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PARKWOOD - UNION HILL COMMUNITY PLAN APRIL REVIEW DRAFT

This is the review draft of the Parkwood - Union Hill Community Plan of NashvilleNext. It is part of Volume III (Communities) of the draft General Plan.

We appreciate that you are giving time to reviewing this work. This chapter is the result of three years of effort on NashvilleNext, combining public visioning and community engagement with guidance from topical experts to create a plan for Nashville and Davidson County over the next 25 years.

Comments

The public review period is during April, 2015. We are eager to hear your thoughts on the plan. Here's how to provide input:

- » Online: www.NashvilleNext.net
- » Email: info@nashvillenext.net
- » At public meetings
 - » April 18: Tennessee State University (Downtown Campus), 10am - 1:30 pm
 - » April 20: 5 - 7pm at both the North Nashville Police Precinct and the Edmondson Pike Branch Library
 - » April 27: 5 - 7pm at both the Madison Police Precinct and the Bellevue Branch Library
- » Phone: 615-862-NEXT (615-862-6398)
- » Mail: Metro Nashville Planning Department, P.O. Box 196300, Nashville TN 37219-6300

We ask that you include contact information with your comments. We also request that you be as specific as possible in your requests. Referring to a specific page or section is greatly appreciated.

Next steps

The most up to date information is always available at www.NashvilleNext.net. Here is our tentative adoption schedule:

- » **Mid-May:** Post static draft of plan in advance of public hearing
- » **June 10:** First public hearing at Planning Commission (tentative; special date)
- » **June 15:** Second public hearing at Planning Commission (tentative; special date)

PARKWOOD-UNION HILL

chapter one

Description of the Parkwood-Union Hill Community

The Parkwood – Union Hill Community extends from the junction of I-24 and I-65 north to the Robertson County line. It is bordered on the east by I-65, Goodlettsville, and Sumner County; it is bordered on the west by I-24, Crocker Springs Road and Ivey Point Road. Parkwood -Union Hill is one of Nashville’s quietest communities, with less change over the past 15 years than most of Davidson County.

The southern part of the community, Parkwood, is primarily suburban. Union Hill is rural and rural residential, with hills, large wooded areas, and farmland. Its distance from Downtown Nashville, its adjacency to rural counties, and its concentration of hilly terrain, floodplain and farmland create its split suburban and rural character.

Major Neighborhoods/Communities

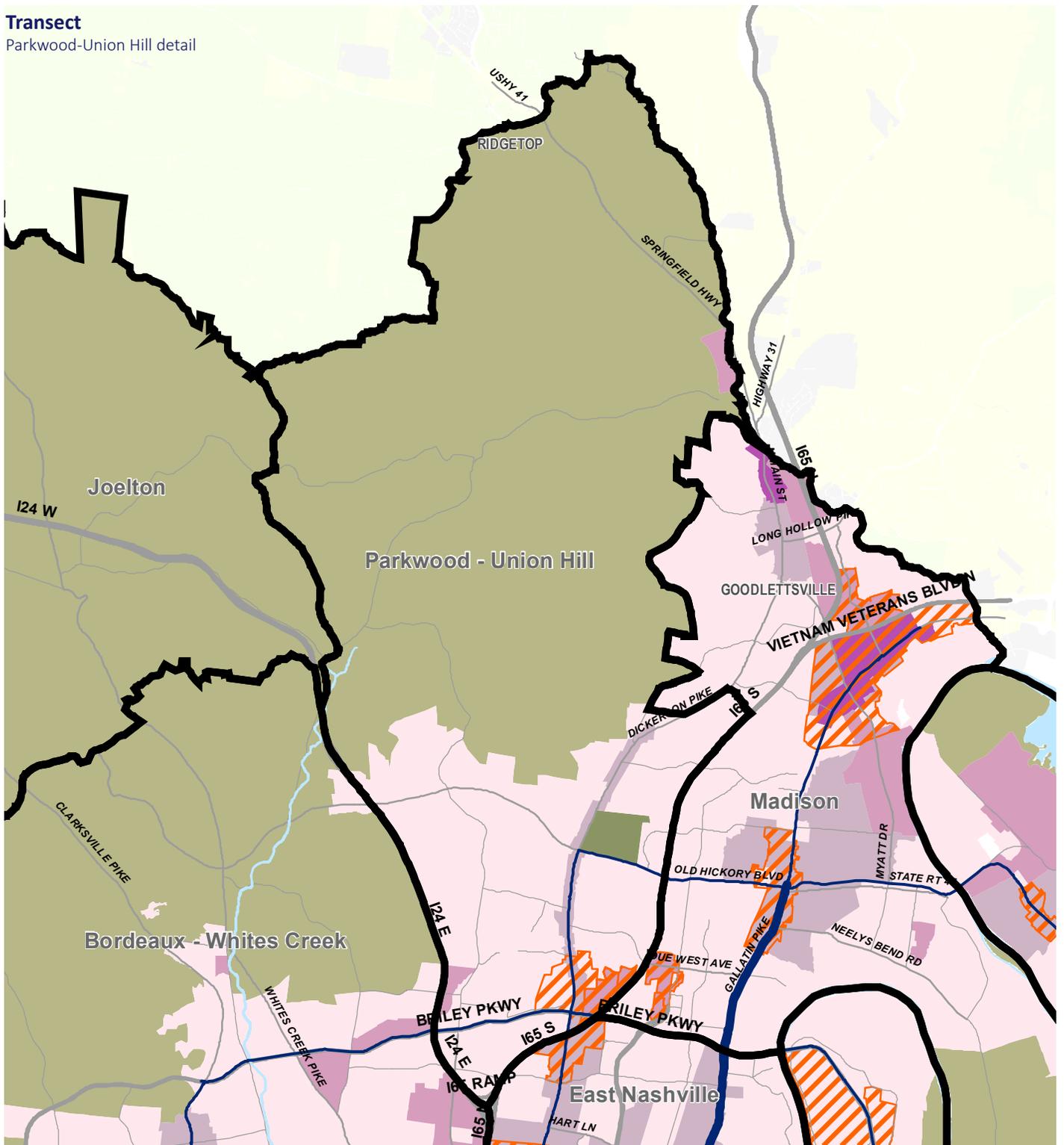
Parkwood and the southern suburbs

The southern part of Parkwood/Union Hill is bordered by Campbell Road to the north. It is bordered by I-65 to the East, and I-24 and Crocker Springs Road to the west. Parkwood has about one-fourth of the community’s land area and is mostly developed. This southern area contains both urban and suburban characteristics. The southern area has more extensive residential and commercial development compared to the northern area for several reasons. Access to major roads such as Briley Parkway, I-65, and I-24 make the Parkwood area more attractive for development. The southern area also has services like water and sewer available, due in part to its flatter terrain.

Older, established neighborhoods like Parkwood and Bellshire are mixed with newer suburban development, like Belle Arbor, Chesapeake, and Timberwood. Newer developments are close to commercial services on Old Hickory Boulevard and Dickerson Pike. These newer developments include Mulberry Downs and Quail Ridge. Single family subdivisions, townhomes and garden apartments are common. The southern area has most of the community’s businesses, civic activities, and public services.

Transect

Parkwood-Union Hill detail

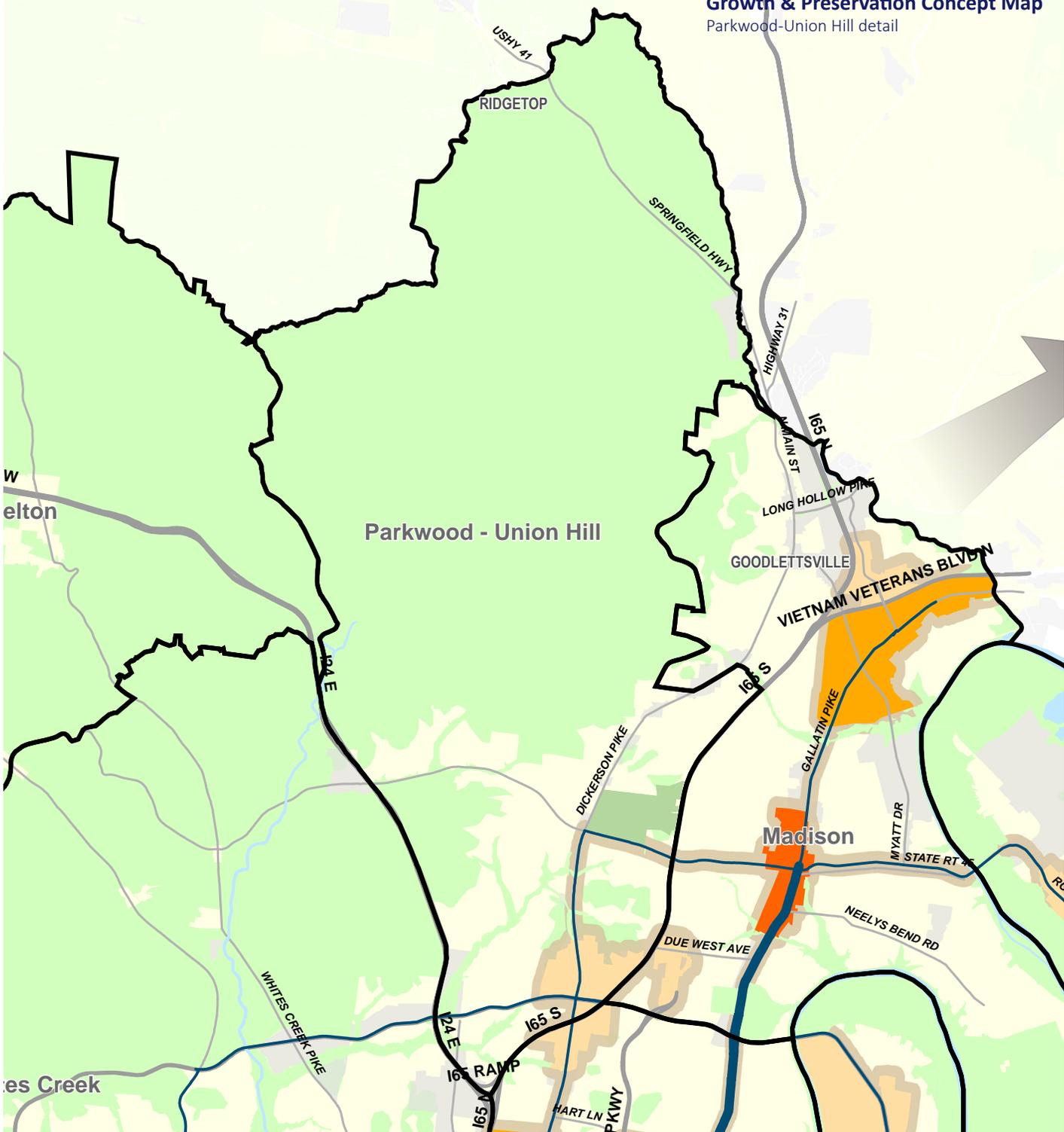


Transects

- | | |
|--|---|
|  T1 Natural |  T5 Center |
|  T2 Rural |  T6 Downtown |
|  T3 Suburban |  D District |
|  T4 Urban |  W Water |



