



**METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY**

Metropolitan Historic Zoning Commission  
Sunnyside in Sevier Park  
3000 Granny White Pike  
Nashville, Tennessee 37204  
Telephone: (615) 862-7970  
Fax: (615) 862-7974

**STAFF RECOMMENDATION**  
**2001 Russell Street**  
**April 16, 2014**

**Application:** Demolition—outbuilding; New construction—detached accessory dwelling unit

**District:** Lockeland Springs-East End Neighborhood Conservation Zoning Overlay

**Council District:** 06

**Map and Parcel Number:** 083140086.00

**Applicant:** Kim Kennedy, Bootstrap Architecture and Construction

**Project Lead:** Melissa Baldock, melissa.baldock@nashville.gov

**Description of Project:** The applicant proposes to build a Detached Accessory Dwelling Unit (DADU) at the rear of the property, behind a historic house.

**Recommendation Summary:** Staff recommends approval of the project with the following conditions:

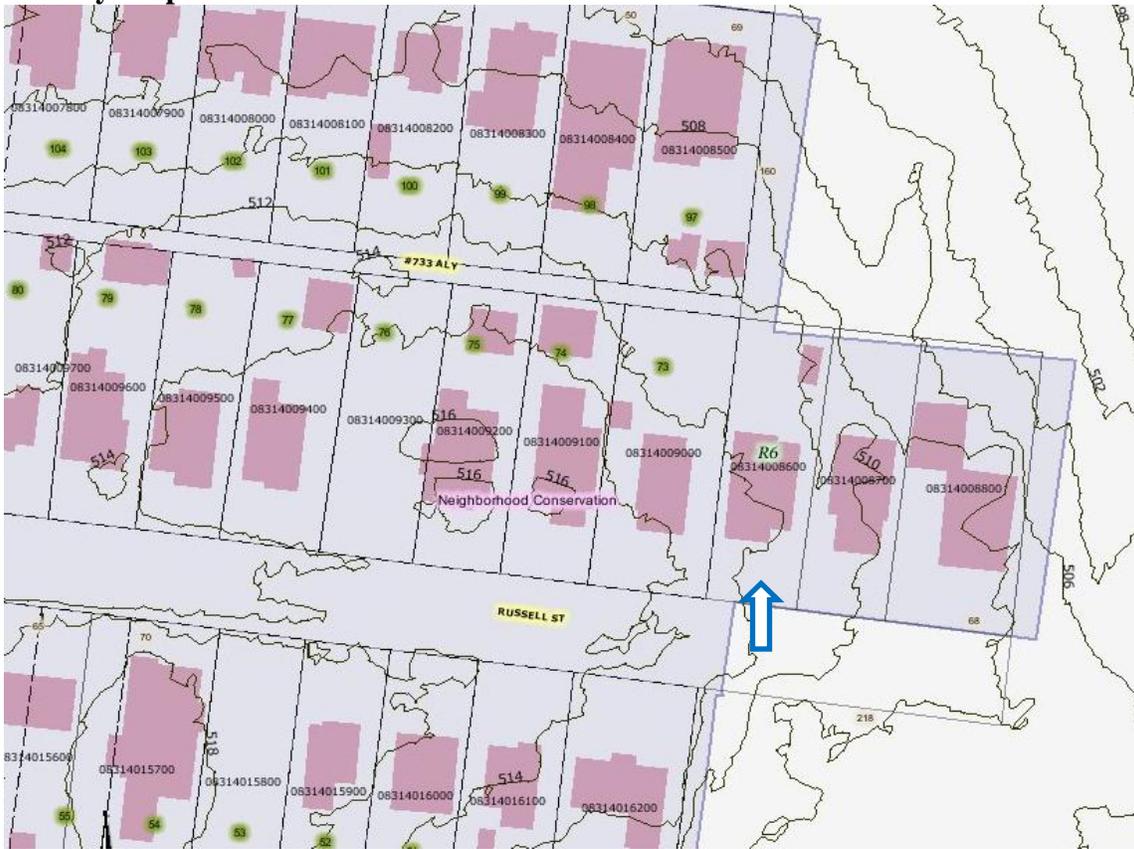
1. The owner file a restrictive covenant for the detached accessory dwelling unit; and,
2. Staff approve the window and door specifications and the asphalt shingle color prior to purchase and installation.

With these conditions, staff finds that the project meets Sections II.B. and IV.B. of the *Lockeland Springs-East End Neighborhood Conservation District: Handbook and Design Guidelines* and the standards for a detached accessory dwelling unit.

**Attachments**

- A:** Photographs
- B:** Site Plan
- C:** Elevations

**Vicinity Map:**



**Aerial Map:**



## Applicable Design Guidelines:

### II.B. New Construction

#### 8. Outbuildings

- a. Garages and storage buildings should reflect the character of the existing house and surrounding buildings and should be compatible in terms of height, scale, roof shape, materials, texture, and details.

