



STF & TAC Meeting #7

Draft Recommendations & Phasing

October 18, 2023



What We'll Cover Today

- 1 Welcome, Introductions, & Updates
- 2 DRAFT Priority Recommendations
- 3 DRAFT Project Phasing
- 4 Upcoming Engagement Activities & Next Steps





Welcome, Introductions, & Updates

Mobility Challenges to Address



People are stuck in traffic.



Travel options don't match desires.



Transit is slow and unreliable.



Loading space is a priority.



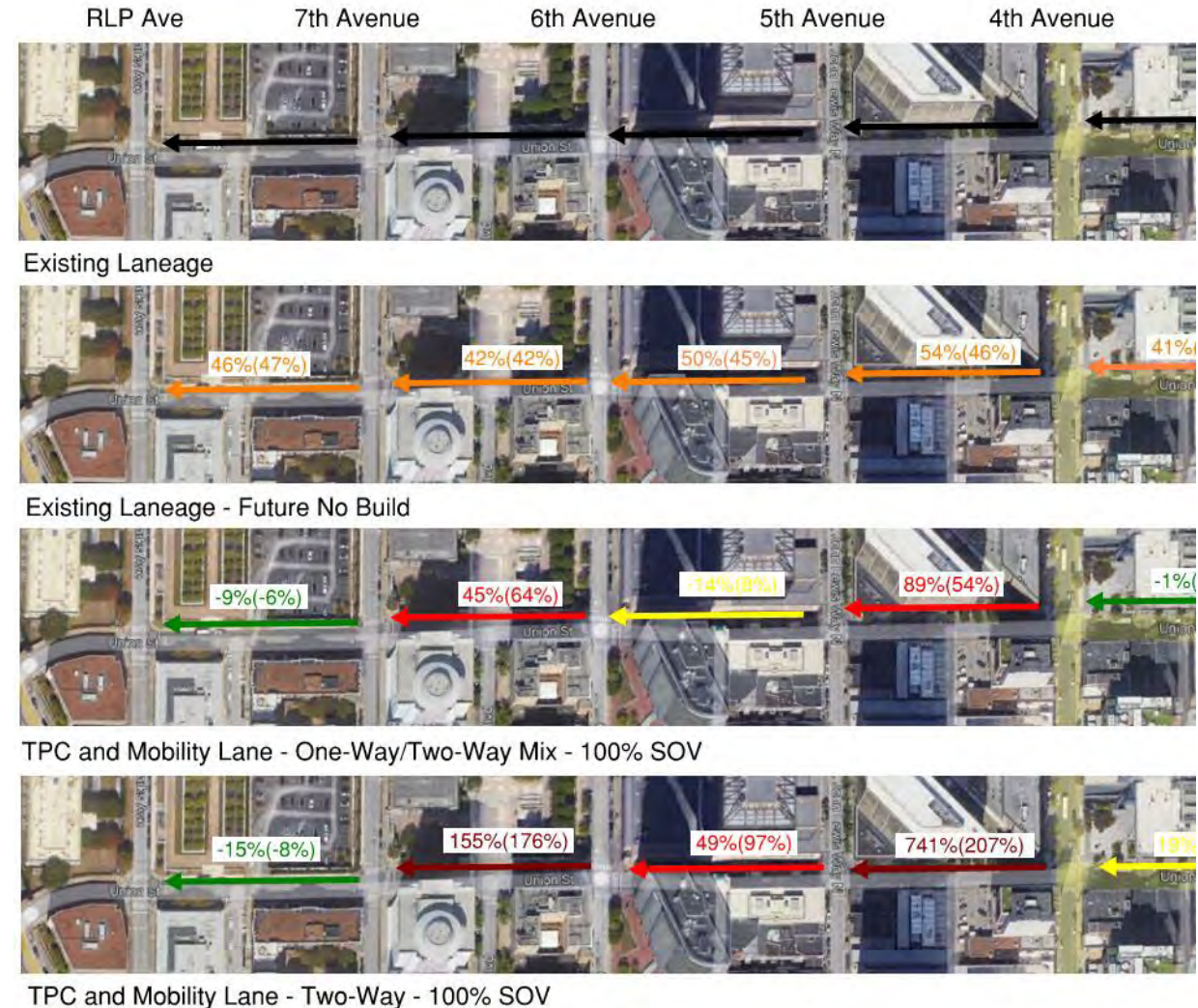
Nashville is popular.



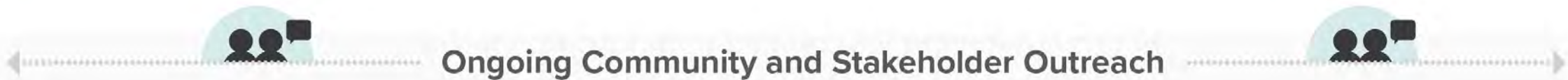
Streets are unsafe.

Recent Activities

- Incorporated STF/TAC feedback into projects and programs
- Completed system- and corridor-level modeling
- Refined select projects to address operational issues and conflicts
- Developed preliminary project phasing and cost estimates
- Prepared DRAFT recommendations and engagement approach



Connect Downtown Schedule





DRAFT Priority Recommendations

Developing Recommendations

Engagement

- STF/TAC
- Stakeholder meetings
- Summer 2022 outreach
- Fall 2022 outreach
- Spring 2023 outreach
- Presentations and briefings
- Traditional and social media

Technical Analysis

- Traffic counts
- Existing conditions
- TPC screening and evaluation
- Corridor- and system-level modeling
- Off-model analyses