Growth & Preservation Concept Map
Parkwood-Union Hill detail



- Centers**
- Tier One
- Tier Two
- Tier Three
- Green network**
- Open space anchor
- Missing an anchor
- Neighborhood**
- Transition**
- Special impact area**
- High capacity transit corridors**
- Immediate need
- Long-term need
- Regional connection**

Union Hill

The northern area of Parkwood/Union Hill is Union Hill. It is bordered by Campbell Road and Hunter's Lane to the south, Goodlettsville and the county line to the east and north, and Cocker Springs Road to the west. Union Hill has a predominately rural character. Large farms and open land are prominent. This area has retained its rural character and has experienced limited development due to steep slopes, the lack of sewer services, and effects of current zoning and land use policies. These factors have contributed to a slow pace of development for this area of the county.

Dickerson Pike Corridor

The most important corridor in the community is Dickerson Pike. Dickerson Pike winds along the area's eastern border north to Goodlettsville. Dickerson Pike is a major thoroughfare and the most significant provider of services to the community. Because of this corridor's importance, its development is a major focus of this plan. A key factor in the character of development along Dickerson Pike is Commercial Service (CS) zoning. This zoning is nearly continuous from the I-65 interchange to Dry Creek Road.

Dickerson Pike is a major transportation corridor. It is also the main service center in the community. Dickerson Pike has the community's largest concentration of nonresidential development, mostly south of Old Hickory Boulevard. The biggest development is the Skyline area near Dickerson Pike and Briley Parkway (see below). Dickerson Pike also has neighborhood-scale commercial businesses, light industry, and civic uses. There is also a significant amount of undeveloped land, particularly north of Old Hickory Boulevard.

Dickerson Pike has most of the commercial or light industrial development in Parkwood/Union Hill. There is also non-residential development in three other areas. One area is near I-24 and Old Hickory Boulevard. Another is along I-24 near Briley Parkway and Ewing Drive. The third is on Springfield Highway north of Goodlettsville. The corridor consists of mainly strip commercial centers south of Old Hickory Boulevard. There are a mix of land uses north of Old Hickory Boulevard. These uses include parks and recreation, vacant, residential, industrial, and commercial land uses.

Skyline

The commercial and medical center at Skyline has seen the greatest change in Parkwood-Union Hill over the last two decades. At the southern end of the community, Skyline is at the intersection of I-65, Briley Parkway, and Dickerson Pike. Beginning with the Skyline Medical Center in 2000 and followed by the build-out of Skyline Commons, the Skyline area has substantially added to the community's employment and retail base. With more than 700,000 square feet of commercial space in Skyline Commons, the area is a major regional destination.

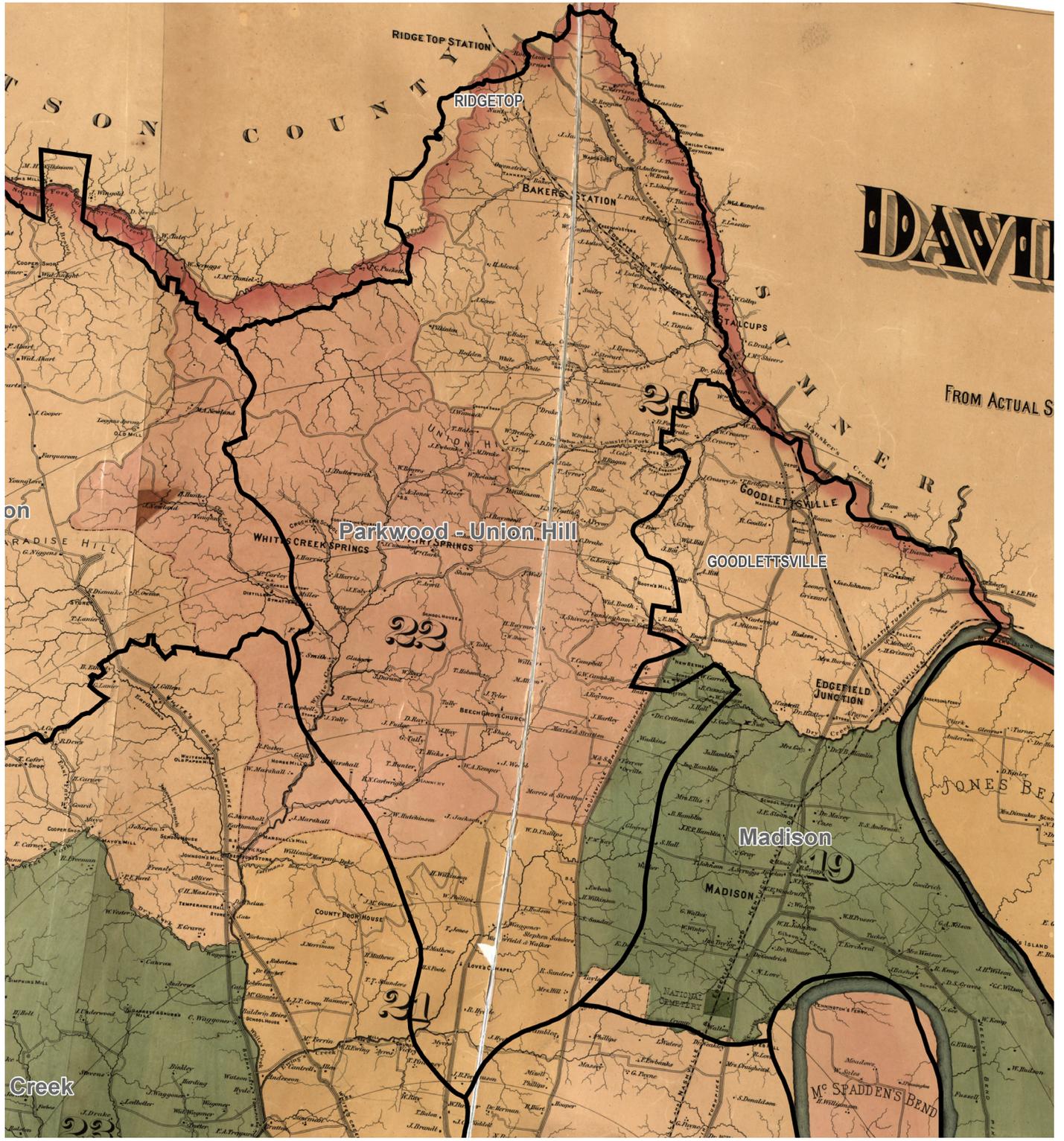
The major developments in Skyline are auto-oriented. Recently, smaller developments have begun to fill in around Skyline Commons and the Medical Center, but terrain has limited how walkable they are.

Parkwood-Union Hill Community History

Parkwood-Union Hill is a short drive out of downtown. Traveling through the community is a trip through Nashville's character, from the intensive office and retail development at Skyline. Continue traveling along Brick Church Pike and you will see modest suburban homes. Farther on, the terrain becomes hilly and the area takes on a rural character. The distance from the new Wal-Mart at the south to Galbreath's Shop-Rite covers not just the span of character areas in Nashville, but also its history.

The majority of historically significant sites in Parkwood-Union Hill are scattered throughout the northern portion area of the community. Much of the history of the North Study Area is not well documented, but found in the rolling hills, farm landscape, and sense of traditions that characterize the smaller communities within the area. The Abner T. Shaw House is the only property in the community that is listed on the National Register of Historic Places. Built before the Civil War in the 1850s, the house and its outbuildings represent an intact rural farm and the earliest known use of concrete construction in Davidson County. Two features in the North Study Area have been designated National Register Eligible – the Butterworth House at 5387 Lickton Pike and the Williamson House at 1151 Springfield Hwy. Several other sites have been deemed Worthy of Conservation (WOC) by the Metro Historical Commission. The former Union Hill School, which is located at 1538 Union Hill Road, is one such community

1871 map Davidson County
Parkwood-Union Hill detail



asset. Originally built in 1940-41 to replace an earlier school that burned, the existing school structure retains a great deal of exterior integrity and includes an unusual Art Deco-detailed entry.

The South Study Area southern area of the community has had more widespread recent development and, as a result, has fewer historic sites. A number of several older and more conventional neighborhoods were built in the late 1950s and early 60s. Historically significant features associated with these areas may have been lost as development occurred. The Jackson House, located at 3500 Brick Church Pike, is the only site in South Study Area the southern area that has been designated National Register Eligible (NRE) by the Historical Commission.

Late twentieth century development has only intruded into Parkwood Union Hill occasionally. When Rivergate Mall opened in 1971, Goodlettsville boomed, with a surge of new residential and commercial development. The slopes of Union Hill limited the expansion of new homes into the community, though some commercial and residential development crept down Dickerson Pike.

Following the early subdivisions in Parkwood, Bellshire, and Dalemere between Dickerson and Brick Church Pikes, and subdivisions like Sherry Heights, Kemper Heights, Ridgemont Estates, and Pleasant Hill along Dickerson Pike, development of new housing slowed. New subdivisions were added slowly from the 1970s through today. Many of the area's neighborhoods organized to oppose new developments, seeking to keep traffic low and rural areas undisturbed.

Since 2000, however, the Skyline area at I-65, Briley Parkway, and Dickerson Pike has boomed. Starting with Skyline Medical and followed by development of Skyline Commons, the area continues to attract development interest.

