

KARL F. DEAN
MAYOR



METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

Metropolitan Historic Zoning Commission
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STAFF RECOMMENDATION 3713 Richland Avenue June 19, 2013

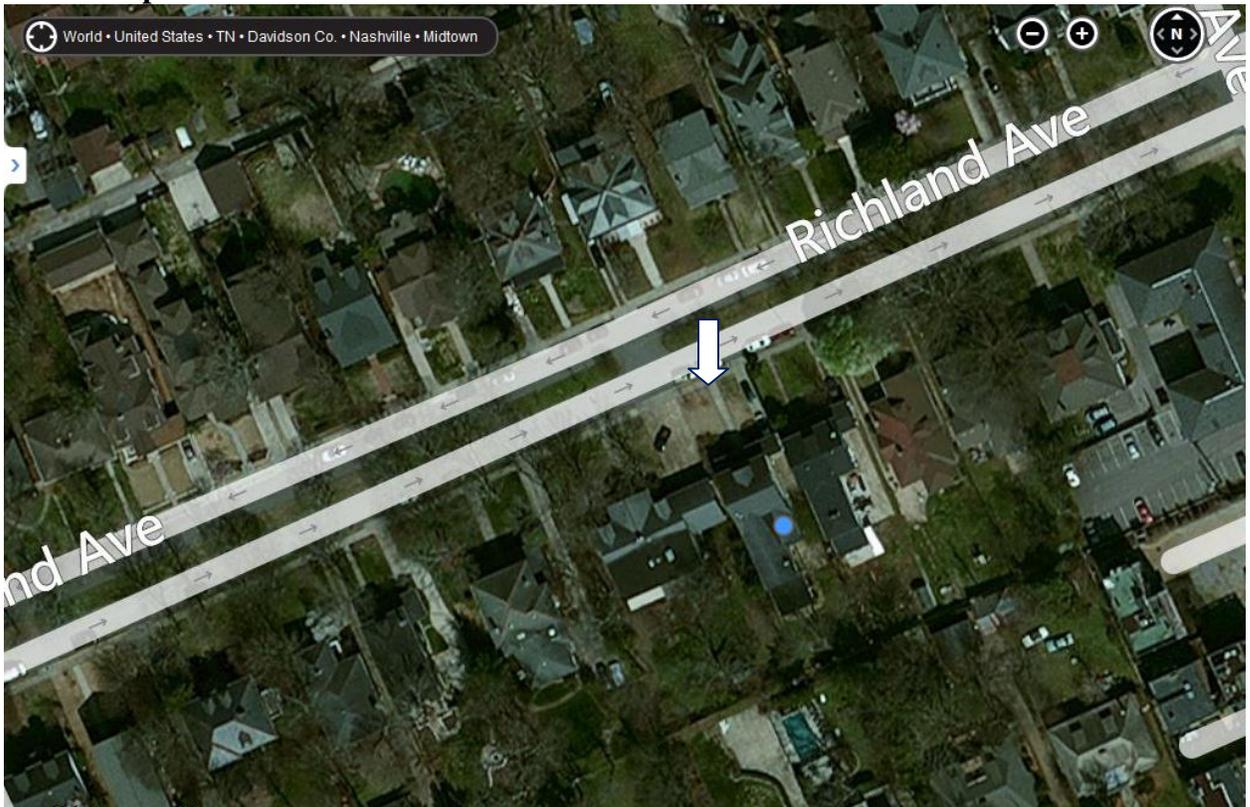
Application: New Construction—Addition; Setback reduction
District: Richland-West End Neighborhood Conservation Zoning Overlay
Council District: 24
Map and Parcel Number: 10409011700
Applicant: Preston Quirk
Project Lead: Melissa Baldock, melissa.baldock@nashville.gov

<p>Description of Project: Application is to construct an attached carport that requires a setback reduction.</p> <p>Recommendation Summary: Staff recommends approval of the project, finding that the carport and setback reduction meet Section II.B.1. and II.B.2. of the <i>Richland-West End Neighborhood Conservation Zoning Overlay: Handbook and Design Guidelines</i>.</p>	<p>Attachments A: Photographs B: Site Plan C: Elevations</p>
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Vicinity Map:



Aerial Map:



Applicable Design Guidelines:

II.B.1 New Construction

a. Height

The height of the foundation wall, porch roof(s), and main roof(s) of a new building shall be compatible, by not contrasting greatly, with those of surrounding historic buildings.

b. Scale

The size of a new building and its mass in relation to open spaces shall be compatible, by not contrasting greatly, with surrounding historic buildings.