*Historically, outbuildings were either very utilitarian in character, or (particularly with more extravagant houses) they repeated the roof forms and architectural details of the houses to which they related. Generally, either approach is appropriate for new outbuildings.*

##### *Outbuildings: Roof*

*Generally, the eaves and roof ridge of any new accessory structure should not be higher than those of the existing house.*

*Roof slopes on simple, utilitarian buildings do not have to match the roof slopes of the main structure, but must maintain at least a 4/12 pitch.*

*The front face of any street-facing dormer should sit back at least 2' from the wall of the floor below.*

##### *Outbuildings: Windows and Doors*

*Publicly visible windows should be appropriate to the style of the house.*

*Double-hung windows are generally twice as tall as they are wide and of the single-light sash variety.*

*Publicly visible pedestrian doors must either be appropriate for the style of house to which the outbuilding relates or be flat with no panels.*

*Metal overhead doors are acceptable on garages when they are simple and devoid of overly decorative elements typical on high-style wooden doors.*

*For street-facing facades, garages with more than one-bay should have multiple single doors rather than one large door to accommodate more than one bay.*

*Decorative raised panels on publicly visible garage doors are generally not appropriate.*

##### *Outbuildings: Siding and Trim*

*Brick, weatherboard, and board-and-batten are typical siding materials. Outbuildings with weatherboard siding typically have wide cornerboards and window and door casings (trim).*

*Exterior siding may match the existing contributing building's original siding; otherwise, siding should be wood or smooth cement-fiberboard lap siding with a maximum exposure of five inches (5"), wood or smooth cement-fiberboard board-and-batten or masonry.*

*Four inch (4" nominal) corner-boards are required at the face of each exposed corner.*

*Stud wall lumber and embossed wood grain are prohibited.*

*Four inch (4" nominal) casings are required around doors, windows, and vents within clapboard walls.*

*Trim should be thick enough to extend beyond the clapboard. Double or triple windows should have a 4" to 6" mullion in between.*

*Brick molding is required around doors, windows, and vents within masonry walls but is not appropriate on non-masonry clad buildings.*

- b. Garages, if visible from the street, should be situated on the lot as historically traditional for the neighborhood.

*Generally new garages should be placed close to the alley, at the rear of the lot, or in the original location of an historic accessory structure.*

*Lots without rear alleys may have garages located closer to the primary structure. The appropriate location is one that matches the neighborhood or can be documented by historic maps.*

*Generally, attached garages are not appropriate; however, instances where they may be are:*

*· Where they are a typical feature of the neighborhood; or*

*When the location of the attached garage is in the general location of an historic accessory building, the new garage is located in the basement level, and the vehicular access is on the rear elevation.*

- c. The location and design of outbuildings should not be visually disruptive to the character of the surrounding buildings.

#### **IV. B. Demolition**

##### **1. Demolition is not appropriate**

- a. if a building, or major portion of a building, is of such architectural or historical interest and value that its removal would be detrimental to the public interest; or
- b. if a building, or major portion of a building, is of such old or unusual or uncommon design and materials that it could not be reproduced or be reproduced without great difficulty and expense.

##### **2. Demolition is appropriate**

- a. if a building, or major portion of a building, has irretrievably lost its architectural and historical integrity and significance and its removal will result in a more historically appropriate visual effect on the district;
- b. if a building, or major portion of a building, does not contribute to the historical and architectural character and significance of the district and its removal will result in a more historically appropriate visual effect on the district; or
- c. if the denial of the demolition will result in an economic hardship on the applicant as determined by the MHZC in accordance with section 17.40.420 (Historic Zoning Regulations), Metropolitan Comprehensive Zoning Ordinance.

#### **17.16.030. F. Standards for Accessory Dwelling Units:**

An accessory dwelling unit should follow the design guidelines for the historic overlay and the following standards:

1. Lot Area. The lot area on which the detached accessory dwelling is to be placed shall comply with Table 17.12.020A.
2. Density. A detached accessory dwelling is not allowed if the maximum number of dwelling units permitted for the lot has been met.
3. Ownership.

No more than one detached accessory dwelling shall be permitted on a single lot in conjunction with the principal structure.

The detached accessory dwelling cannot be divided from the property ownership of the principal dwelling.

The detached accessory dwelling shall be owned by the same person as the principal structure and one of the two dwellings shall be owner-occupied.

4. Setbacks. The setbacks for a detached accessory dwelling shall meet the setbacks found in Section 17.12.040.E. for Accessory buildings.
5. Site Requirements.

A detached accessory dwelling may only be located in the established rear yard. The detached accessory dwelling is to be subordinate to the principal structure and therefore shall be placed to the rear of the lot.

There shall be a minimum separation of ten (10) feet between the principal structure and the detached accessory dwelling.

6. Driveway Access.

On lots with no alley access, the lot shall have no more than one curb-cut from a public street for driveway access to the principal structure as well as the detached accessory dwelling.

Parking accessed from a public street shall be limited to one driveway for the lot with a maximum width of 12 feet.

If the detached accessory dwelling is part of a garage and an alley exists to the rear of the lot, the garage shall be alley loaded and no curb-cut provided from the front of the lot.

7. Bulk and Massing.

No accessory structure shall exceed 200 square feet when there is a detached accessory dwelling on the lot.

The living space of a detached accessory dwelling shall not exceed 700 square feet.

The footprint single-story detached accessory dwelling shall not exceed 700 square feet or 50% of the first floor area of the principal structure, whichever is less.

The footprint of a two-story detached accessory dwelling shall not exceed 550 square feet or 40% of the first floor area of the principal structure, whichever is less.