For the most current information on Nashville's historic properties and resources, contact the Metro Historical Commission

<http://www.nashville.gov/Historical-Commission.aspx>

History of the Planning Process

In 1988, the Metro Planning Department began creating “community plans” as a means of fine tuning the countywide general plan. These community plans examined specific issues and needs, projected growth, development and preservation in fourteen communities. The Parkwood-Union Hill Community Plan was first adopted by the Planning Commission in June 1995, after working with a Citizens’ Advisory Committee composed of residents nominated by the Mayor, the three Councilmembers represented in the community, the Nashville Neighborhood Alliance, and the Nashville Area Chamber of Commerce.

The Parkwood-Union Hill Plan’s first update was completed in 2006. The update process included 9 community meetings attended by more than 90 community members. Dickerson Pike DNDP.

In 2008, the Metro Planning Department reorganized its approach to community plans. The guiding document for plans prior to 2008, the Land Use Policy Application, delineated land uses and density characteristics, but was unable to capture nuances in the character of different parts of the county. These nuances were often central to residents’ concerns about new development. In key areas, they were addressed through Detailed Neighborhood Design Plans, such as the one for Dickerson Pike included in the 2006 plan update. The new approach focused on the current and proposed character of different parts of the county and lessened the focus on density. It was based on a new tool called the Community Character Manual, adopted by Planning Commission in 2008.

In 2013, as part of NashvilleNext, Parkwood-Union Hill’s Community Structure Plan (created under the older Land Use Policy Application process) was updated to the newer Community Character Policies. Three review workshops were held in March 2013; an online map of the prior and proposed policies was also available for review. The revised map was adopted by Planning Commission in June of that year. Subsequent refinements to the map during the development of NashvilleNext’s Growth and Preservation Concept Map were reviewed at public meetings and online.

This update of the Parkwood-Union Hill Community Plan reflects the values and vision of the numerous participants in the 2006 update planning process as well as participation in the NashvilleNext planning process, balanced with sound planning principles to achieve a realistic, long-term plan for sustainable growth, development and preservation.

Community Demographic Information

Parkwood and Union Hill have grown modestly since the 1990s, at a lower pace than the rest of Davidson County and slower still since 2000. From 1990 to 2010, Parkwood-Union Hill grew by 2,000, from 16,351 to 18,420 residents. Parkwood-Union Hill accounts for about 3% of Davidson County's population.

Since 1990, new housing has been built through small subdivisions scattered throughout Parkwood-Union Hill: near existing subdivisions and close to Skyline, Dickerson Pike, and Old Hickory Boulevard.

Slightly more than half of residents are black; slightly under half are white. That balance represents a shift from 2000, when the black population accounted for 46% of the population. Parkwood-Union Hill has fewer working age adults, with more children under 18 and more seniors over 64 than the County as a whole.

Economically, Parkwood-Union Hill has a lower per capita income and lower educational attainment than the county, but a higher homeownership rate. Residents work throughout the county, with most commuting to South Nashville, Madison, or Green Hills/Midtown. This suggests that neighborhoods of Parkwood and Union Hill offer lower cost opportunities for homeownership.

		Davidson County		Parkwood Union Hill	
		#	%	#	%
Population	Total, 2010	626,681		18,420	2.9%
	Household Population	605,463	96.6%	18,155	98.6%
	Group Quarters Population	23,650	3.8%	14	0.1%
	Population, 1990	510,784		16,351	3.2%
	Population, 2000	569,891		17,792	3.1%
	Population Change, 1990- 2000	59,107	11.6%	1,441	8.8%
	<i>U.S. Census (1990, 2000, 2010)</i> Population Change, 2000- 2010	56,790	10.0%	628	3.4%
	Population Density (persons/acre)	1.69	n/a	0.69	n/a
	<i>American Community Survey (2008 - 2012 5-year estimate)</i> Average Household Size	2.37	n/a	2.50	n/a
	Male	304,566	48.6%	8,330	45.2%
Female	324,547	51.8%	9,839	53.4%	
Population	Total	626,681		18,420	2.9%
Race	White	385,039	61.4%	8,097	44.0%
	Black or African American	173,730	27.7%	9,396	51.0%
	American Indian/ Alaska Native	2,091	0.3%	44	0.2%
	Asian	19,027	3.0%	110	0.6%
	Native Hawaiian or Pacific Islander	394	0.1%	11	0.1%
	Other Race	30,757	4.9%	477	2.6%
	<i>U.S. Census (2010)</i> Two or More Races	15,643	2.5%	285	1.5%
Ethnicity	Hispanic or Latino	61,117	9.8%	901	4.9%
Age	Less than 18	136,391	21.8%	4,472	24.3%
	18-64	424,887	67.8%	11,690	63.5%
	<i>U.S. Census (2010)</i> Greater than 64	65,403	10.4%	2,258	12.3%
Families	Total	142,821		4,966	n/a
	Married Couple Families with Children	37,098	26.0%	1,073	21.6%
	<i>American Community Survey (2008 - 2012 5-year estimate)</i> Single Parent Families with Children	26,291	18.4%	1,037	20.9%
	Female Householder with Children	21,528	15.1%	900	18.1%

		Davidson County		Parkwood Union Hill	
		#	%	#	%
Housing Units	Total	284,328		7,997	2.8%
	Owner Occupied	141,805	49.9%	4,725	59.1%
	Renter Occupied	114,082	40.1%	2,532	31.7%
	Occupied	255,887	90.0%	7,257	90.7%
	Vacant	28,441	10.0%	740	9.3%
<i>American Community Survey (2008 - 2012 5-year estimate)</i>					
Travel	Mean Travel Time to Work (min)	23.1		24.5	
	Workers	309,633		7,835	2.5%
	Drove Alone	246,391	79.6%	6,569	83.8%
	Carpooled	32,633	10.5%	752	9.6%
	Public Transportation	6,588	2.1%	194	2.5%
	Walked or Biked	6,806	2.2%	37	0.5%
	Other	3,232	1.0%	29	0.4%
	Worked from home	13,983	4.5%	254	3.2%
<i>American Community Survey (2008 - 2012 5-year estimate)</i>					
Income	Per Capita Income	\$28,513		\$22,391	78.5%
Education	Population 25 years and over	419,807		12,663	3.0%
	Less than 9th grade	20,687	4.9%	562	4.4%
	9th to 12th grade, No Diploma	38,664	9.2%	1,757	13.9%
	High School Graduate (includes equivalency)	103,024	24.5%	4,213	33.3%
	Some College, No Degree	86,498	20.6%	3,046	24.1%
	Associate Degree	23,963	5.7%	645	5.1%
	Bachelor's Degree	92,765	22.1%	1,658	13.1%
Graduate or Professional Degree	54,206	12.9%	782	6.2%	
<i>American Community Survey (2008 - 2012 5-year estimate)</i>					
Employment	Population 16 Years and Over	505,034	80.6%	14,088	76.5%
	In Labor Force	348,250	69.0%	8,869	63.0%
	Civilian Labor Force	347,862	99.9%	8,869	100.0%
	Employed	317,719	91.2%	8,078	91.1%
	Unemployed (actively seeking employment)	30,143	8.7%	791	8.9%
	Armed Forces	388	0.1%	0	0.0%
	Not in Labor Force	156,784	31.0%	5,219	37.0%
<i>American Community Survey (2008 - 2012 5-year estimate)</i>					

Parkwood-Union Hill's Role in the County and Region

The Parkwood – Union Hill community includes about 8 percent of the land area in the County. The number of housing units (single and multi-family) is less than 3 percent of the County total housing units. The office, commercial and industrial development is estimated to be only 1.2 percent of the County total.

The Parkwood-Union Hill community has commercial services that serve a regional area. Southern Parkwood/Union Hill attracts regional commercial activity as it has access to main roads. These roads are I24, I65, Briley Parkway, and Dickerson Pike. There are two significant developments. One is the 59-acre campus Skyline Medical Center located just north of the I-65/Dickerson Pike/Briley Parkway interchanges. The Nashville Commons at Skyline, an approximately 718,000 sq. ft. regional retail and restaurant mall, is located across from the hospital along Dickerson Pike.

The most significant public open space in the Parkwood-Union Hill Community is Cedar Hill Park. This 221-acre regional park serves much of the northern part of Davidson County.

Parkwood-Union Hill in a Regional Context

Parkwood-Union Hill plays an important role in the Middle Tennessee region, and the community's resources are abundant. Perhaps the community's greatest strengths, in the regional setting, are its convenient location and natural features. The proximity of Parkwood-Union Hill to I-65, I-24, and Briley Parkway, and other major corridors, which provide quick travel to Downtown and Goodlettsville, and the community's attractive natural features – rivers, hills, trees – will continue to make Parkwood-Union Hill a desirable place to live in the future.

Parkwood has experienced residential growth and commercial development over the past thirty years, while the Union Hill remains predominantly rural, mainly due to its steep terrain. One goal of the plan is to support the continued use and development of the established employment districts and service centers and committed areas. These areas include industrial, mixed commercial and neighborhood centers. These places are located in the following areas of the community: vicinity of

- I-24 & Old Hickory Boulevard.
- Brick Church Pk. & Old Hickory Boulevard
- Briley Pkwy & Brick Church Pike
- I-24 & Ewing Drive
- Dickerson Pike next to Goodlettsville; and
- Along Springfield Highway north of Goodlettsville.

Residential Development

Parkwood-Union Hill has neighborhoods that offer affordable single-family housing. Almost all land in the community is either single-family homes (60%) or rural/vacant (32%). Parkwood includes many small neighborhoods offering homeownership opportunities for families and seniors.

	Acres	Percent acres
Park	236	1%
Residential 1 unit	15,167	59%
Residential 2-3 unit	586	2%
Residential 4+ unit	122	0%
Residential non hh	1	0%
Community, institutional, utility	604	2%
Office or medical	78	0%
Commercial	415	2%
Auto parking	4	0%
Industrial	208	1%
Vacant/farm	8,102	32%
TOTAL	25,524	

Natural Features and Environmental Treasures

Natural features and ecosystems maintain good air and water quality for all residents. Floodplains manage stormwater, and natural systems contribute to solid waste management. Other natural features include slopes, soils and geologic formations. The Parkwood-Union Hill Community has steep terrain, major waterways, and floodplains. There are also problem soils, sinkholes, wetlands, and rare and endangered species

Steep Slopes

Roughly 11,000 acres (41 percent) of the Parkwood-Union Hill Community's land has steep slopes. The northern section of Parkwood – Union Hill is very hilly and dominated by terrain with slopes exceeding 20 percent. There is also a sizeable area south of Old Hickory Boulevard, between I24 and Brick Church Pk. with steep slopes. Finally, there is another very hilly area from west of Dickerson Pk. Eastward to I65 and extending from Briley Pkwy. to north of Due West Ave. Many problem soils are found in or near such steeply sloping areas.