Foundation lines should be visually distinct from the predominant exterior wall material. Examples are a change in material, coursing or color.

c. Setback and Rhythm of Spacing

The setback from front and side yard property lines established by adjacent historic buildings should be maintained. Generally, a dominant rhythm along a street is established by uniform lot and building width. Infill buildings should maintain that rhythm.

The Commission has the ability to reduce building setbacks and extend height limitations of the required underlying base zoning for new construction, additions and accessory structures (ordinance no. BL2007-45).

Appropriate setback reductions will be determined based on:

- *The existing setback of the contributing primary buildings and accessory structures found in the immediate vicinity;*
- *Setbacks of like structures historically found on the site as determined by historic maps, site plans or photographs;*
- *Shape of lot;*
- *Alley access or lack thereof;*
- *Proximity of adjoining structures; and*
- *Property lines.*

Appropriate height limitations will be based on:

- *Heights of historic buildings in the immediate vicinity*
- *Existing or planned slope and grade*

d. Materials, Texture, Details, and Material Color

The materials, texture, details, and material color of a new building's public facades shall be visually compatible, by not contrasting greatly, with surrounding historic buildings. Vinyl and aluminum siding are not appropriate.

T-1-11- type building panels, "permastone", E.I.F.S. and other artificial siding materials are generally not appropriate. However, pre-cast stone and cement fiberboard siding are approvable cladding materials for new construction; but pre-cast stone should be of a compatible color and texture to existing historic stone clad structures in the district; and cement fiberboard siding, when used for lapped siding, should be smooth and not stamped or embossed and have a minimum of a 5" reveal.

Shingle siding should exhibit a straight-line course pattern and exhibit a maximum exposure of seven inches (7").

Four inch (4") nominal corner boards are required at the face of each exposed corner.

Stud wall lumber and embossed wood grain are prohibited.

Belt courses or a change in materials from one story to another are often encouraged for large two-story buildings to break up the massing.

When different materials are used, it is most appropriate to have the change happen at floor lines.

Clapboard sided chimneys are generally not appropriate. Masonry or stucco is appropriate. Texture and tooling of mortar on new construction should be similar to historic examples. Asphalt shingle is an appropriate roof material for most buildings. Generally, roofing should not have strong simulated shadows in the granule colors which results in a rough, pitted appearance; faux shadow lines; strongly variegated colors; colors that are too light (e.g.: tan, white, light green); wavy or deep color/texture used to simulate split shake shingles or slate; excessive flared form in the shingle tabs; uneven or sculpted bottom edges that emphasize tab width or edges, unless matching the original roof.

e. **R o o f S h a p e**

The roof(s) of a new building shall be visually compatible, by not contrasting greatly, with the roof shape, orientation, and pitch of surrounding historic buildings.

Roof pitches should be similar to the pitches found in the district. Historic roofs are generally between 6/12 and 12/12.

Roof pitches for porch roofs are typically less steep, approximately in the 3-4/12 range.

Generally, two-story residential buildings have hipped roofs.

Generally, dormers should be located on the roof. Wall dormers are not typical in the historic context and accentuate height so they should be used minimally and generally only on secondary facades. When they are appropriate they should be no wider than the typical window openings and should not project beyond the main wall.

f. **O r i e n t a t i o n**

The orientation of a new building's front facade shall be visually consistent with surrounding historic buildings.

New buildings shall incorporate at least one front street-related porch that is accessible from the front street.

Side porches or porte cocheres may also be appropriate as a secondary entrance, but the primary entrance should address the front.

Front porches generally should be a minimum of 6' deep, have porch racks that are 1'-3' tall and have posts that include bases and capitals.

For multi-unit developments, interior dwellings should be subordinate to those that front the street. Subordinate generally means the width and height of the buildings are less than those that front the street.

For multi-unit developments, direct pedestrian connections should be made between the street and any interior units. The entrances to those pedestrian connections generally should be wider than the typical spacing between buildings along the street.

Shared driveways should be a single lane, not just two driveways next to each other. Sometimes this may be accomplished with a single lane curb cut that widens to a double lane deeper into the lot.