**Draft
Recommendations**

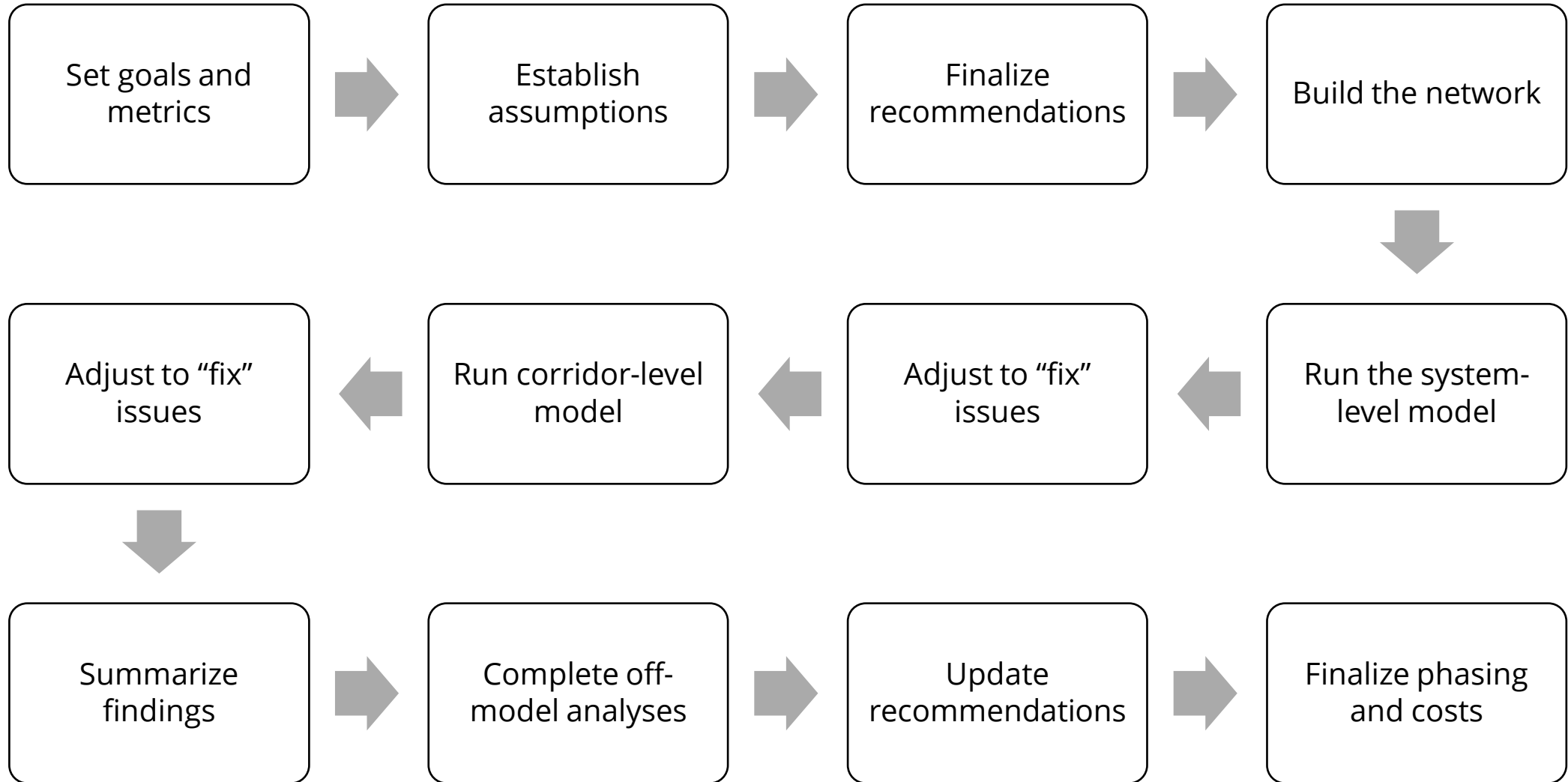
Proven Solutions & National Expertise

- Research
- Strategy papers
- Peer city conversations
- Consultant team

Past Work & Local Expertise

- Metro Nashville Transportation Plan
- nMotion and Better Bus
- WalknBike and Vision Zero Action Plan
- TDOT Long-Range Transportation Plan
- Client team

Our Modeling Process



Modeling Scenarios

Existing Scenario

- Current lane configurations
- Existing volumes

Future No Build Scenario

- Current lane configurations
- 10-year future growth volumes (+10-48% depending on growth zone)

Future Build Scenario

- TPC network, mobility lanes, and operational conversions
- Range of new growth accommodated in non-drive-alone modes

Key Modeling Takeaways

- Doing nothing is not an option
- The recommendations (our build scenario) work well overall and require only minor adjustments
- More mode shift makes the system function better than less mode shift
- Investing in transit is critical to achieve mode shift
- Complementing capital projects with a robust TDM program will further advance mode shift



DRAFT Recommendations by Mode

Traffic and Congestion Management

- **Traffic Management Center**
- **Adaptive signals**
 - Throughout inner loop
- **Operational conversions**
 - 2nd Ave two-way
 - 3rd Ave and 4th Ave one-way pair
 - 7th Ave one-way (MLK to Demonbreun)
- **TSMO enhancements**
 - Camera enforcement
 - “Don’t Block the Box” paint and signs
 - Digital message signs
 - Access management



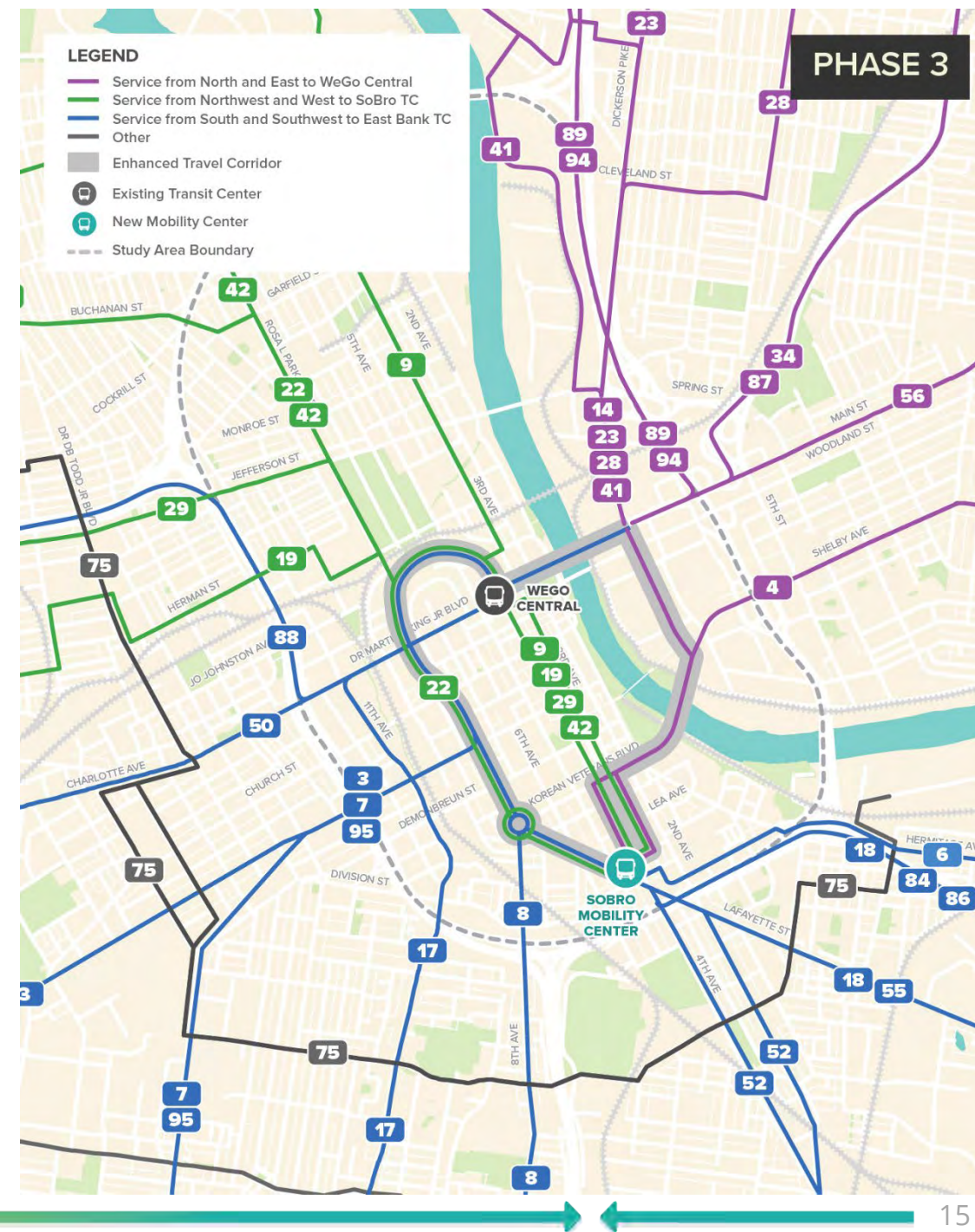
Transit Facilities

- **Mobility centers**
 - SoBro
 - East Bank
- **Transit Priority Corridors**
 - Westside
 - James Robertson
 - East Bank
- **Additional transit priority**
 - 3rd/4th Avenues
 - Better Bus “Major” routes