Major Waterways and Floodplains

About 6 percent of the land in Parkwood-Union Hill is within the 100-year floodplain and floodways along major creeks. These creeks are the east and north forks of Ewing, Little, Whites, Lickton, Bakers, Dry, and Mansker creeks. Several unnamed major tributaries of these creeks also have 100-year floodplains. Most land along these creeks is private residential property with floodplain in a portion of the yards. As some soils in Parkwood-Union Hill pose a problem in areas that flood, floodplains are best used for greenways. Greenways are being planned along many of the creeks mentioned above. There are small wetlands in Parkwood – Union Hill, but there are no large concentrated wetland areas.

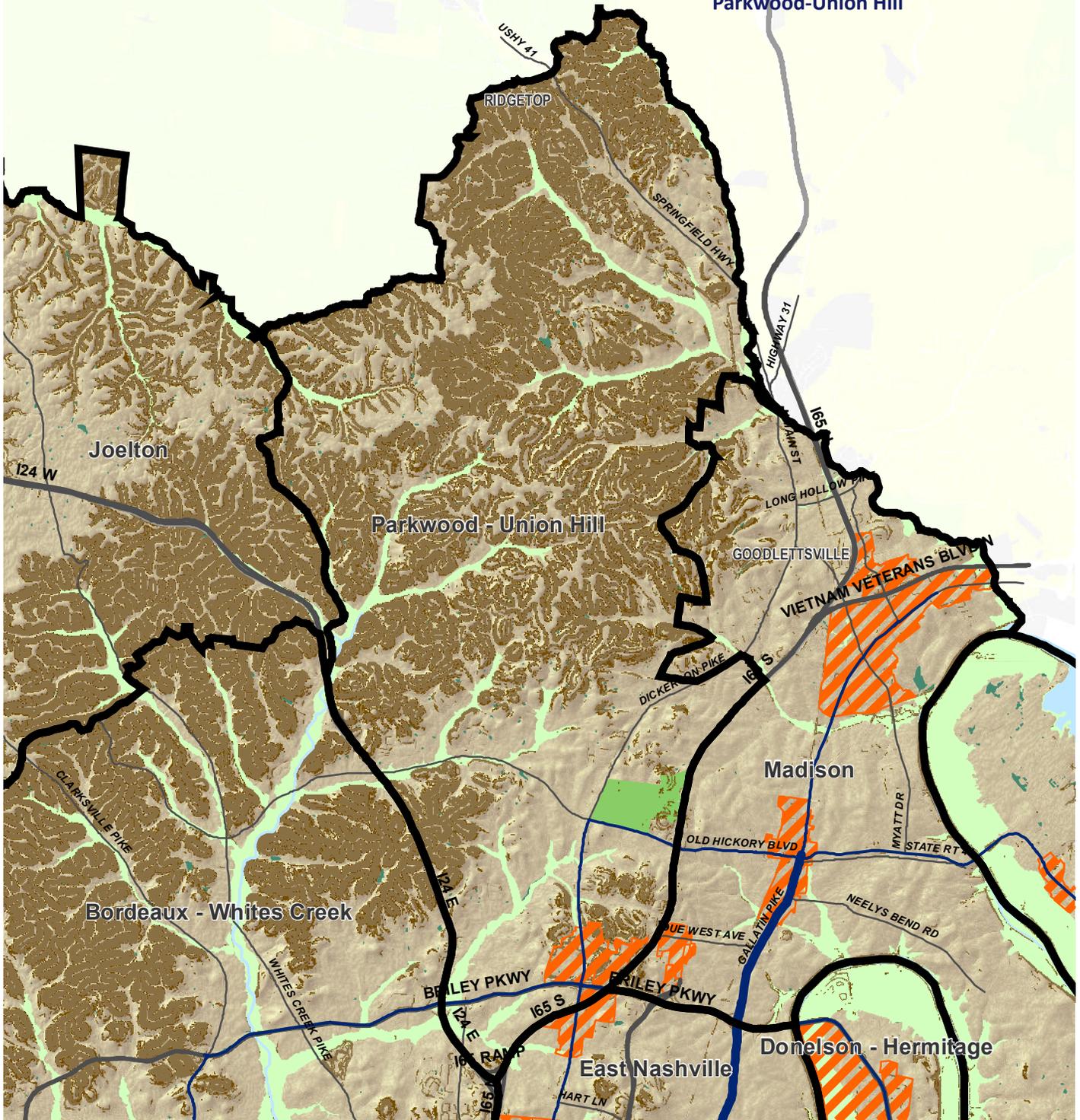
Rare and Endangered Species

The Parkwood – Union Hill Community contains a variety of rare or endangered plant and animal species. The Tennessee Department of Environment and Conservation maintains information about these species.

Recreation – Community Services and Open Space

The most significant public open space in the Parkwood- Union Hill Community is Cedar Hill Park. This is a 221 acre regional park that serves much of the northern part of Nashville/Davidson County. Other parks and open spaces include a variety of sites that are mostly neighborhood parks and schools. There are also two sizeable cemeteries next to Dickerson Pike in the Old Center area.

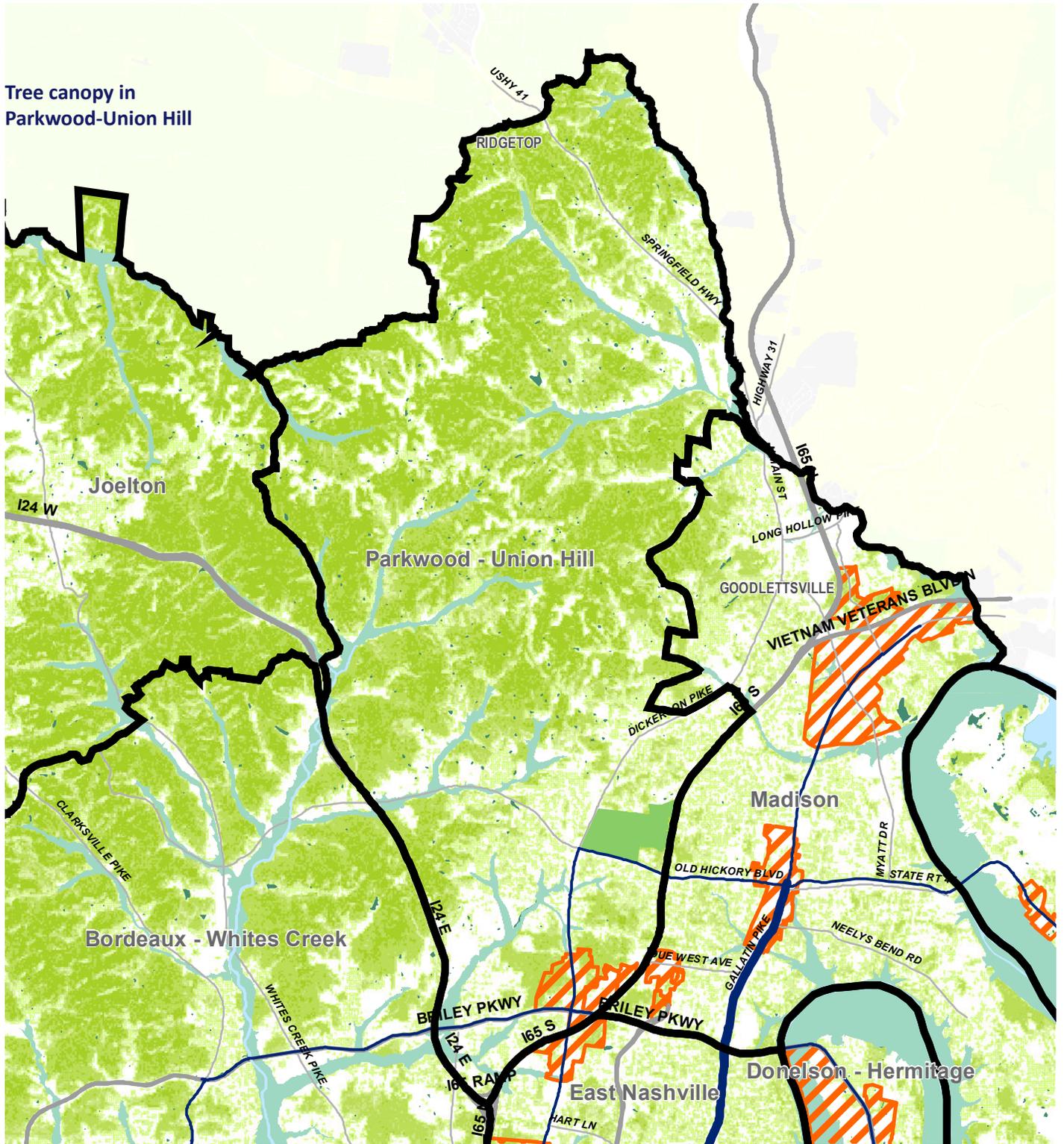
Open space features in
Parkwood-Union Hill



Slopes & Terrain Legend



**Tree canopy in
Parkwood-Union Hill**



Tree Canopy Legend

	Water Bodies	Tree Canopy
	Anchor Parks	 1 - 20%
	Floodplain Areas	 21 - 40%
	Wetlands	 41 - 60%
		 61 - 80%
		 81 - 100%



The existing neighborhood parks in the community are Willow Creek Park and Parkwood Park. Some community elementary schools have open space serving as parks. These include Old Center, Bellshire and Chadwell Elementary schools.

Greenways

Adding greenways or other trails can improve an area's quality of life as residential, commercial, employment, and recreational uses develop. Increasing connectivity among residential and commercial centers, as well as schools, encourages this development. Trail connections, greenways, improved roadway crossings, and paths increase connectivity. This adds value to a neighborhood by providing residents with alternative transportation options. It also encourages healthier and more active lifestyles.