*One partial-width, open porch, that is six feet deep or less, is not included in the footprint calculation.*

The detached accessory dwelling shall maintain a proportional mass, size, and height to ensure it is not taller than the principal structure on the lot. The detached accessory dwelling height shall not exceed the height of the principal structure as measured to the eave line, with a maximum eave height of 10 feet for single-story and 17 feet for two-story detached accessory dwellings.

The roof ridge line of the detached accessory dwelling must be less than the primary structure and shall not exceed 25 feet in height.

8. Design Standards.

Detached accessory dwellings with a second story dwelling unit shall enclose the stairs interior to the structure and properly fire rated per the applicable life safety standards found in the code editions adopted by the Metropolitan Government of Nashville.

The detached accessory dwelling shall be of similar style, design and material color as used for the principal structure and shall use similar architectural characteristics, including roof form and pitch, to the existing principal structure.

The detached accessory dwelling may have dormers that relate to the style and proportion of windows on the detached accessory dwelling and shall be subordinate to the roof slope by covering no more than 50% of the roof.

Detached accessory dwellings may have dormers that are setback a minimum of two feet from the exterior wall.

**Background:** 2001 Russell Street is a c. 1930 brick bungalow which contributes to the historic character of the Lockeland Springs-East End Neighborhood Conservation Zoning Overlay. MHZC staff recently approved administratively an application for a rear dormer and addition to the historic house.



**Analysis and Findings:** The applicant proposes to build a Detached Accessory Dwelling Unit (DADU) at the rear of the property, behind a historic house.

Demolition: As part of the application, the applicant is proposing to demolish a non-contributing accessory structure (Figure 2). Staff finds that the accessory structure lacks historic and architectural interest, and that it does not contribute to the historic character of the site or of the historic Lockeland Springs neighborhoods. Staff therefore finds that its demolition meets Section IV.B.2. and does not meet Section IV.B.1. of the design guidelines.



Figure 2. The outbuilding to be demolished.

Lot Area. 2001 Russell Street is zoned R6 and has a lot that is approximately seven thousand, nine hundred square feet (7,900 sq. ft.). The foot print of the primary structure is approximately two thousand and seventy-five square feet (2,075 sq. ft.), and the proposed DADU will have a footprint of six hundred and ninety-four square feet (694 sq. ft.). The total lot coverage is approximately two thousand, seven hundred and sixty-nine square feet (2,769 sq. ft.), or about thirty-five percent (35%), which is less than the maximum lot coverage of fifty percent (50%). The lot area therefore meets Standard 1 of the Detached Accessory Dwelling Unit regulations.

Density. The R6 zone allows for two dwelling units. Currently, there is one dwelling unit on the site in the primary structure. Adding the proposed accessory structure would result in two dwelling units, which meets the zoning. Therefore the density of the site meets Standard 2 of the Detached Accessory Dwelling Unit regulations.

Ownership. Staff has asked the owner of the property to file a restrictive covenant stating that s/he lives in the principle building or plans to live in the detached accessory dwelling unit and acknowledging that s/he cannot separate ownership of the primary structure and the detached accessory dwelling unit. To date, staff has not received a copy of the covenant, and staff asks that a condition of approval be that the owner file a restrictive covenant for a detached accessory dwelling unit and forward a copy on to the MHZC staff prior to the issuance of the permit.

Setbacks. The proposed outbuilding meets all base zoning setbacks. It is five feet (5') from the rear property line, five feet (5') from west property line, and more than twenty feet (20') from the east property line. The structure therefore meets Standard 4 of the Detached Accessory Dwelling Unit regulations.

Site Requirements. The detached accessory dwelling unit is located in the rear yard of the primary structure at 2001 Russell Street (Figure 3). There will be approximately thirty-one feet (31') between the primary residence and the accessory structure. The structure therefore meets Standard 5 of the Detached Accessory Dwelling Unit regulations.



Figure 3. The rear yard of 2001 Russell street, where the DADU will be located.

Driveway Access. The vehicular access to the garage portion of the structure will be via an existing driveway on Russell Street. Although an alley is mapped behind this site, the paved, serviceable alley in fact stops before the property line for 2001 Russell Street (Figure 4). Therefore access via an existing front driveway is appropriate and meets Standard 6 of the Detached Accessory Dwelling Unit regulations.



Figure 4. The paved, serviceable alley stops prior to the property line of 2001 Russell Street.

Bulk and Massing. The accessory structure is one-and-a-half stories tall. Its footprint is approximately six hundred and ninety-four square feet (694 sq. ft), less than the maximum footprint of seven hundred square feet (700 sq. ft.) for one-story structures. The ridge height is approximately twenty-one feet (21') from grade and the eave height is

ten feet (10'). These are both subordinate to the historic house, which has an eave height of eleven feet (11') and a ridge height of twenty-four feet (24'). The structure meets Standard 7 of the Detached Accessory Dwelling Unit regulations.

Design Standards. The outbuilding's design is compatible with the style of the historic structure. Its roof, detailing, and overall form do not contrast greatly with the primary structure. The structure's stairs are located on the interior. Its roof form includes shed dormers on the north and south elevations. The dormers sit back two feet (2') from the wall below them, and they occupy less than fifty percent (50%) of the roof. The fenestration pattern is appropriate for an outbuilding, and the structure is in a minimally-visible location at the rear of the historic structure. The proposed outbuilding therefore meets Standard 8 of the Detached Accessory Dwelling Unit regulations and II.B.1.8. of the design guidelines.