Generally, curb cuts should not be added.

g. **P r o p o r t i o n a n d R h y t h m o f O p e n i n g s**

The relationship of width to height of windows and doors, and the rhythm of solids (walls) to voids (door and window openings) in a new building shall be compatible, by not contrasting greatly, with surrounding historic buildings.

Window openings on the primary street-related or front façade of new construction should be representative of the window patterns of similarly massed historic structures within the district. In most cases, every 8-13 horizontal feet of flat wall surface should have an opening (window or door) of at least 4 square feet. More leniencies can be given to minimally visible side or rear walls. Double-hung windows should exhibit a height to width ratio of at least 2:1.

Windows on upper floors should not be taller than windows on the main floor since historically first floors have higher ceilings than upper floors and so windows were typically taller on the first floor.

Single-light sashes are appropriate for new construction. If using multi-light sashes, muntins should be fully simulated and bonded to the glass, and exhibit an interior bar, exterior bar, as well as a spacer between glass panes.

Four inch (nominal) casings are required around doors, windows and vents on non-masonry buildings. (Brick molding is only appropriate on masonry buildings.)

Brick molding is required around doors, windows and vents within masonry walls.

h . Outbuildings

1) A new garage or storage building should reflect the character of the period of the house to which the outbuilding will be related. The outbuilding should be compatible, by not contrasting greatly, with surrounding historic outbuildings in terms of height, scale, roof shape, materials, texture, and details.

Historically, outbuildings were either very utilitarian in character, or (particularly with more extravagant houses) they repeated the roof forms and architectural details of the houses to which they related. Generally, either approach is appropriate for new outbuildings. Brick, weatherboard, and board - and -batten are typical siding materials. Outbuildings with weatherboard siding typically have wide cornerboards and window and door casings (trim). Generally, the minimum roof pitch appropriate for outbuildings is 12:4. Decorative raised panels on publicly visible garage doors are generally not appropriate. Publicly visible pedestrian doors must either be appropriate for the style of house to which the outbuilding relates or be flat with no panels. Publicly visible windows should be appropriate to the style of the house.

Roof

- *Generally, the eaves and roof ridge of any new accessory structure should not be higher than those of the existing house.*
- *Roof slopes on simple, utilitarian buildings do not have to match the roof slopes of the main structure, but must maintain at least a 4/12 pitch.*
- *The front face of any dormer must be set back at least 2' from the wall of the floor below.*

Windows and Doors

- *Metal overhead doors are acceptable on garages when they are simple and devoid of overly decorative elements typical on high-style wooden doors.*
- *Publicly visible pedestrian doors must either be appropriate for the style of house to which the outbuilding relates or be flat with no panels.*
- *Publicly visible windows should be appropriate to the style of the house.*
- *Double-hung windows are generally twice as tall as they are wide and of the single-light sash variety.*
- *For street-facing facades, garages with more than one-bay should have multiple single doors rather than one large door to accommodate more than one bay.*
- *Decorative raised panels on publicly visible garage doors are generally not appropriate.*

Siding and Trim

- *Brick, weatherboard, and board-and-batten are typical siding materials. Outbuildings with weatherboard siding typically have wide cornerboards and window and door casings (trim).*
- *Exterior siding may match the existing contributing building's original siding; otherwise, siding should be wood or smooth cement-fiberboard lap siding with a maximum exposure of five inches (5"), wood or smooth cement-fiberboard board-and-batten or masonry.*
- *Four inch (4") (nominal) corner-boards are required at the face of each exposed corner.*
- *Stud wall lumber and embossed wood grain are prohibited.*
- *Four inch (4") (nominal) casings are required around doors, windows, and vents within clapboard walls. (Brick molding is not appropriate on non-masonry clad buildings.)*

- *Brick molding is required around doors, windows, and vents within masonry walls.*
- 2) Outbuildings should be situated on a lot as is historically typical for surrounding historic buildings.

Generally new garages should be placed close to the alley, at the rear of the lot, or in the original location of an historic accessory structure.

Lots without rear alleys may have garages located closer to the primary structure. The appropriate location is one that matches the neighborhood or can be documented by historic maps. Generally, attached garages are not appropriate; however, instances where they may be are:

1. *where they are a typical feature of the neighborhood*
2. *When the location of the attached garage is in the general location of an historic accessory building, the new garage is located in the basement level, and the vehicular access is on the rear elevation.*

i. Utilities

Utility connections such as gas meters, electric meters, phone, cable, and HVAC condenser units should be located so as to minimize their visibility from the street.