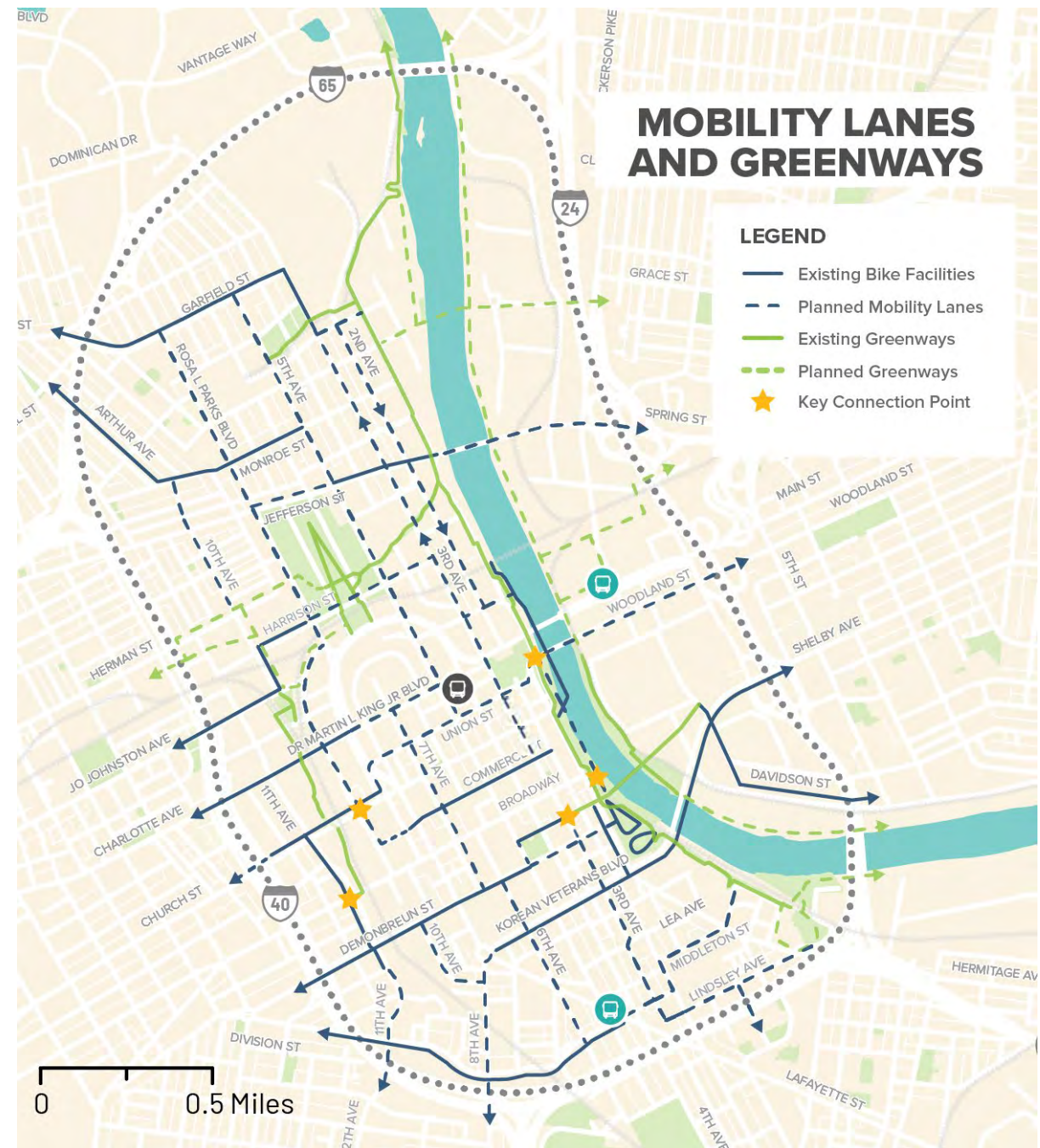
Transit Service

- **Reconfigured service to newly developing areas over time**
 - After Westside Transit Corridor
 - After SoBro Mobility
 - After East Bank Mobility Center
- **Increased trips regionwide (up to 67% more trips)**
- **More frequent service for longer hours (up to 24 hours a day)**



Active Transport

- **Mobility lanes**
 - Priorities include near-term WalknBike projects
 - Select existing facilities are upgraded
- **Planned greenways**
 - All currently anticipated facilities
- **Key connections**
 - Gulch grade changes
 - Multimodal links



Curb Space

- Use flexible approach to the curb as needs change through the day
- Increase number of loading/delivery zones and passenger loading zones while piloting new programs
- Implement no parking zones around Broadway after 4 PM
- Review all valet zone regulations
- Expand curb enforcement
- Develop strategies to address autonomous taxis and deliveries