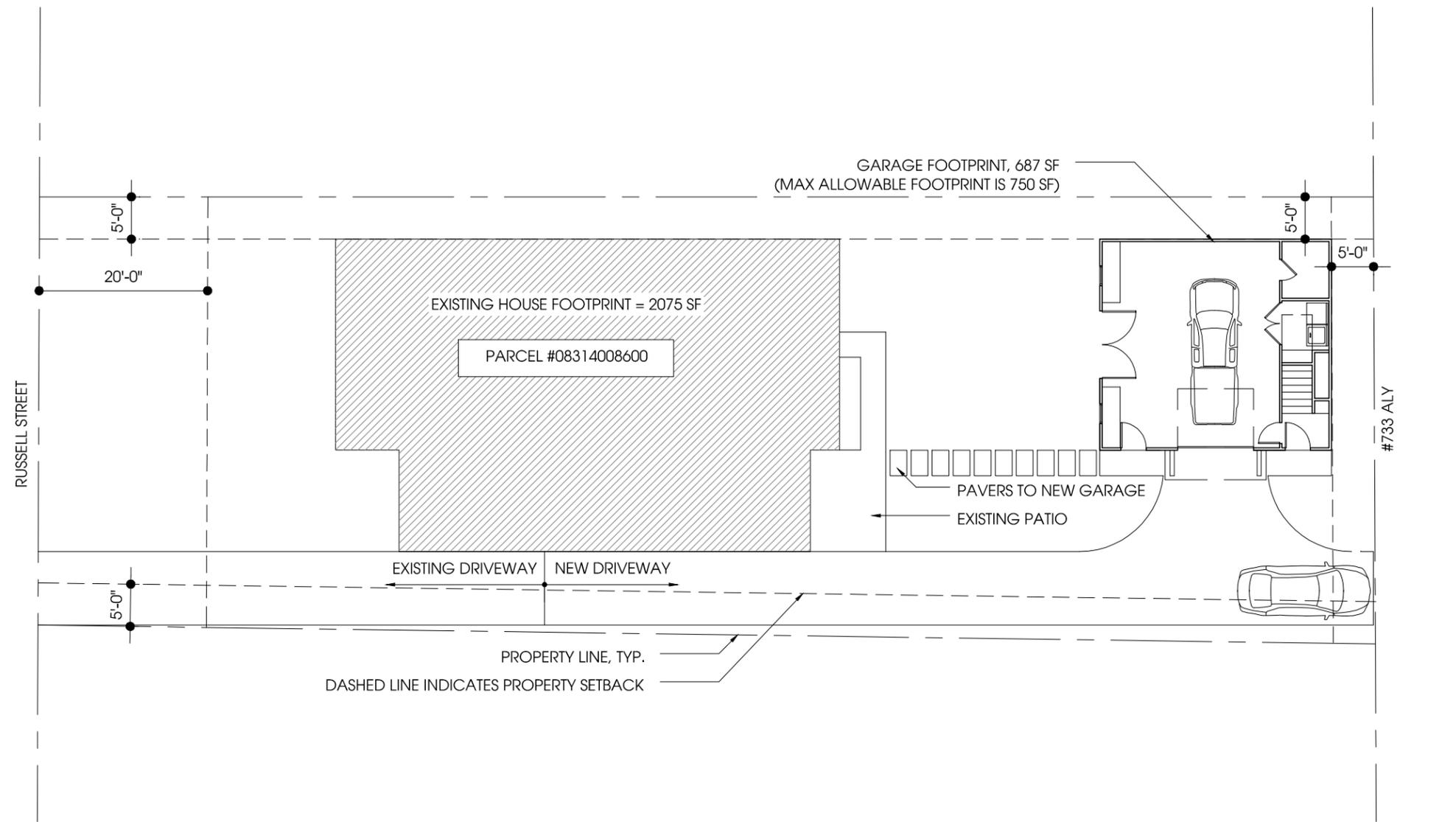
Materials: The foundation is a concrete slab. Primary cladding material will be fiber cement siding with a five inch (5") reveal. In the gable fields, fiber cement smooth panels and battens will be used. Wood brackets will be incorporated to match those on the house. The second story balcony rail will be metal. The windows will be fiberglass or aluminum clad, and staff asks to approve all window and door specifications prior to purchase and installation. The roof will be asphalt shingle, and staff asks to approve the shingle color. With the aforementioned staff approvals, staff finds that the materials meet section II.B.8. of the design guidelines.

Roof form: The outbuilding will have a side-gabled roof with clipped gables, matching the roof form of the historic house. The gable will have a pitch of 8/12. The shed dormers will have a pitch of 3/12. Staff finds that the outbuilding's roof form meets section II.B.8. of the design guidelines.

**Recommendation Summary:** Staff recommends approval of the project with the following conditions:

1. The owner file a restrictive covenant for the detached accessory dwelling unit; and,
2. Staff approve the window and door specifications and the asphalt shingle color prior to purchase and installation.

With these conditions, staff finds that the project meets Sections II.B. and IV.B. of the *Lockeland Springs-East End Neighborhood Conservation District: Handbook and Design Guidelines* and the standards for a detached accessory dwelling unit.



### SQUARE FOOTAGE CALCULATIONS

FIRST FLOOR GARAGE	617 S.F.
FIRST FLOOR DWELLING UNIT	70 S.F.
SECOND FLOOR DWELLING UNIT	506 S.F.
TOTAL SQUARE FOOTAGE	1193 S.F.
BALCONY	42 S.F.