Generally, utility connections should be placed no closer to the street than the mid point of the structure. Power lines should be placed underground if they are carried from the street and not from the rear or an alley.

j. Public Spaces

Landscaping, sidewalks, signage, lighting, street furniture and other work undertaken in public spaces by any individual, group or agency shall be presented to the MHZC for review of compatibility with the character of the district.

II.B.2. Additions

- a. Generally, an addition should be situated at the rear of a building in such a way that it will not disturb either front or side facades.

Additions normally not recommended on historic structures may be appropriate for non-historic structures. Front or side alterations to non-historic buildings that increase habitable space or change exterior height should be compatible, by not contrasting greatly, with the adjacent historic buildings.

Placement

- *Additions should be located at the rear of the existing structure.*
- *Additions should be physically distinguished from the historic building and generally fit within the shadow line of the existing building.*
- *Connections to additions should, as much as possible, use existing window and door openings rather than remove significant amounts of rear wall material.*
- *Generally rear additions should inset one foot, for each story, from the side wall.*

In order to assure that an addition has achieved proper scale, the addition should generally be shorter and thinner than the existing building. Exceptions may be made when unusual constraints make these parameters unreasonable, such as:

- *An extreme grade change*
- *Atypical lot parcel shape or size*

In these cases, an addition may rise above or extend wider than the existing building; however, generally the addition should not be higher and extend wider.

When an addition needs to be wider:

Rear additions that are wider than an existing historic building may be appropriate when the building is narrower than 30' or shifted to one side of the lot. In these instances, a structural

alcove or channel must separate the existing building from the new addition. The structural alcove should sit in a minimum of 1' and be at least twice as long as it is deep. In addition, a rear addition that is wider should not wrap the rear corner.

Foundation

Foundation walls should set in from the existing foundation at the back edge of the existing structure by one foot for each story or half story. Exception: When an addition is a small one-room deep (12' deep or less) addition that spans the width of the structure, and the existing structure is masonry with the addition to be wood (or appropriate substitute siding) since the change in materials will allow for a minimum of a four inch (4") inset.

Foundation height should match or be lower than the existing structure.

Foundation lines should be visually distinct from the predominant exterior wall material. Examples are a change in materials or a change in masonry coursing, etc.

Roof

The height of the addition's roof and eaves must be less than or equal to the existing structure.

Visually evident roof slopes should match the roof slopes of the existing structure, and roof planes should set in accordingly for rear additions.

Skylights should not be located on the front-facing slope of the roof. Skylights should be flat (no bubble lenses) with a low profile (no more than six inches tall) and only be installed behind the midpoint of the building.)

Side additions

When a lot width exceeds 60' or the standard lot width on the block, it may be appropriate to add a side addition to a historic structure.

- *The addition should set back from the face of the historic structure and should be subservient in height, width and massing to the historic structure.*
- *Side additions should be narrower than half of the historic building width and exhibit a height of at least 2' shorter than the historic building.*
- *To deemphasize a side addition, the roofing form should generally be a hip or side-gable roof form.*

b. The creation of an addition through enclosure of a front porch is not appropriate

The addition should set back from the face of the historic structure (at or beyond the midpoint of the building) and should be subservient in height, width and massing to the historic structure.

Side additions should be narrower than half of the historic building width and exhibit a height of at least 2' shorter than the historic building.

To deemphasize a side addition, the roofing form should generally be a hip or side-gable roof form.

c. Contemporary designs for additions to existing properties are not discouraged when such additions do not destroy significant historical, architectural, or cultural material; and when such design is compatible, by not contrasting greatly, with the size, scale, color, material, and character of the property, neighborhood, or environment.

Side porch additions may be appropriate for corner building lots or lots more than 60' wide.

d. A new addition should be constructed in such a manner that if the addition were to be removed in the future, the essential form and integrity of the original structure would be unimpaired.

e. Additions should follow the guidelines for new construction.

Connections should, as much as possible, use existing window and door openings rather than remove significant amounts of rear wall material.

Background: 3713 Richland Avenue is a c. 1915 four square house that is listed as contributing to the Richland-West End National Register Historic District.



Analysis and Findings:

Application is to construct an attached carport that requires a setback reduction.