CURB AND LOADING RECOMMENDATION 4PM - 4AM



Balancing Needs: 3rd Avenue

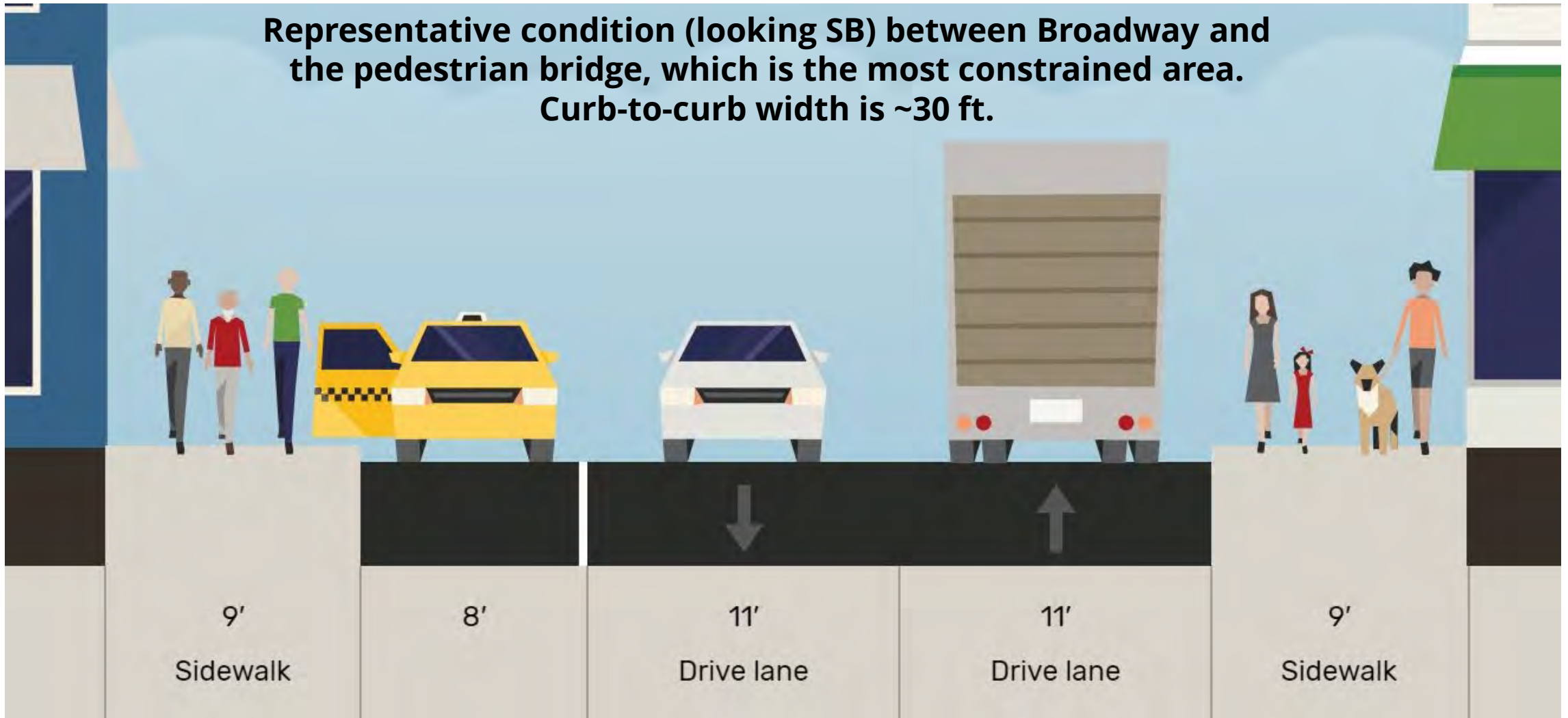
3rd Ave Desires

- **Transit:** Select dedicated lanes, carefully placed queue jumps, signal priority
- **Mobility Lane:** Two-way protected facility for bikes and scooters
- **Loading:** Delivery (Kid Rock, Ole Red), valet (Moxy), and passenger pick-up/drop-off throughout the day
- **Traffic:** Garage ingress and egress (Pinnacle Building, 211 Commerce, One Nashville) and event-related traffic (Symphony, Bridgestone)



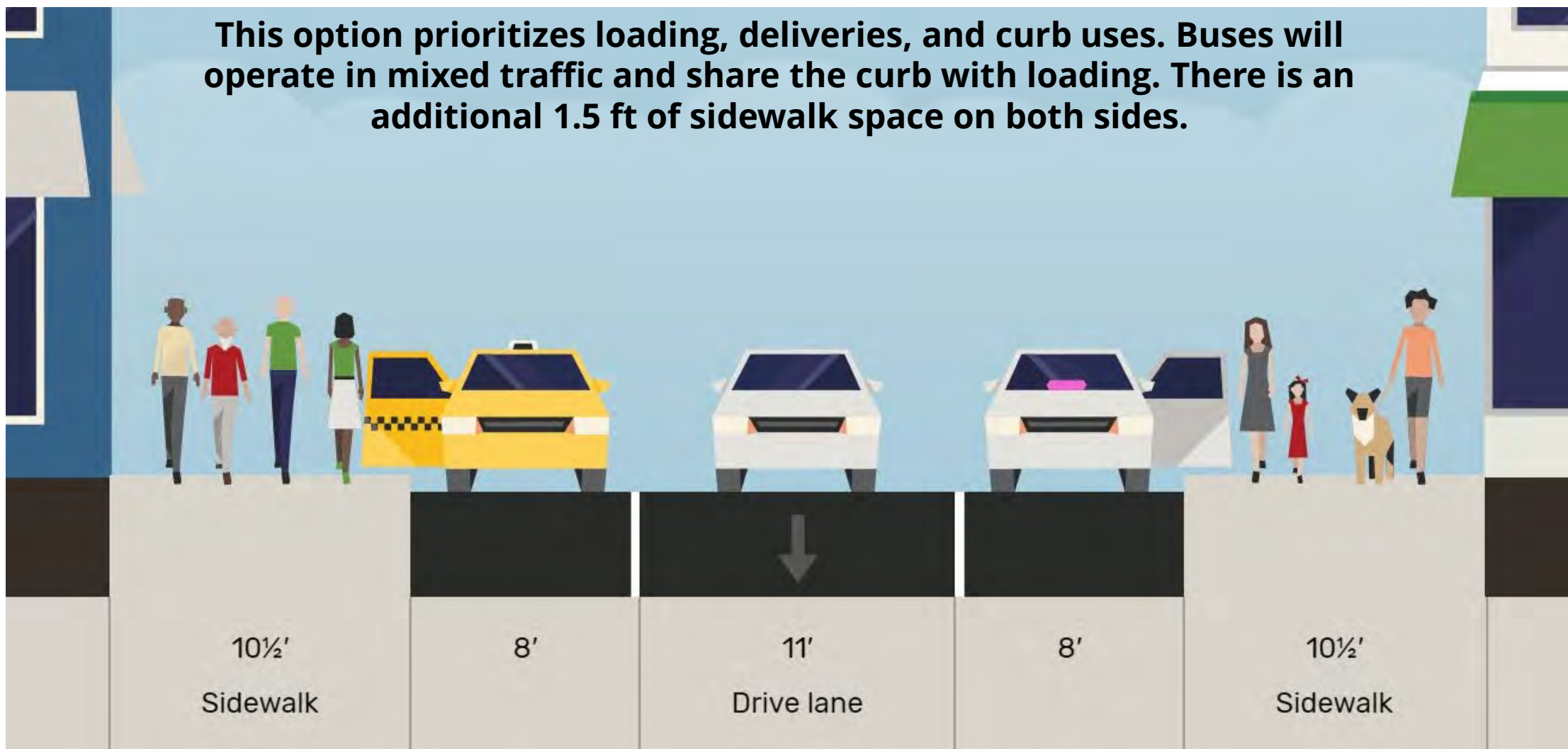
3rd Ave Current Condition

Representative condition (looking SB) between Broadway and the pedestrian bridge, which is the most constrained area. Curb-to-curb width is ~30 ft.