Local Work-Force Assets

According to the 2008-2012 American Community Survey, the Parkwood-Union Hill Community has 8,500 workers. These workers reported a mean travel time of 25 minutes to employment. A majority work in South Nashville, Madison, Green Hills-Midtown, Downtown, and Old Hickory. Only a modest number work in Parkwood Union-Hill. In addition, 254 workers reported working from home, a lower rate than the rest of Davidson County. Fewer residents have advanced degrees as compared to Davidson County as a whole. The 2008-2012 American Community Survey reported that 1,658 people, or 13 percent, held bachelor's degrees and 782 people, or 6 percent, held graduate or professional degrees. This compares to 15 percent holding bachelor's degrees and 9 percent holding graduate or professional degrees in Davidson County. The per capita income (the average income per person) in Parkwood-Union Hill is also lower at \$22,391 as compared to Davidson County's at \$28,513, suggesting less disposable income. Despite this, the community has a higher homeownership rate of 59%, 10 percentage points higher than Davidson County.

Commuting patterns of residents and employees in Parkwood Union Hill

Parkwood Union Hill residents who work in these areas	8,439	Employees who work in Parkwood Union Hill come from these areas	6,183
Parkwood Union Hill	564	Parkwood Union Hill	564
South Nashville	1,124	Madison	604
Madison	1,070	Southeast	399
Green Hills Midtown	1,003	Donelson Hermitage Old Hickory	395
Downtown	824	Antioch Priest Lake	368
Donelson Hermitage Old Hickory	578	East Nashville	323
North Nashville	532	West Nashville	318
Bordeaux Whites Creek	489	Green Hills Midtown	175
East Nashville	485	Bordeaux Whites Creek	169
West Nashville	315	Bellevue	140
Antioch Priest Lake	307	South Nashville	84
Southeast	167	North Nashville	48
Bellevue	45	Joelton	30
Joelton	35	Downtown	0
Beyond Davidson County	901	Beyond Davidson County	2,566

Source: Census Transportation Planning Products, using the American Community Survey (2006-2010 five-year estimates)

PARKWOOD-UNION HILL

chapter two

Growth and Preservation Concept Map and the Community's Role

The Growth and Preservation Concept Map is a county-wide vision and tool for aligning spending, regulations, and Metro programs to shape improvements in quality of life so that new development and redevelopment aligns with community values. The Concept Map provides guidance for the entire county. Six key factors reflecting Nashville/Davidson County community members' priorities guided the design of the Growth and Preservation Concept Map:

- » Protect sensitive environmental features.
- » Build a complete transit network.
- » Maintain household affordability across income levels.
- » Create “activity centers” – areas of employment, residences, services, civic uses, retail and restaurants – throughout most parts of Davidson County.
- » Protect and enhance the character of different parts of Davidson County.
- » Allow for strategic infill that supports transit lines and activity centers.

The Concept Map for Parkwood-Union Hill illustrates the key concepts listed above: preserving environmentally sensitive features and open space; preserving established residential areas; strategically locating new residential development; enhancing commercial centers and corridors to improve quality of life and access to services for Parkwood-Union Hill; and adding more connectivity, primarily through bikeways, greenways multi-use paths, and improved transit service along Dickerson Pike.

Green Network

The green network on the Growth and Preservation Concept Map reflects natural and rural areas that provide natural resources (such as water and land for farming), ecological services (such as cleaning air and slowing water runoff), wildlife habitat, and recreation opportunities. The network also includes sensitive natural features that can be disturbed or destroyed by

development or that pose a health or safety risk when they are developed (such as steep slopes and floodplains).

In Parkwood-Union Hill, a large part of the Growth and Preservation Map is the green network. Much of this area is contiguous steep slopes, woodlands, floodways and floodplains. Nearly all of the Union Hill (the northern part of the community) is part of the Green Network. Some of Parkwood is included, particularly hilly, wooded areas in the Whites Creek headwaters and hilly terrain near Skyview.

Neighborhoods

Neighborhood areas are primarily residential areas offering a mix of housing types and character, with smaller civic and employment areas and small neighborhood centers. Neighborhoods have different context – rural, suburban, urban, or downtown – depending on their location and character.

Parkwood's neighborhoods are predominantly suburban in character. To the north, Union Hill is predominantly farmland and rural residential.

Transitions and Infill

Transition and infill areas are areas of moderate density multifamily residential and small-scale offices that are appropriate along and around prominent corridors and centers to provide a harmonious connection to surrounding neighborhoods. These areas provide transitions – in building types as well as scale and form – between higher intensity uses or major thoroughfares and lower density residential neighborhoods. These areas provide housing in proximity to transit and commercial services, increasing the likelihood that residents can walk or bike to meet some of their daily needs. These areas also provide a diversity of housing types that are attractive to Nashvillians.

In Parkwood-Union Hill, transitional areas are located near Skyline and along Briley Parkway, Dickerson Pike north to Old Hickory Boulevard, and Old Hickory east from Dickerson.

Centers

The centers included in the Growth and Preservation Concept Map build on existing commercial center areas to evolve into active, mixed-use places serving as a neighborhood or community gathering place. Centers are anticipated to become pedestrian-friendly areas with frequent transit service that contain a dense mix of homes, shops, jobs and parks, as well as services, schools and cultural amenities. The Concept Map places center areas in three tiers:

Tier One

These centers are the focus of coordinated investments to shape growth and support transit service in the next ten years.

Tier Two

These centers receive some investments to manage growth, though less than Tier One centers.

Tier Three

These centers show areas that could receive coordinated investments in response to opportunities identified by the private sector.

Skyline is a Tier Three Center. As transit improvements are made on Briley Parkway or Dickerson Pike north of Trinity, Skyline will become a higher priority. As private development occurs in and near Skyline, they should be encouraged to incorporate sidewalks and connections to begin creating a walkable neighborhood center.

Parkwood-Union Hill – Community Character Policy Plan

The Parkwood-Union Hill Community Character Policy Plan builds upon the Growth and Preservation Concept Map. The Community Character Policies take the Concept Map to the next level of detail by addressing the form and character of each area in Parkwood and Union Hill. The Community Character Policies are the standard by which development and investment decisions are reviewed, including requests to change zoning.

The Parkwood-Union Community Plan provides guidance, applying to every property in Parkwood and Union Hill, through the policies found in the Community Character Manual. Those policies are intended to ensure that the elements of development are coordinated to ensure the intended character of an area is achieved. The Community Character Manual is the first part of Volume III.

Union Hill's natural and open space areas include areas with environmentally sensitive features, such as floodplains, steep slopes and unstable soils, as well as public parks and open space. As a result of other communities' experiences in the May 2010 flood, the plan encourages the preservation of all environmentally sensitive features, but particularly floodplains and floodways, through the use of Conservation policy. The policy encourages the preservation and/or reclamation of these features. Conservation policy also includes steep slopes. Research has shown that the headwaters of many streams and tributaries to the Cumberland River lie in these steep slopes. Preservation of these areas can reduce the impact of flooding in the future by slowing down and absorbing stormwater runoff during rain events.

The Parkwood-Union Hill Community's desire to maintain and enhance its rural and suburban residential neighborhoods is shown by the placement of Neighborhood Maintenance policy. However, to maintain long-term sustainability of the community and to provide housing for residents at every point in their lives, an appropriate mixture of housing types must still be provided, primarily in Parkwood and along Dickerson Pike. Appropriate locations for additional residential development are indicated by applying Neighborhood Evolving and Center policy areas.

Another area of emphasis is enhancing centers and corridors. The primary corridor in Parkwood-Union Hill is Dickerson Pike. It provides services and connects residents to workplaces and retail south to downtown and north to Goodlettsville and Sumner County. Smaller corridors with less commercial and residential development include Brick Church Pike and Old Hickory Boulevard. Parkwood-Union Hill's primary center is at Skyline. Smaller centers are located along Brick Church Pike (especially at Briley and I-24), and Old Hickory Pike (also near I-24). Despite these smaller centers and corridors, new walkable and commercial development should focus on the Community Center at Skyline, Dickerson Pike, and a small neighborhood center at Old Hickory Boulevard and Brick Church Pike. These areas should be enhanced by adding a mixture of uses, additional housing options, additional connections for pedestrians and cyclists, and additional transportation options such as transit. The transition between these higher-intensity areas and the surrounding neighborhoods must also be addressed through as well-designed land use transitions to adjacent residential areas.

Parkwood-Union Hill Special Policies

This Plan provides guidance through the policies found in the Community Character Manual (chapter one of NashvilleNext Volume III). Those policies are intended to ensure that the elements of development are coordinated to ensure the intended character of an area is achieved. They provide guidance on appropriate building types/designs, appropriate location of buildings on property, and other elements, including sidewalks, landscaping, bikeways and street connections. In addition to the guidance found in the Community Character Manual, there are also associated special policies contained in this section that provide additional detailed guidance in a few select areas.

How to use the community plan and special policies with the larger CCM

Within some Community Character Policy areas there are unique features that were identified during the community planning process where additional guidance is needed beyond what is provided in the CCM. This additional guidance is referred to as a Special Policy and is included in each community plan. The Special Policies may provide additional specificity to the broad language in CCM or it may describe conditions that deviate slightly from the CCM policy. In all cases, users should first refer to the separate CCM document to understand the policy's general intent, application, characteristics, and design principles. Then look at the Community Plan for any Special Policies that discuss unique conditions that may exist. The Special Policies are not identified as a separate graphic on the map, but are found within the text of a Community Character Policy Area. Thus, when a user looks up a Community Character Policy Area on a map, its number will correspond with the special policies in the text. The Community Character Policy Plan and Special Policies are found in Chapter II.

Special Policy Area #1

Davidson Academy

The Suburban Neighborhood Evolving (T3 NE) policy for this special policy area applies only if:

1. Davidson Academy relocates and
2. the Davidson Academy facilities and campus will redevelop, rather than be used by another institutional, civic or public benefit use.

Redevelopment and rezoning should be based on a single unified plan for the entire special policy area. Proposals should be implemented only through the “SP” (Specific Plan) base zone district or a “UDO” (Urban Design Overlay) district combined with appropriate base districts. Without a single unified plan, partial rezoning and redevelopment of this area based on the Neighborhood Evolving policy is not recommended. Instead RL (Residential Low Density) should be the applicable policy.

Special Policy Area #2

Former Union Hill School Site

The Union Hill school building is historically significant. It is considered “worthy of conservation” by the Metro Historical Commission and should be preserved. The plan recommends applying a “NL” (Neighborhood Landmark) overlay district to this site to protect the building from demolition and provide necessary flexibility for its reuse. If the historic building is not reused, then Rural policy applies to this site. It should be noted that several community members expressed a strong preference for the Union Hill School to be re-opened as a school in the future.