Location, Setback: The proposed carport addition is attached to a non-historic covered porch portion of the house at the rear. It will be accessed via an existing driveway. The carport will extend beyond the main body of the house, but will be located in line with the existing porte cochere at the front of the house. The carport will be no wider than the existing porte cochere. The carport requires a setback reduction. Base zoning requires that the structure be at least five feet (5') from the side property line, but the structure is proposed to be only three feet (3'). Staff finds the proposed setback reduction to be appropriate in this instance because it lines up with the existing porte cochere, which also does not meet the required five foot (5') setback. Staff finds that the location and setback for the proposed addition meet Section II.B.1c. and II.B.2. of the design guidelines.

Height, Scale: The existing house is two and a half stories tall and has a ridge height of approximately thirty-five feet (35') and an eave height of approximately twenty-eight feet (28'). The addition will be one story and significantly shorter than the house. Because of the slope of the site, the roof of the carport will be approximately in line with the house's foundation line.. It will have a maximum height of approximately nine feet, nine inches (9'9").

The existing house is approximately thirty-two feet, six inches (32'6") wide not including the porte cochere. With the porte cochere, the house is forty-two feet, six inches (42'6") wide. The house is one hundred and eleven feet (111') deep, and the carport will not increase the depth of the house. The addition attaches to the non-historic porch addition that is inset twelve feet, six inches (12'6") from the left sidewall of the house. The carport is twenty-one feet, five and a half inches (21'5½") wide and twenty feet, three inches (20'3") deep. The carport extends approximately eight feet, six inches (8'6") beyond the sidewall of the house, matching the line of the porte cochere at the front of the house.

Staff finds that the addition's height and scale are modest and meet Sections II.B.1.a, II.B.1.b., and II.B.2. of the design guidelines.

Materials: The new carport will be constructed of cedar posts with Azek columns that include a cap and a base. The roof will have cedar trellis beams and will not be covered. The structure will remain open on all sides. Staff finds that these materials meet Sections II.B.1.d and II.B.2. of the design guidelines

Roof Form: The existing house's primary roof form is a side gable. The front porte cochere's roof ties into the porch roof and is flat. The proposed carport will also have a flat roof, which is appropriate for its use and because it matches the front porte cochere. Staff finds that the roof form meets Sections II.B.1.e. and II.B.2. of the design guidelines.

Proportion and Rhythm of Openings: The carport will be open on all three sides, which is appropriate. Staff therefore finds that it meets Section II.B.1.g. and II.B.2. of the design guidelines.

Recommendation Summary: Staff recommends approval of the project, finding that the carport and setback reduction meet Section II.B.1. and II.B.2. of the *Richland-West End Neighborhood Conservation Zoning Overlay: Handbook and Design Guidelines*.



Side of the house beyond the porte cochere



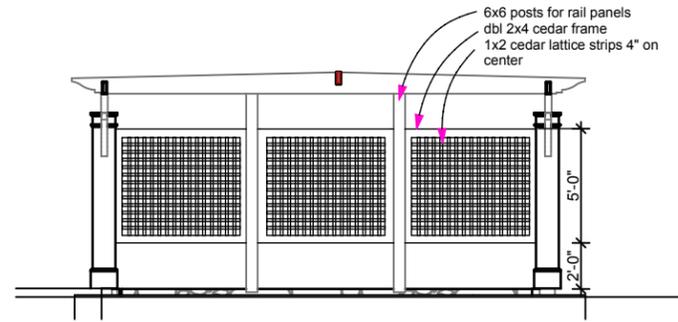
The carport will attach to the screened porch at the rear of the house. Its roof will match the porch floor height.