### GENERAL DESCRIPTION

**PROJECT NAME:** RUSSELL STREET GARAGE  
**PROJECT LOCATION:** 2001 RUSSELL STREET  
 NASHVILLE, TN 37206  
 PARCEL ID# 08314008600  
**PROJECT SUMMARY:** A NEW ONE-CAR DETACHED GARAGE WITH ACCESSORY DWELLING UNIT IS PROPOSED FOR THE RESIDENCE AT 2001 RUSSELL STREET. THE MATERIALS ARE FIBERCEMENT SIDING, 5" EXPOSURE, FIBERCEMENT BOARD & BATTEN PANELS AND ASPHALT SHINGLE ROOFING.  
**ZONING:** R-6  
 PROPERTY IS IN NEIGHBORHOOD CONSERVATION OVERLAY  
 PROPERTY IS IN URBAN ZONING OVERLAY  
**APPLICABLE CODES:** 2006 INTERNATIONAL RESIDENTIAL CODE

### VICINITY MAP



SITE

**bootstrap**  
 architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

(615) 715-4164  
 KIM KENNEDY

**RUSSELL STREET GARAGE**

2001 RUSSELL STREET  
 NASHVILLE, TENNESSEE 37206

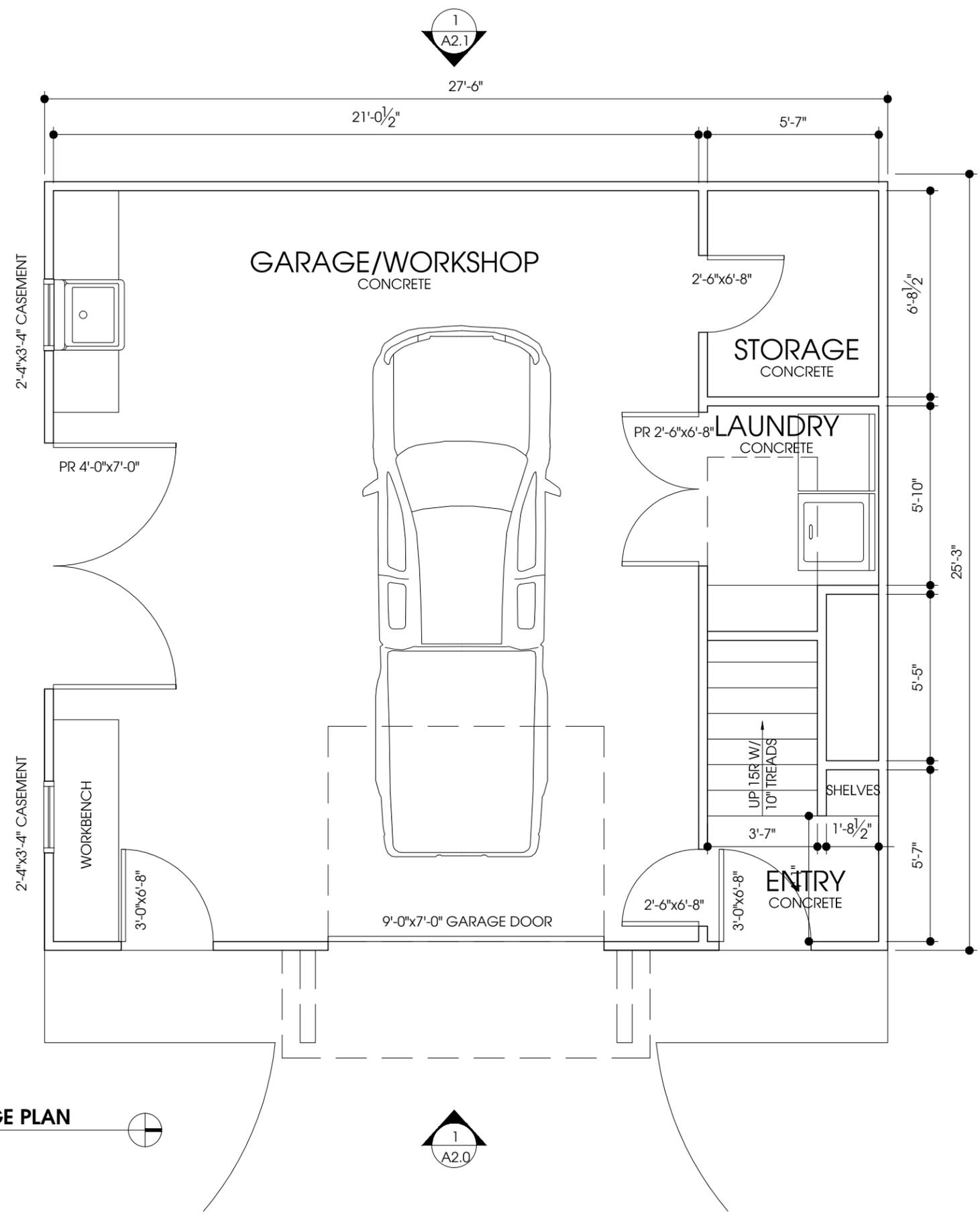
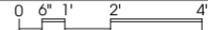
**PRESERVATION PERMIT**