Special Policy Area #3

Brick Church Pike & I-24

This large Suburban Neighborhood Evolving (T3 NE) area should develop to create an interconnected road network. The density should be similar to existing nearby development, although areas of higher density are appropriate along Brick Church Pike to protect sensitive environmental features.

Development Scenarios

In some of the following policy areas, development scenarios are included to help the reader envision what development may look like under the guidance of the policies. The development scenarios are:

- » Sustainable Multi-Family Residential in Areas with Steep Slopes
- » Conservation subdivisions

It is important to note, however, that development scenarios are only examples and illustrations of what the land use policy would support in the specific area. There are other ideas and examples beyond what is illustrated in these scenarios that would also meet the intent of the community character policies. The development scenarios are not actual or required development plans, but can be used to help inspire new development in the Bellevue Community.

Sustainable Multi-Family Development in Areas with Steep Slopes

Figure 1 represents how multi-family has often been developed during previous years. Figure 2 illustrates the benefits and design characteristics of a *sustainable* multi-family development. This type of “low-impact” development has many sustainable benefits including: reducing harmful stormwater runoff, protecting headwaters, improving overall water quality, preserving viewsheds within the community, minimizing site grading, reducing construction costs, preserving open space, preventing landslides, and reducing flooding. The design characteristics of this sustainable development include concentrating development in a compact form, which reduces the amount of grading required for the site, preserving steep slopes and reducing the impervious area of the site that causes harmful stormwater runoff. Limiting the impact of development on steep slopes reduces or eliminates the possibility of landslides and minimizes the possibility of flooding. Furthermore, open space and tree preservation on-site improves stormwater quality by allowing stormwater to infiltrate into the ground within the site and allowing trees and other vegetation to filter the runoff of harmful pollutants.

Maintenance of natural viewsheds within Parkwood-Union Hill support a number of important community goals including protecting the natural environment, ensuring quality of life, and promoting economic

development of the area.

For more information on these design concepts, please refer to the Conservation section of the Community Character Manual at the beginning of Volume III.

Figure 1 shows an example of how multi-family is often developed. Note the lines representing topography and how the buildings and parking cut into the sides of hills, requiring retaining walls and having a greater impact on the steep slopes and potentially unstable soils on the site.

Figure 2 shows an example of how multi-family development can be made more sustainable. Note how the buildings and parking run parallel to the slopes, requiring less cutting into the hill. Also, note how the development is more compact, preserving more of the site and its environmentally sensitive features. Finally, trails are added to encourage the use of the natural areas as an asset for the residents.

The Dickerson Pike Corridor

The pattern of development and the overall appearance of the Dickerson Pike corridor are a major focus of the plan. This is due to because of the corridor's significance, high visibility as It is a transportation artery, and its role as a major business district, and a retail service area.

This plan aims to enhance the overall appearance and pedestrian-friendliness of the Dickerson Pike corridor. It will develop mixed-use nodes to serve neighborhoods at key entrances. Between these neighborhood-serving nodes, mixed uses will line the corridor. These areas should include office and commercial activities, community services, and residential uses.



Figure 1: Conventional approach



Figure 2: Sustainable approach

Enhancements to the Open Space Network

Each of the Community Plans complements and relies on the *Nashville Open Space Plan* and the *Metropolitan Park and Greenways Master Plan* (“Parks Master Plan”) for projects and enhancements. Both the Open Space Plan and the Parks Master Plan along with current project information may be found at: <http://www.nashville.gov/Parks-and-Recreation/Planning-and-Development.aspx>

The plan also has established a service radius standard of no more than one half mile for neighborhood parks. That means that a park should serve the residents within a one-half mile. Some elementary schools are designated as neighborhood parks in the Parks Master Plan. This plan recommends that the former Brick Church Elementary school be added to this list. This will provide parks near the dense residential area between Brick Church Pike and I-24 south of Briley Parkway. These parks will be within a walkable one-quarter mile distance of the homes. If the former Brick Church Elementary school site is not used as a school, then a small portion of the site should be kept as a small public park. This park would be walkable from the western edge of the Parkwood neighborhood. It could also serve residential areas on the west side of I-24 beside Parkwood – Union Hill.

New parks will help Parkwood-Union Hill meet the standard for parks. This standard says there should be a park within a one-half mile radius of neighborhoods.

Currently, there are no community parks in Parkwood – Union Hill.

Greenway System Connections and Multi-Use Paths

Greenways in the northern, rural section of the community are appropriate for conservation. The open space and natural areas conserved remain in private ownership. Future potential for greenway dedications or conservation easements for public trails is secondary. These possible public trails would accompany proposed new subdivisions or other new developments. There are options for creating a network of greenways. Greenway trails through natural settings are one possibility. Another option is roadside trails that take the place of sidewalks along more rural highways. For instance, this could take place along Lickton Pike and Baker Road.

- » Ewing Creek Greenway
- » Old Hickory Boulevard Multi-Use Path

Enhancements to the Transportation Network

In addition to community character, each of the Community Plans considers the needs of vehicular users, bicyclists, pedestrians, and transit users in its guidance and recommendations. It does so by utilizing *Access 2040* as its foundation along with the *Major and Collector Street Plan (MCSP)*. The MCSP is part of, and implements, *Access 2040*. The MCSP maps the vision for Nashville's major and collector streets and ensures that this vision is fully integrated with the city's land use, mass transit, bicycle and pedestrian planning efforts. The *Strategic Plan for Sidewalks and Bikeways* establishes high-priority sidewalk areas and outlines future sidewalk and bikeway projects for the city. There are additional plans that outline committed funding and project priorities, including the city's Capital Improvements and Budget Program.

Please refer to *Access 2040* for details and information on these plans, the city-wide vision for various modes of transportation, recommended projects, and other details.

Major and collector streets

Parkwood-Union Hill detail



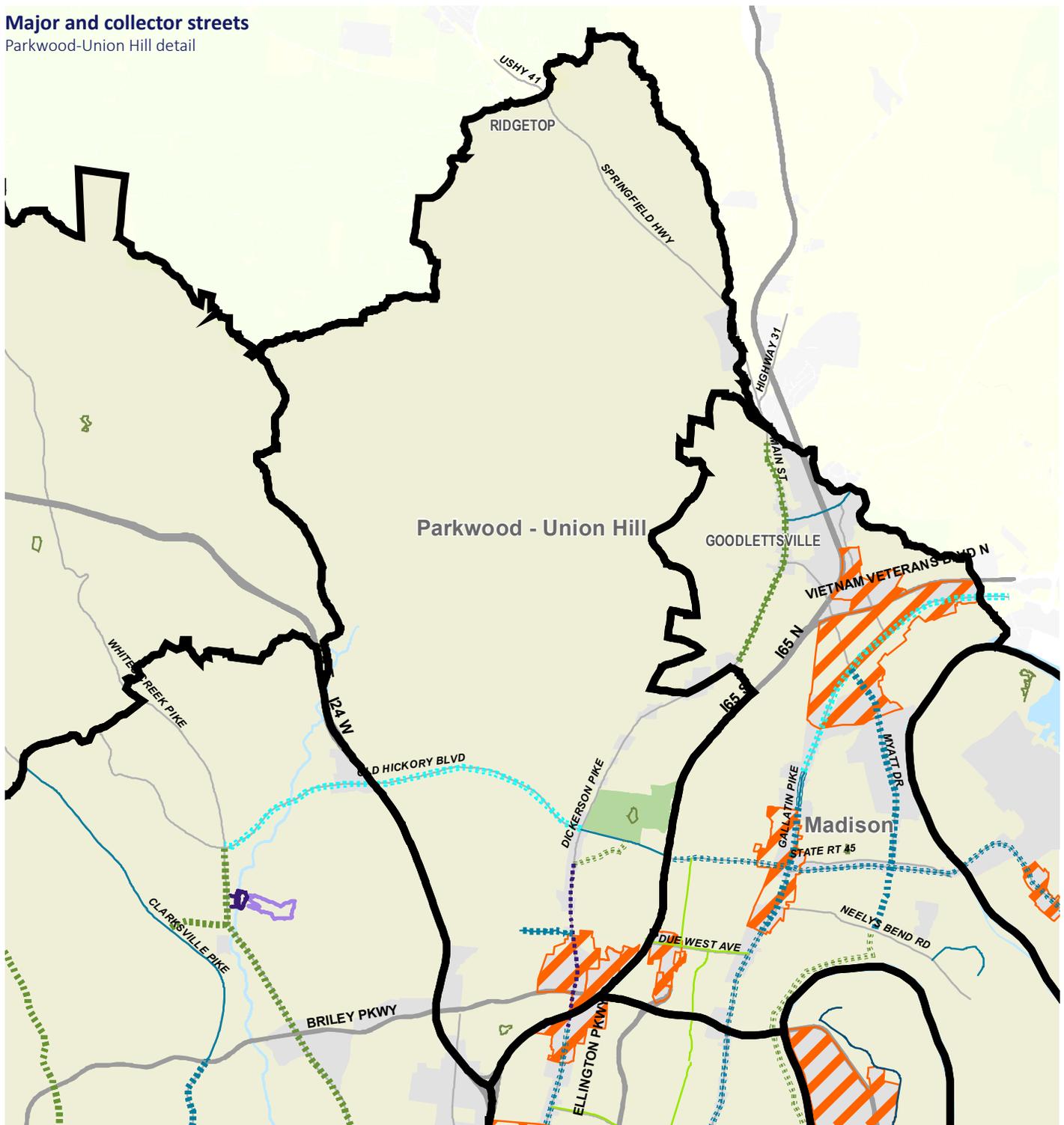
Major and Collector Street Legend

- | | | |
|---|---|---|
|  Potential Multimodal Freeway Corridor |  Planned Arterial-Boulevard |  Local Street |
|  Planned Multimodal Freeway Corridor |  Collector-Avenue |  Planned Local Alley |
|  Arterial-Parkway Scenic |  Planned Collector-Avenue |  Freeway or Expressway |
|  Arterial-Boulevard Scenic |  Downtown Local Street |  Ramp |
|  Planned Arterial-Boulevard Scenic |  Planned Downtown Local Street |  Planned Ramp |
|  Arterial-Boulevard |  Planned Downtown Alley | |