Future: Loading & Wider Sidewalks

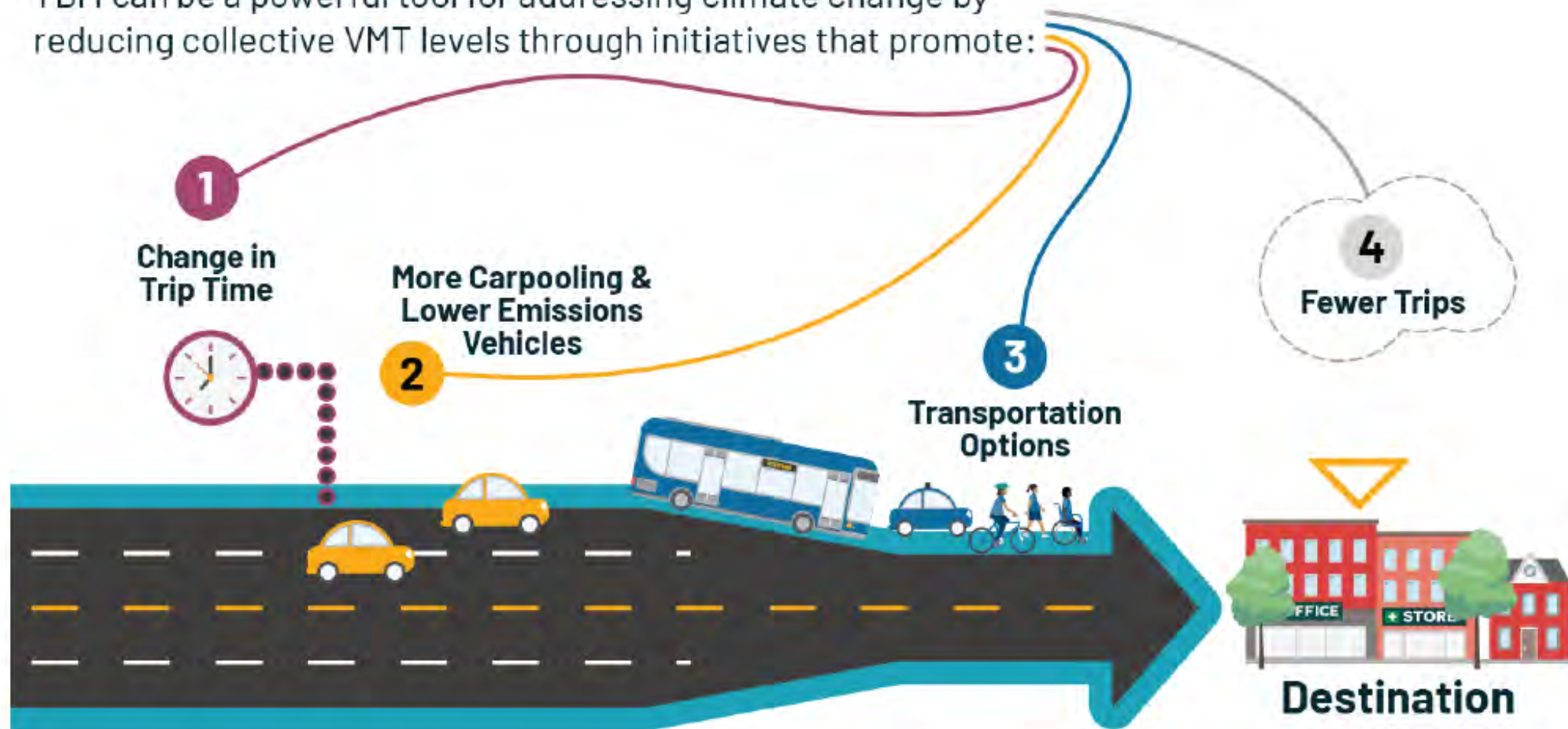
This option prioritizes loading, deliveries, and curb uses. Buses will operate in mixed traffic and share the curb with loading. There is an additional 1.5 ft of sidewalk space on both sides.



Programmatic Priorities

Transportation Demand Management

TDM can be a powerful tool for addressing climate change by reducing collective VMT levels through initiatives that promote:



TDM Building Blocks



Pricing Measures

- Charging for parking
- Cash benefits for non-drivers
- Discounts for HOVs or EVs



Physical Measures

- Real-time travel information
- Bike parking and amenities
- Shared vehicle stations