● 29 MARCH 2014



**A0.1**

**1 FIRST FLOOR GARAGE PLAN**



**bootstrap**  
architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

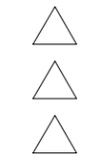
(615) 715-4164  
KIM KENNEDY

**RUSSELL STREET GARAGE**

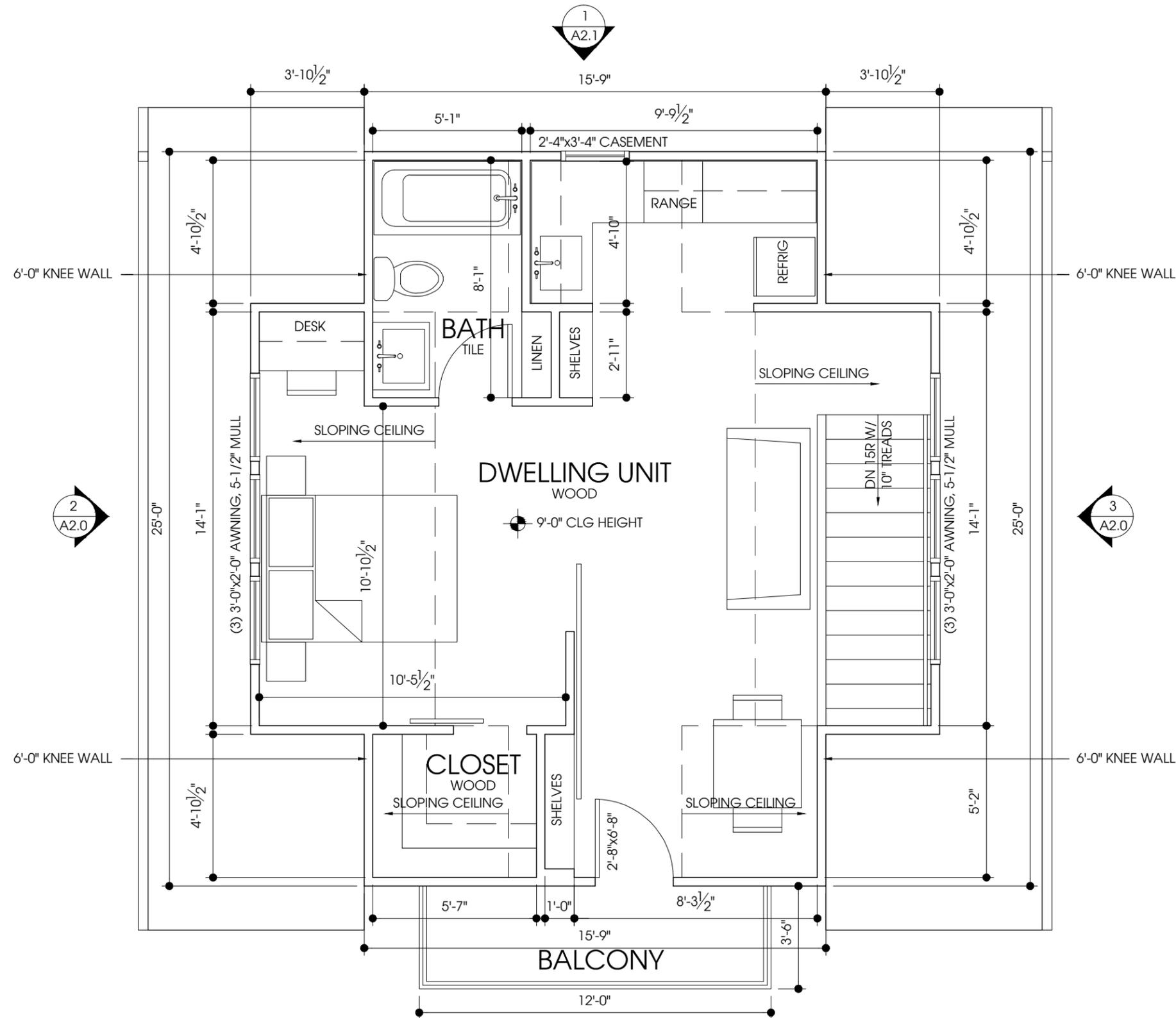
2001 RUSSELL STREET  
NASHVILLE, TENNESSEE 37206

**PRESERVATION PERMIT**

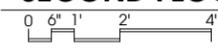
● 29 MARCH 2014



**A1.0**



**1 SECOND FLOOR GARAGE PLAN**



**bootstrap**  
architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

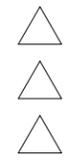
(615) 715-4164  
KIM KENNEDY

**RUSSELL STREET GARAGE**

2001 RUSSELL STREET  