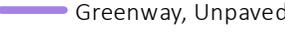
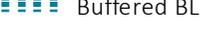
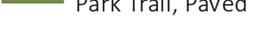
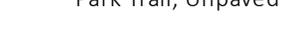


Major and collector streets

Parkwood-Union Hill detail

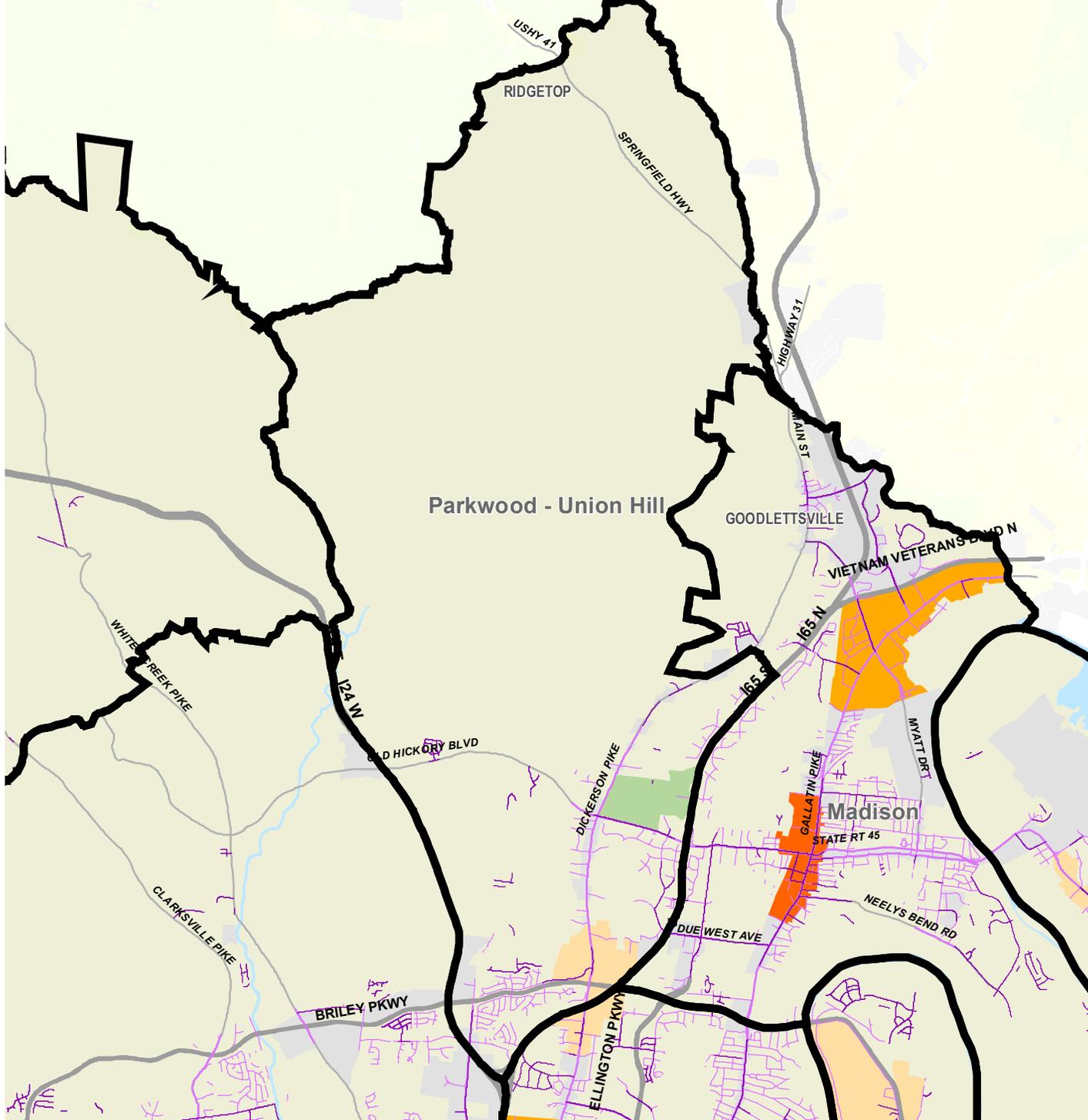


Bikeways and Greenways Legend

- | | | | |
|---|---|--|---|
|  Anchor Parks |  Bike Lane Vision |  Greenway Vision |  Existing Greenways |
|  Water Bodies |  BL/Buffered BL |  Bike Lane, Paved |  Greenway, Paved |
|  Special Uses |  Bike Lane |  Greenway, Paved |  Greenway, Unpaved |
| |  Buffered BL | |  Mountain Bike Trail, Unpaved |
| |  Signed Shared Route | |  Park Trail, Paved |
| | | |  Park Trail, Unpaved |



Sidewalk network
Parkwood-Union Hill detail



Sidewalks Legend

- | | | |
|--|---|---|
|  Water Bodies |  Missing Sidewalks |  Centers |
|  Anchor Parks |  Local |  Third |
|  Special Uses |  Major Road |  Second |
|  Existing Sidewalks |  Minor Road |  First |



Walking

- » Brick Church Pike Sidewalks - Construct sidewalks along Brick Church Pike from Briley Parkway to Bellshire Drive.
- » Skyline to Bellshire Sidewalks - Fill in sidewalk gaps and improve street crossings along Dickerson Pike between Briley Parkway and Old Hickory Boulevard
- » Old Hickory Boulevard Walking Improvements - Improve street crossings on Old Hickory Boulevard near Mt. Zion Baptist Church.

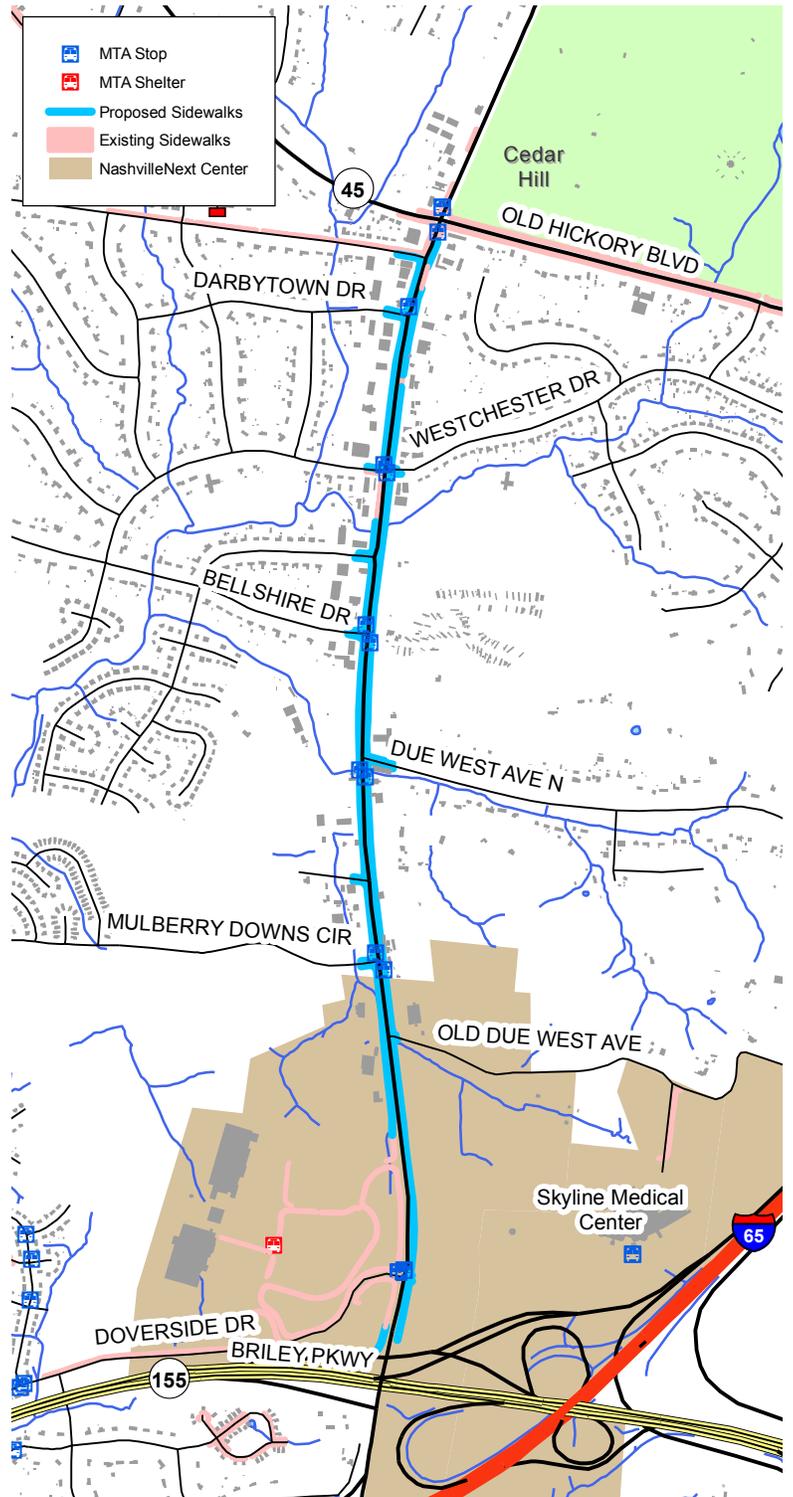
Brick Church Pike Sidewalks

Construct sidewalks along Brick Church Pike from Briley Parkway to Bellshire Drive.