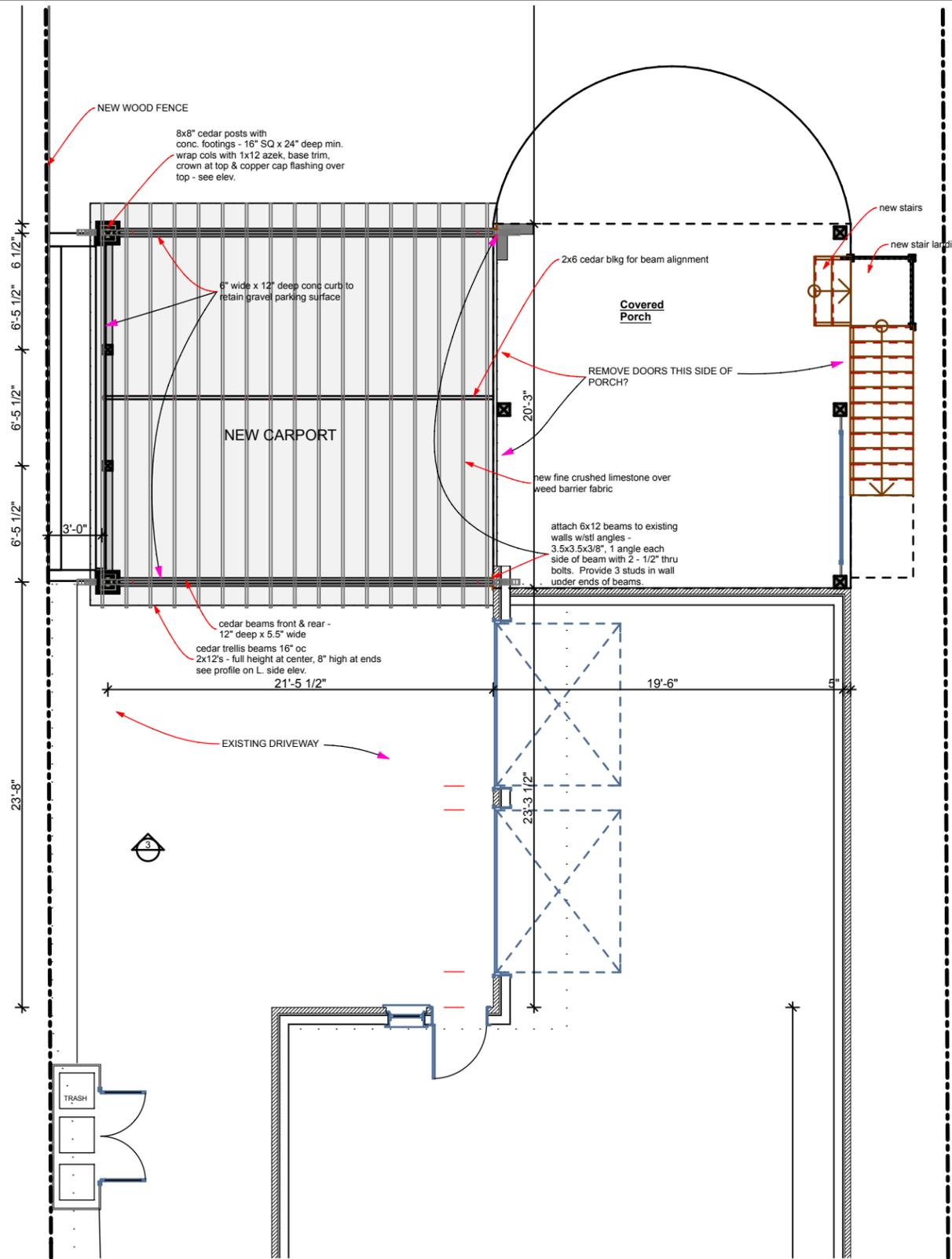
Rear and side yard



Rear and side yard



2 LATTICE ELEVATION, L SIDE
SCALE: 1/8" = 1'-0"



1 BASEMENT LEVEL PLAN
SCALE: 1/8" = 1'-0"

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2031 BERRY HILL DRIVE
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QUIRK DESIGNS

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H298-1508

Renovation Work
Phil & Allison Brooks
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DATE: 5/18/13
REVISION

