROL OFFICERS WILL BE UTILIZED TO MANAGE PEDESTRIANS CROSSING WITHIN ANY CONSTRUCTION OPENINGS AS NEEDED TO ENSURE SAFE AND EFFICIENT OPERATIONS.
- TRAFFIC CONTROL DEVICES SHOULD BE PLACED AS NOT TO IMPEDE ACCESS TO DRIVEWAYS LOCATED OUTSIDE OF THE WORK ZONE.
- TRAFFIC CONTROL DEVICES SHOULD BE PLACED AS NOT TO BLOCK VISIBILITY OF ANY REGULATORY SIGNS OUTSIDE OR WITHIN THE WORK ZONE.
- THE LOCATIONS OF TRAFFIC SAFETY OFFICERS (IF NECESSARY) WITHIN THE WORK ZONE AREA TO BE DETERMINED BY THE SITE COORDINATOR.
- REFER TO TDOT STANDARD DRAWINGS, TDOT WORK ZONE FIELD ZONE MANUAL, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR ADDITIONAL DETAILS RELATED TO WORK ZONE DEVICES.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION)
	PLASTIC WATER-FILLED BARRIER
	PLASTIC WATER-FILLED BARRIER WITH FENCE

NO.	DATE	REVISIONS	
		DRAFT REVIEW	INITIAL SUBMITTAL TO NDOT
1	01/30/24		
2	01/30/24		

DATE: 01/30/24
 SCALE: AS SHOWN
 DRAWN BY: T-SQUARE
 REVIEWER: JDE

TRAFFIC CONTROL PLAN
 SOUTH STREET
 NASHVILLE, TENNESSEE



1/30/24

PROJECT
 24-0137

SHEET
 T-1.1

SOUTH STREET - LANE/SIDEWALK CLOSURE