NASHVILLE, TENNESSEE 37206

**PRESERVATION PERMIT**

● 29 MARCH 2014



**A1.1**

**bootstrap**  
architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

(615) 715-4164  
KIM KENNEDY

**RUSSELL STREET GARAGE**

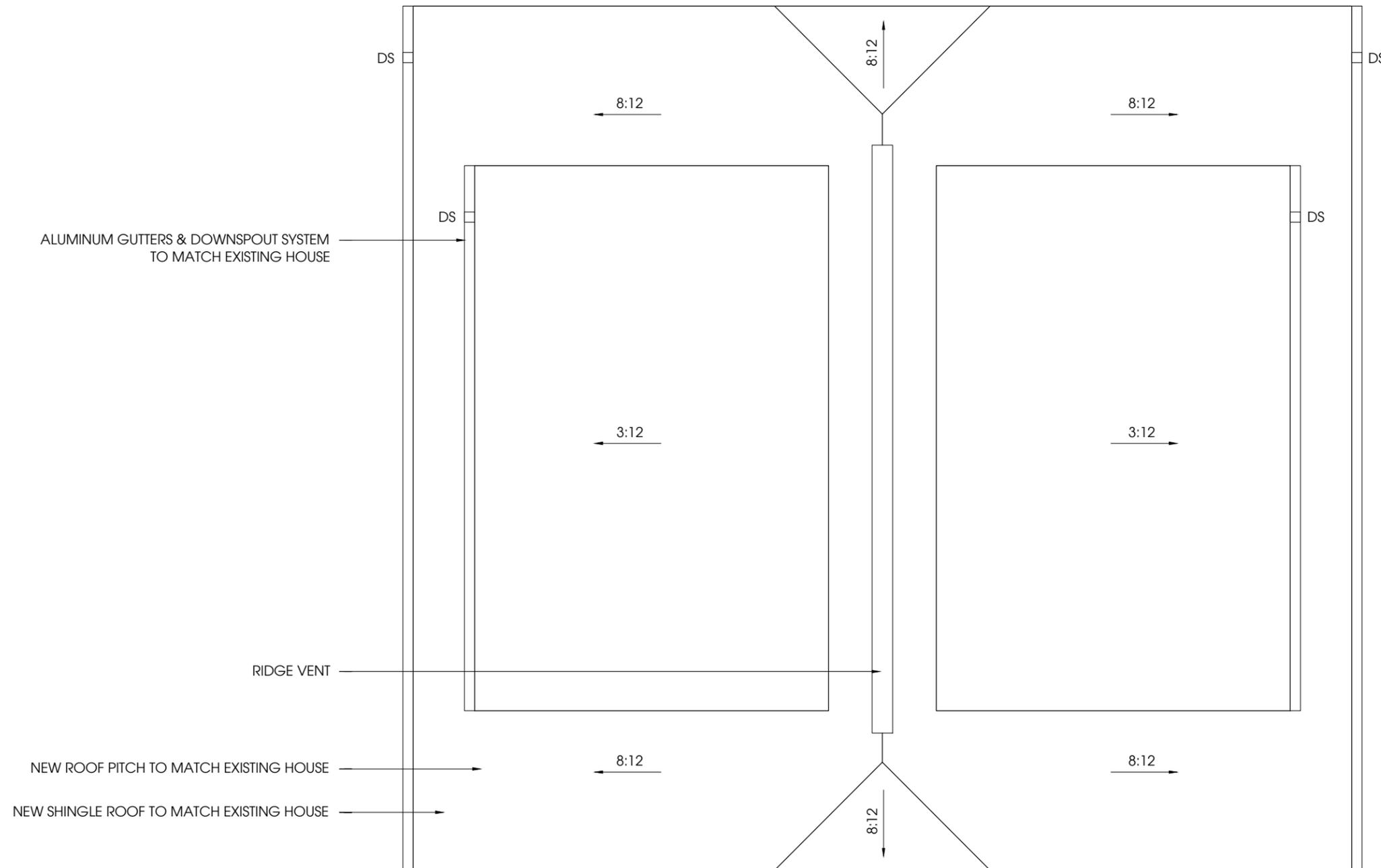
2001 RUSSELL STREET  
NASHVILLE, TENNESSEE 37206

**PRESERVATION PERMIT**

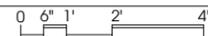
● 29 MARCH 2014



**A1.2**



**1 GARAGE ROOF PLAN**



**bootstrap**  
architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

(615) 715-4164  
KIM KENNEDY

**RUSSELL STREET GARAGE**

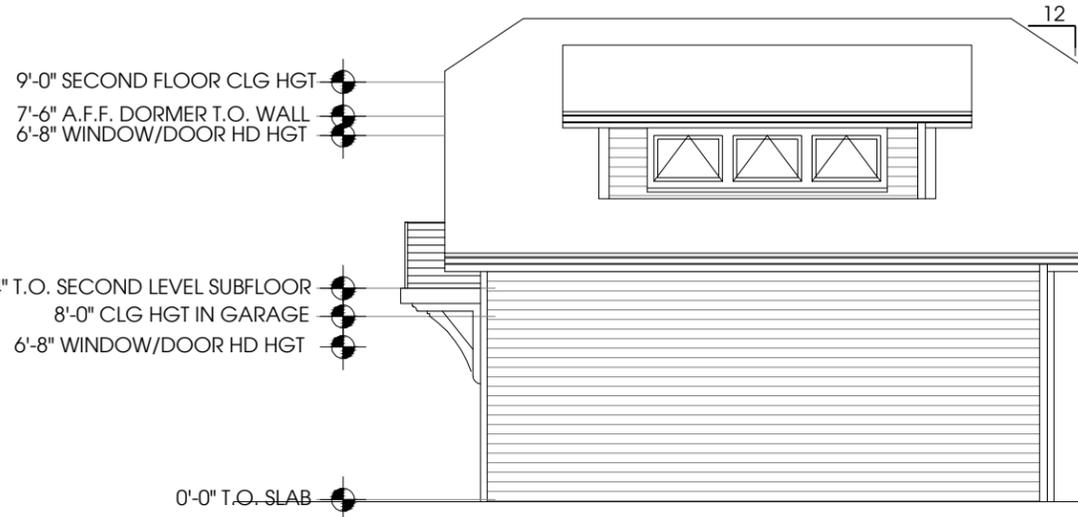
2001 RUSSELL STREET  
NASHVILLE, TENNESSEE 37206