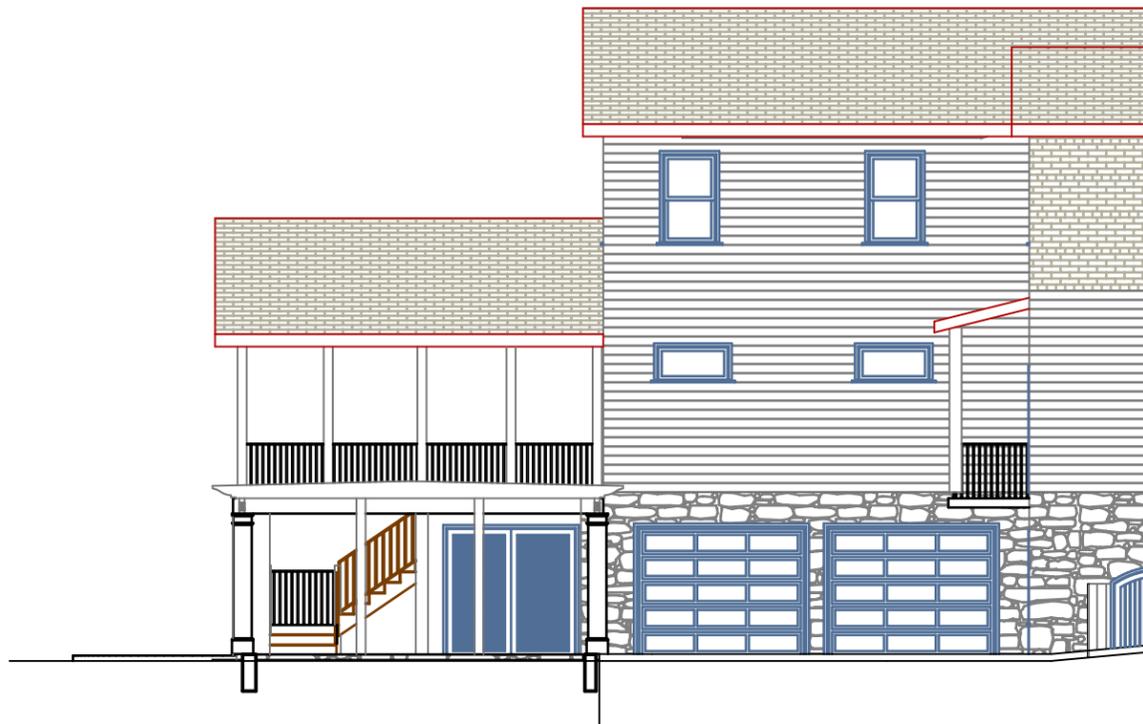
PROJECT NO: 13-007
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QUIRK DESIGNS

PLANS

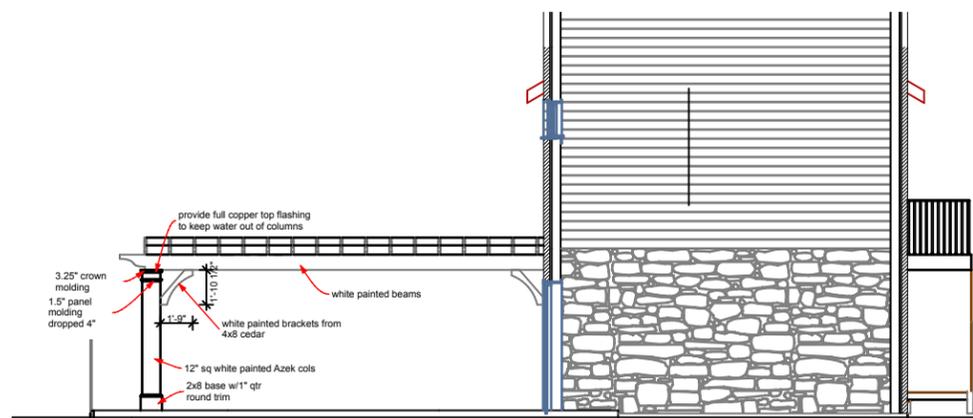
A2
SHEET 9



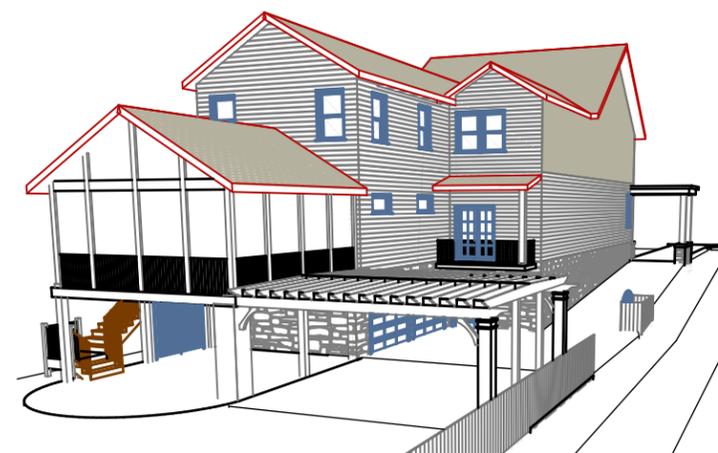
3 REAR ELEVATION
SCALE: 1" = 10'



2 LEFT ELEVATION
SCALE: 1" = 10'



1 FRONT ELEVATION
SCALE: 1" = 10'



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DATE: 5/18/13
REVISION

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QUIRK DESIGNS

ELEVATIONS

A3
SHEET 10