Programs & Policies

- Transit pass subsidies
- Visitor-focused incentives
- Code requirements



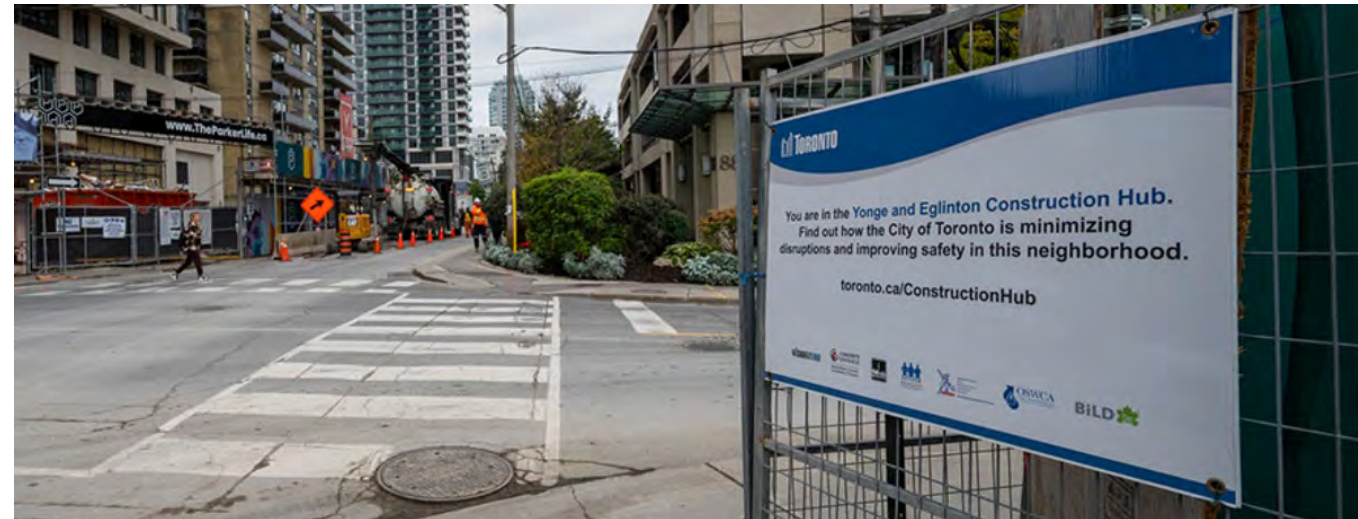
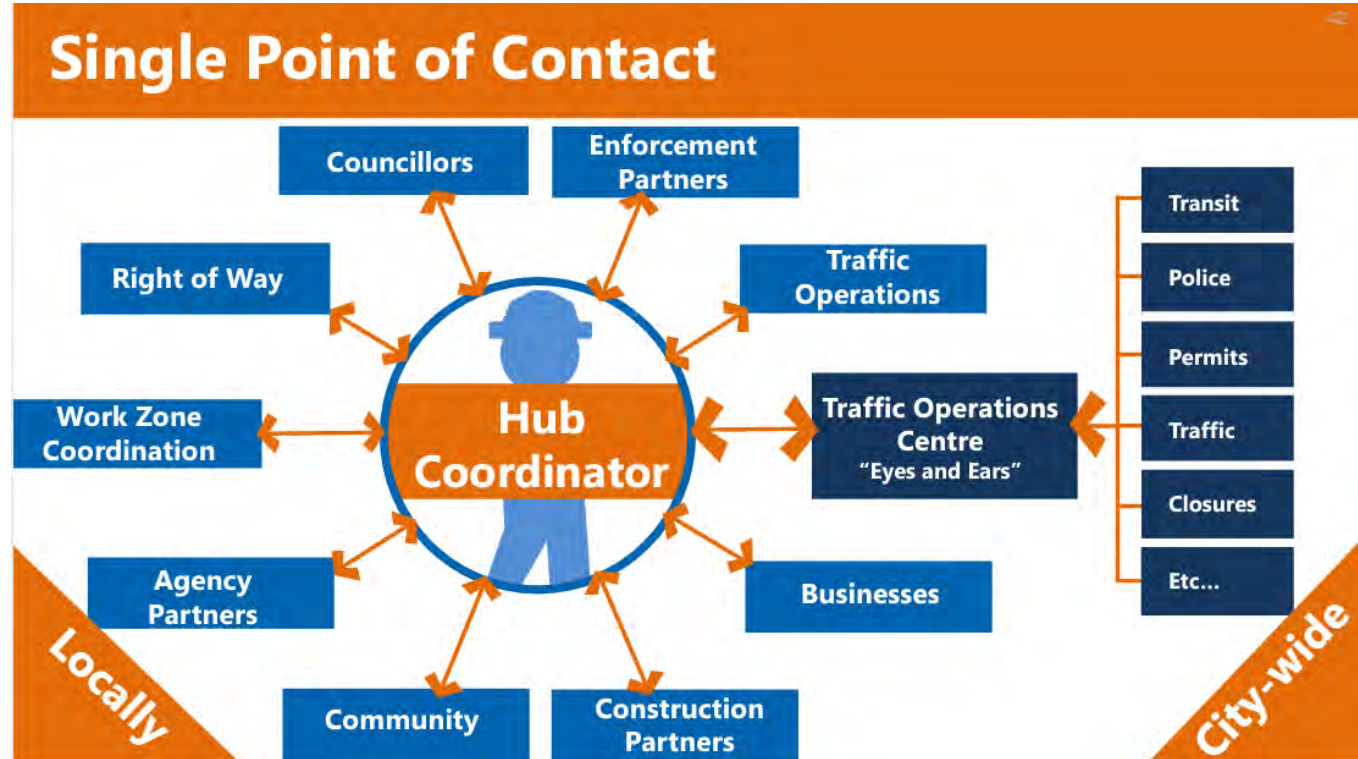
Promotional & Marketing Tools

- Onsite TDM coordinators
- Web and mobile app information
- Activities and competitions

Additional Programs

- New construction hub program
- Expanded event coordination and management program
- Updated micromobility program

Toronto Construction Hub Program



**What questions do you have about
the DRAFT recommendations?**

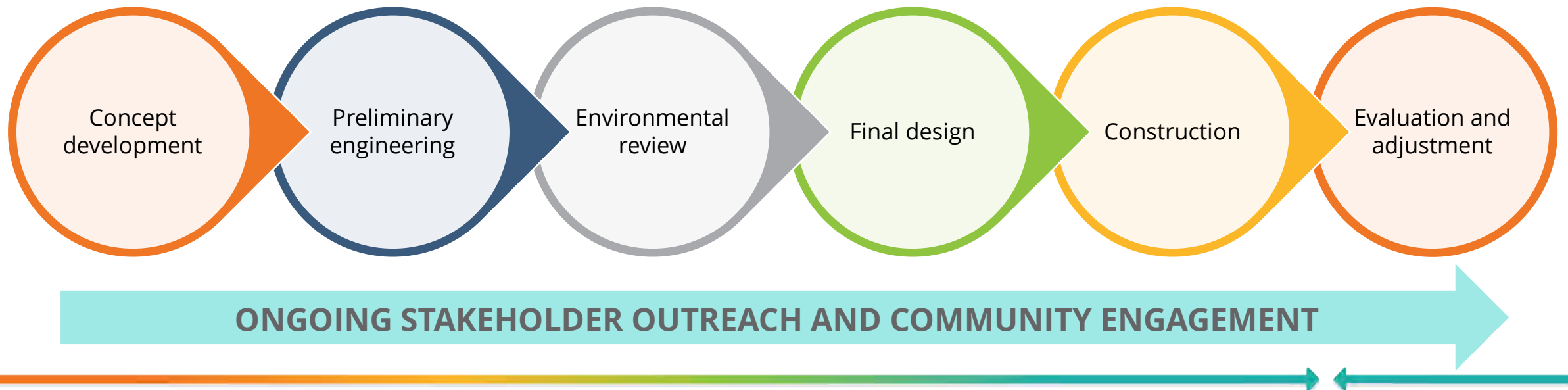
**How well do these projects and
programs meet Nashville's needs?**



DRAFT Project Phasing

Phasing & Funding Considerations

- Focus is a **10-year action plan**, with phasing to reflect priorities and reasonable funding assumptions
- Implementation doesn't happen overnight; **more engagement is needed** to work through design details
- Start with **lower-hanging fruit** to provide quick benefits at lower cost



Recommended Phases



2024-2026



2027-2029



2030+

Traffic Operations

- **Phase 1 (2024-2026):** TMC; adaptive signals at 30-50% of Downtown intersections; Vision Zero signalization; operational conversions
 - 2nd Ave two-way from Broadway to KVB (2024) and Union to Broadway (2025)
 - 3rd Ave one-way NB from MLK to interstate
 - 7th Ave one-way SB from MLK to Demonbreun
- **Phase 2 (2027-2029):** Adaptive signals at most remaining intersections; key intersection adjustments
- **Phase 3 (2030+):** Adaptive signals at all remaining intersections; intersection adjustments to support transit priority; operational conversion
 - 2nd Ave two-way from KVB to interstate