**PRESERVATION PERMIT**

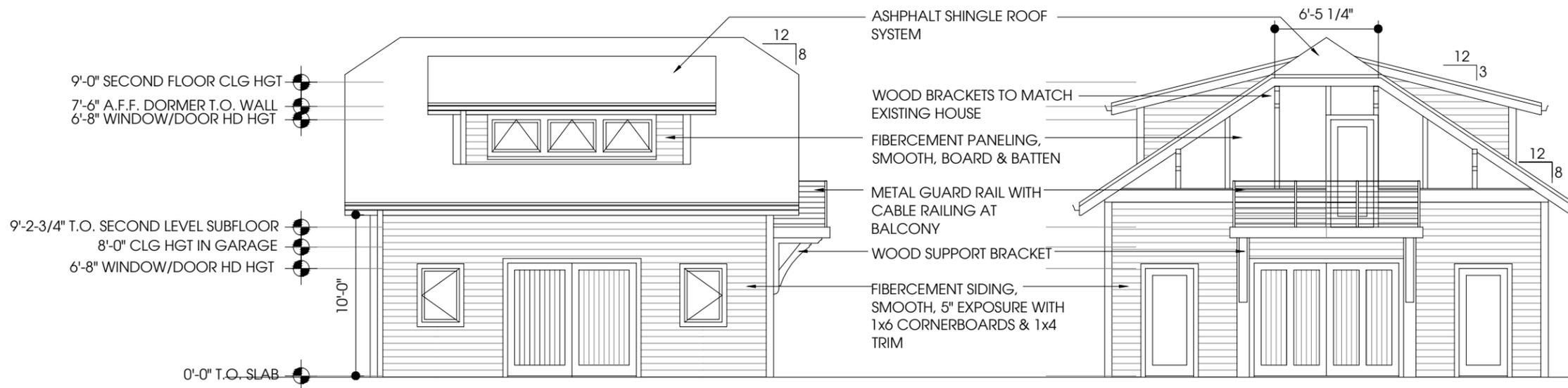
● 29 MARCH 2014



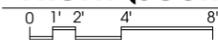
**A2.0**



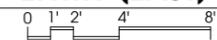
**3 REAR (NORTH) ELEVATION**



**2 FRONT (SOUTH) ELEVATION**



**1 ENTRY (EAST) ELEVATION**



NOTE:  
ALL DOORS AND WINDOWS TO BE FIBERGLASS  
OR ALUMINUM CLAD (WOOD INTERIOR)

**bootstrap**  
architecture + construction

1432 GREENWOOD AVENUE  
**NASHVILLE, TN 37206**

(615) 715-4164  
KIM KENNEDY

**RUSSELL STREET GARAGE**

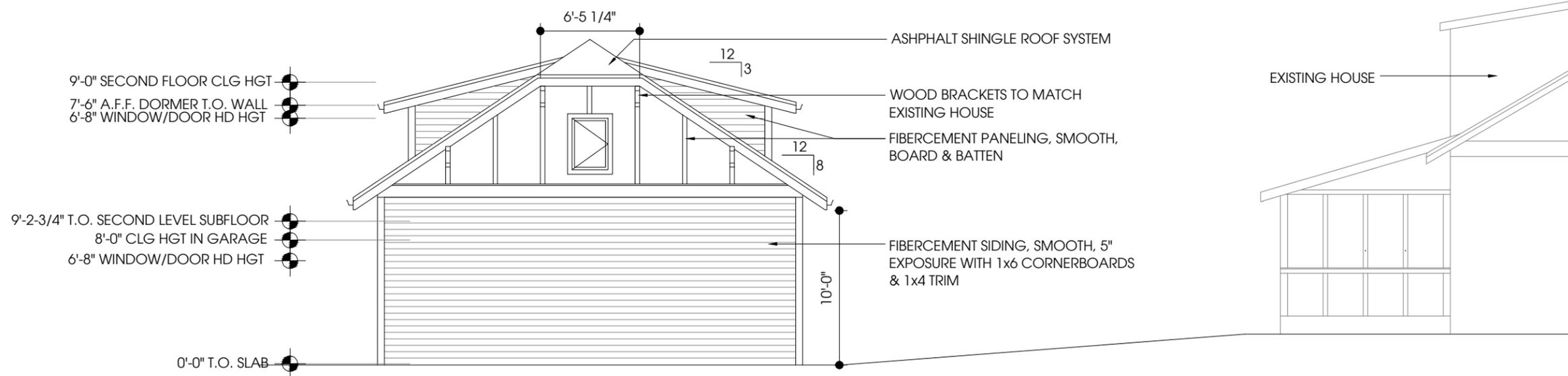
2001 RUSSELL STREET  
NASHVILLE, TENNESSEE 37206

**PRESERVATION PERMIT**

● 29 MARCH 2014



**A2.1**



**1 SIDE (WEST) ELEVATION**