Transit Facilities & Service

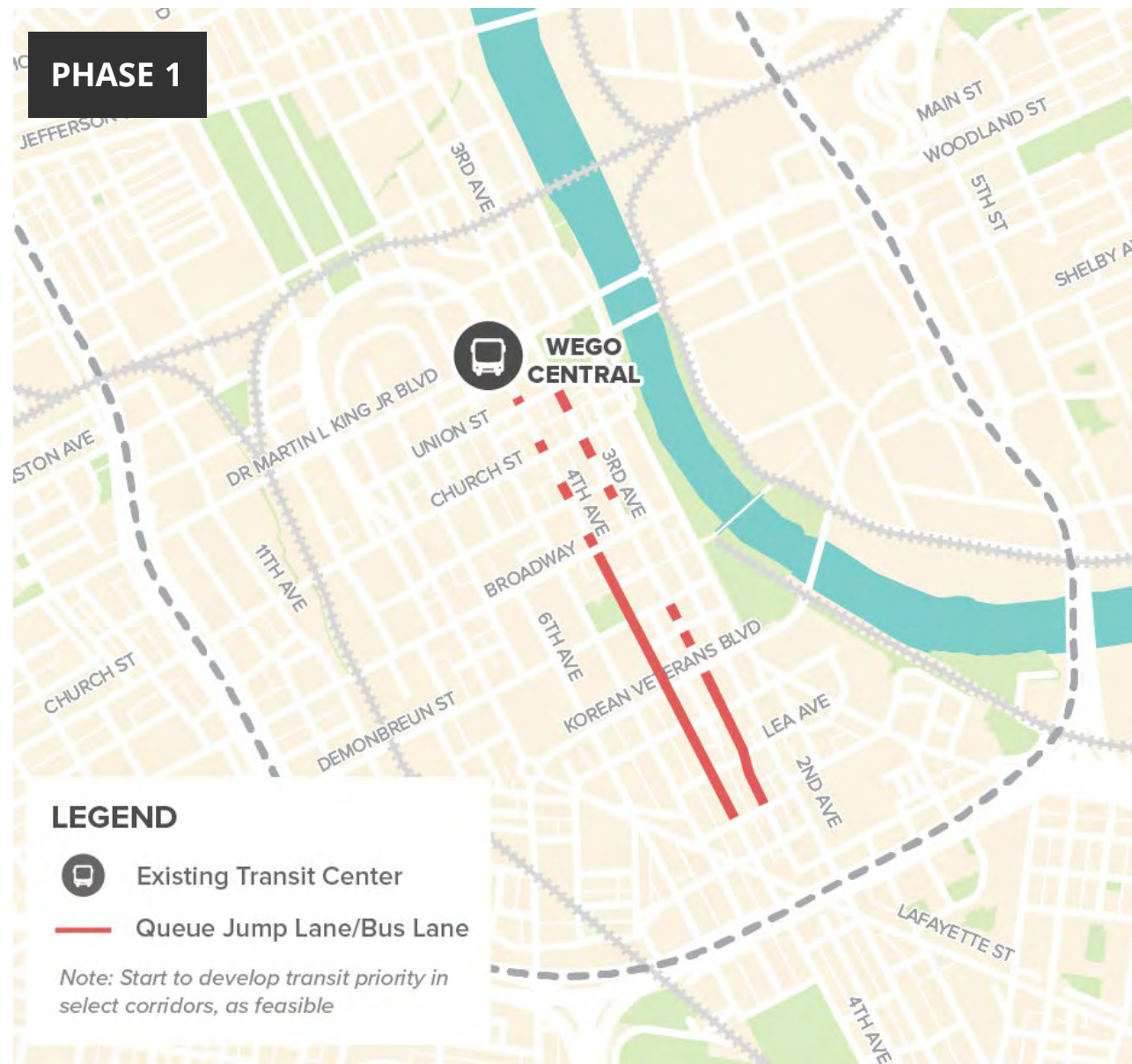
Phase 1 (2024-2026)

Complete:

- 3rd/4th Ave transit priority
- Full implementation of Better Bus Plan (15% increase in trips)

Begin:

- Start to develop transit priority in select frequent bus corridors
- Initiate design of Westside and James Robertson TPCs and SoBro Mobility Center



Transit Facilities & Service

Phase 2 (2027-2029)

Complete:

- Westside and James Robertson TPCs
- SoBro Mobility Center
- Expanded service to south and west sides of Downtown (31% increase in trips)

Continue:

- Develop transit priority in more frequent bus corridors

Begin:

- Initiate design of East Bank Mobility Center and East Bank TPC



Transit Facilities & Service

Phase 3 (2030+)

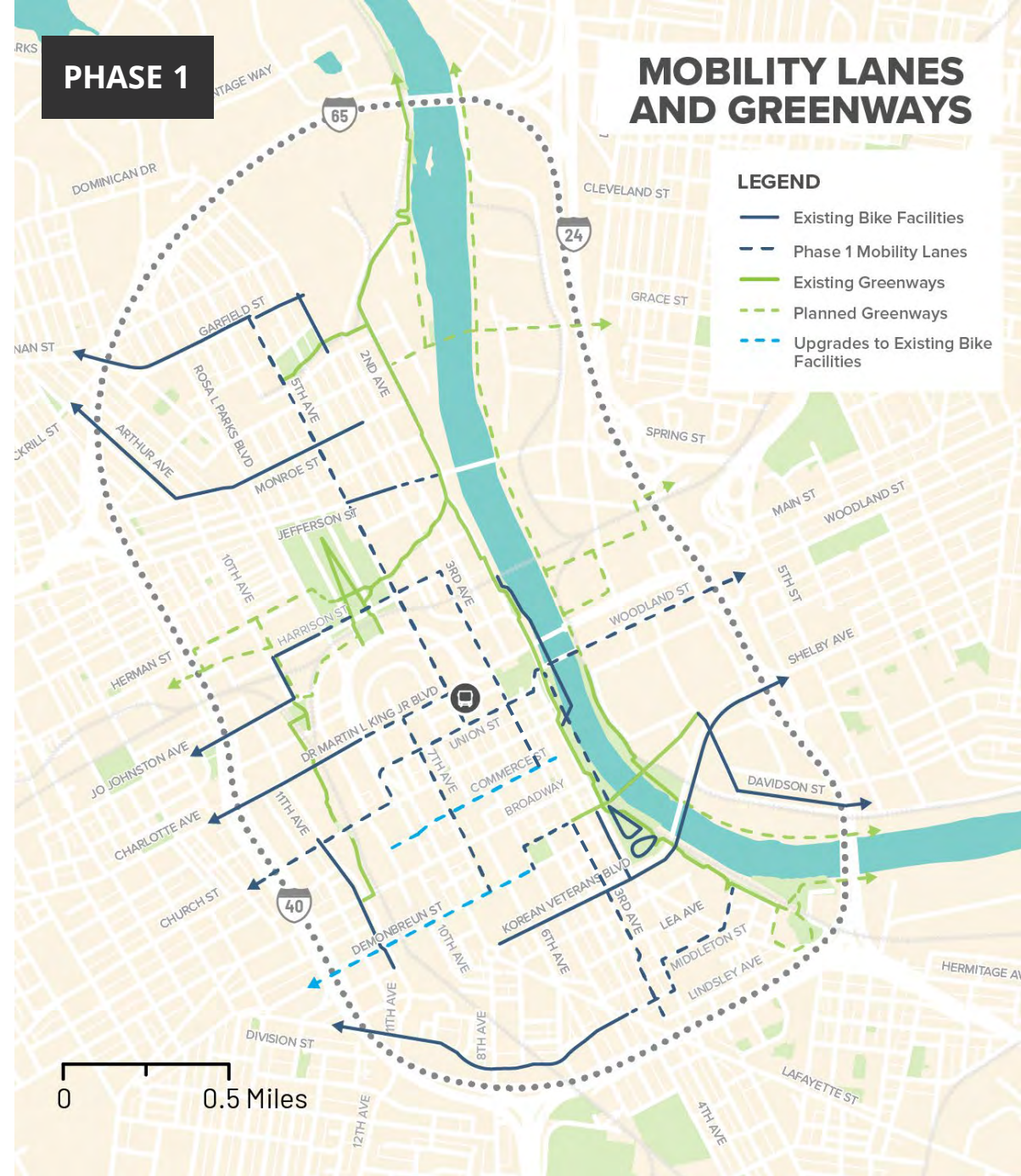
Complete:

- East Bank Mobility Center
- East Bank TPC
- Transit priority in all frequent corridors
- Robust service to East Bank following critical mass of development (67% increase in trips)



Mobility Lanes

- Phase 1 (2024-2026):** Aggressively advances core of Downtown
 - 1st Ave, 3rd Ave (mid and south), Rep John Lewis, and 7th Ave
 - Jefferson St (east), Harrison St, MLK Blvd, Church/Union/Woodland St, and Middleton St
 - Upgrades to Commerce St and Demonbreun St
- Phase 2 (2027-2029):** Fills gaps and expands links outside of the core to greenways
- Phase 3 (2030+):** Connects outer edges as Pie Town, SoBro, and East Bank continue developing



Mobility Lanes

- **Phase 1 (2024-2026):** Aggressively advances core of Downtown
- **Phase 2 (2027-2029):** Fills gaps and expands links outside of the core to greenways
 - 2nd/3rd Aves (north), Rosa Parks Blvd (mid), 8th Ave S, 10th Ave (north and mid), 11th/12th Ave
 - Jefferson St (west), Lindsley Ave
- **Phase 3 (2030+):** Connects outer edges as Pie Town, SoBro, and East Bank continue developing



Mobility Lanes

- **Phase 1 (2024-2026):** Aggressively advances core of Downtown
- **Phase 2 (2027-2029):** Fills gaps and expands links outside of the core to greenways
- **Phase 3 (2030+):** Connects outer edges as Pie Town, SoBro, and East Bank continue developing
 - Rosa Parks Blvd (north), 6th Ave, 10th Ave (south)
 - Jefferson St Bridge
 - Upgrade to KVB with TPC



Curb Management

- **Phase 1 (2024-2026)**
 - Pilot Smart Loading and PU/DO programs
 - Develop strategy for autonomous taxi services
 - Digitize curbside regulations and inventory existing Valet Program
 - Add resources for data analytics, enforcement, and compliance
 - Advance permit system/process
 - Update Land Use Code
 - Expand accessible curbside use
- **Phase 2 (2027-2029)**
 - Create centralized carrier program
 - Develop autonomous delivery services strategy
- **Phase 3 (2030+)**
 - Implement autonomous delivery services



Phase 1: 2024-2026

- Traffic Operations:** TMC; adaptive signals at 30% to 50% of intersections; two-way 2nd Ave between Union and KVB; one-way 3rd and 7th Aves; Vision Zero signalization at key intersections
- Transit Facilities:** 3rd and 4th Ave transit priority opens; transit priority as feasible on select frequent corridors; Westside TPC and SoBro Mobility Center design
- Transit Service:** Full implementation of Better Bus Plan; **15%** increase in trips
- Mobility Lanes:** 1st, 3rd (mid and south), Rep John Lewis, 7th, Harrison, MLK, Church/Union/Woodland, and Middleton; upgrades to Commerce and Demonbreun
- Curb Management:** Pilot two programs; develop strategy for autonomous taxi services; digitize regulations and inventory Valet Program; add enforcement (+) resources; advance permit system; update code; expand accessible curbside use
- Programs:** Event management; construction hubs; residential TDM



Phase 2: 2027-2029

- Traffic Operations:** Adaptive signals at most remaining intersections; key intersection adjustments
- Transit Facilities:** Westside TPC and SoBro Mobility Center open; additional transit priority on select frequent corridors; East Bank TPC and Mobility Center design
- Transit Service:** Better service to west and south sides of Downtown; **38%** increase in trips
- Mobility Lanes:** 2nd/3rd Aves (north), Rosa Parks Blvd (mid), 8th Ave S, 10th Ave (north and mid), 11th/12th Ave, Jefferson St (west), Lindsley Ave
- Curb Management:** Create centralized carrier program; develop strategy for autonomous delivery services
- Programs:** Visitor TDM; expanded employer TDM



Phase 3: 2030+

- Traffic Operations:** Adaptive signals at all remaining intersections; two-way 2nd Ave between KVB and interstate; key intersection adjustments to support TPCs and expanded transit priority
- Transit Facilities:** East Bank Mobility Center and East Bank TPC open; additional transit priority on all frequent corridors
- Transit Service:** Robust service to East Bank following development; **67%** increase in trips
- Mobility Lanes:** Rosa Parks Blvd (north), 6th Ave, 10th Ave (south), Jefferson St Bridge; upgrade KVB
- Curb Management:** Implement autonomous delivery services
- Programs:** Additional investments to support Phases 1 and 2



Are there any changes you would make to the phrasing?

What concerns will those you represent have about these recommendations?



Upcoming Activities & Next Steps

Engagement Tools & Approaches

- Draft recommendations summary
- Brief online survey focused on key recommendations
- 2 general public open houses
- Partner-hosted events (e.g., Civic Design Center)
- Social and traditional media blitz
- Key stakeholder discussions
- Briefings for new elected officials



Key Stakeholder Briefings

- Downtown residents
- Nashville Chamber
- 3rd Ave stakeholders
- Hospitality industry reps
- Musicians
- Transit Alliance Board
- Transit operators
- Delivery drivers/transpotainment
- Amazon (and large employers)

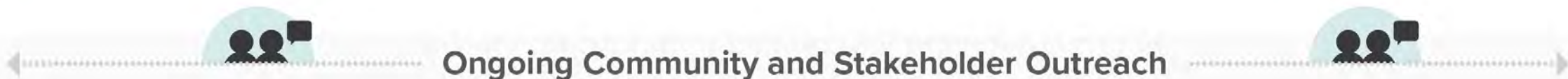


Key Briefing & Engagement Dates

- **Week of 10/23** – Pre-release briefings for key stakeholders and electeds
- **11/1** – Launch online public outreach
- **11/2 to 12/14** – As-needed briefings and meetings
- **11/14 and 11/15** – Connect Downtown open houses
- **11/23** – Thanksgiving
- **11/30** – Civic Design Center event
- **12/15** – Close online public outreach and conclude all briefings/meetings



Connect Downtown Schedule





Thank you!

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