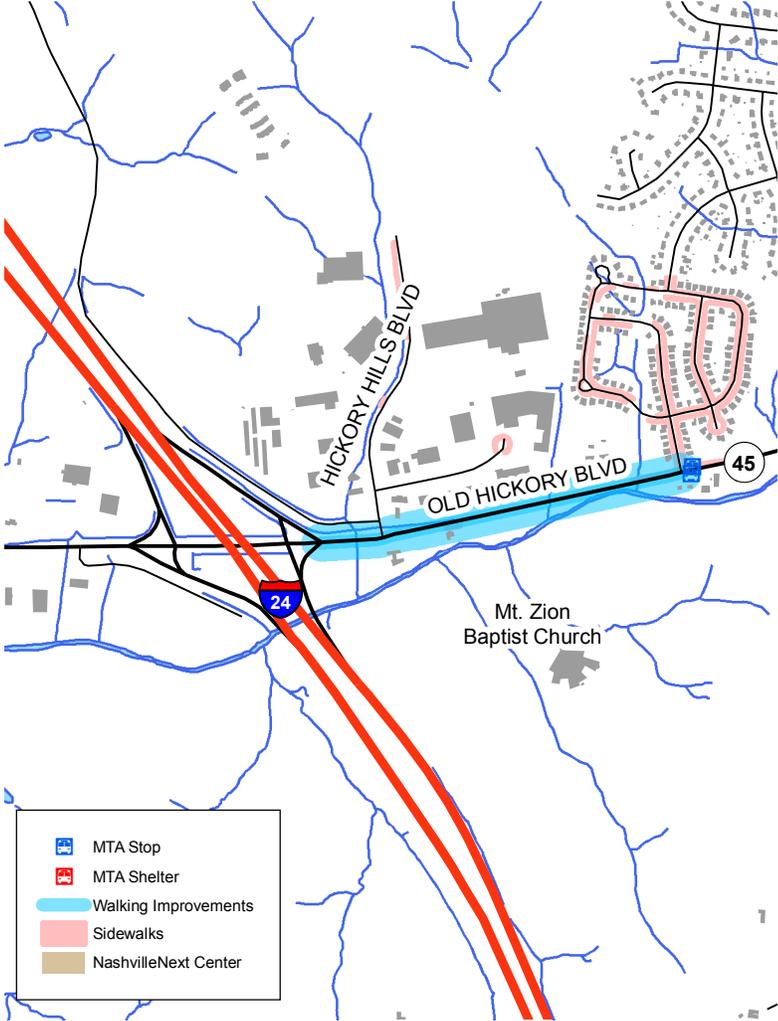
Skyline to Bellshire Sidewalks

Fill in sidewalk gaps and improve street crossings along Dickerson Pike between Briley Parkway and Old Hickory Boulevard



Old Hickory Boulevard Walking Improvements

Improve street crossings on Old Hickory Boulevard near Mt. Zion Baptist Church.

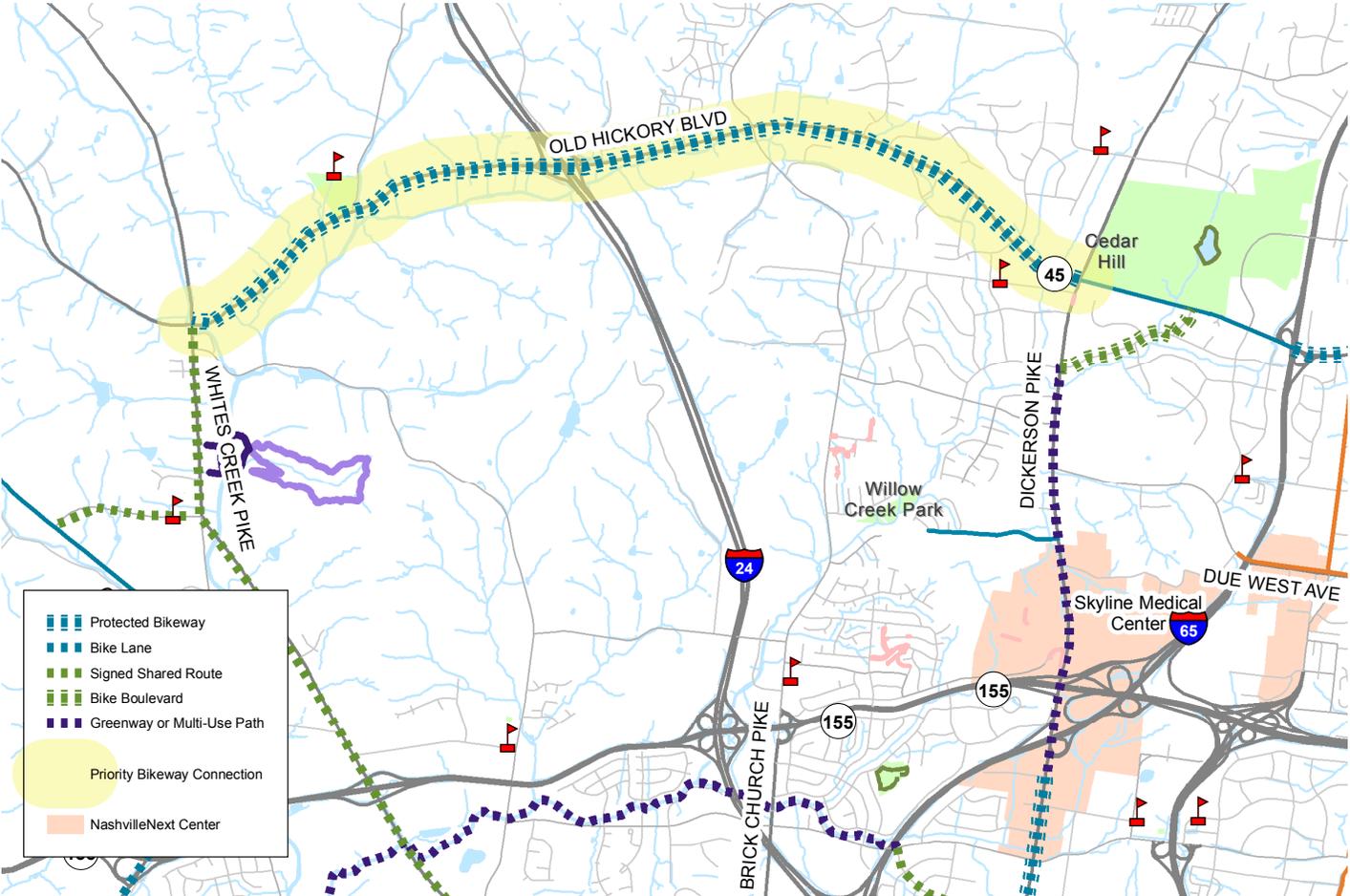


Biking priority projects

- » Old Hickory Boulevard North Multi-Use Path - Develop a multi-use path adjacent to Old Hickory Boulevard between Cedar Hill Park and Whites Creek.
- » Ewing Creek Greenway - Develop from the Whites Creek Greenway to Cedar Hill Park.

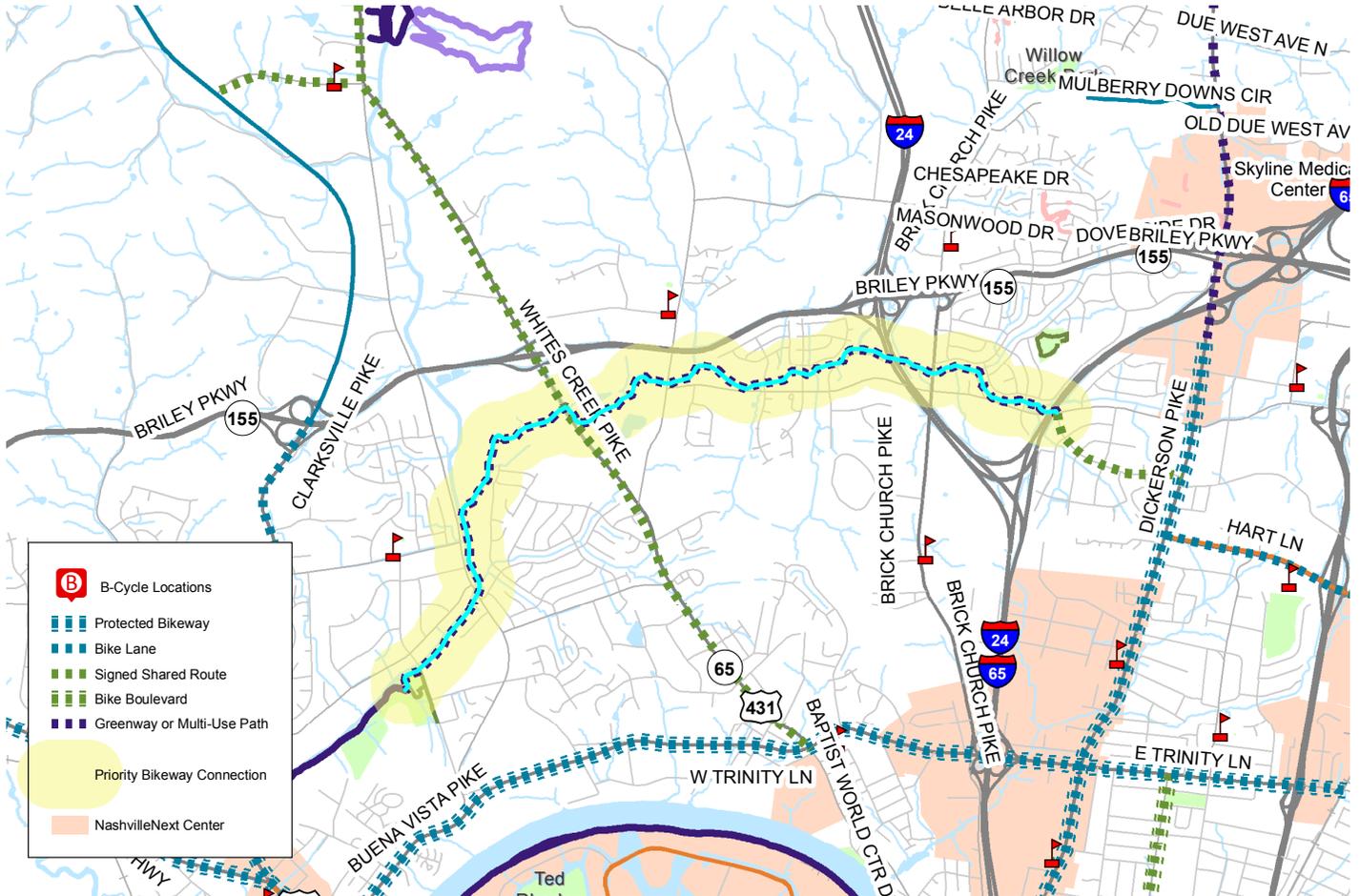
Old Hickory Boulevard North Multi-Use Path

Develop a multi-use path adjacent to Old Hickory Boulevard between Cedar Hill Park and Whites Creek.



Ewing Creek Greenway

Develop from the Whites Creek Greenway to Cedar Hill Park.



Streets priority projects

None identified

Transit needs

Long Term Needs

- » Dickerson Pike from Trinity Lane to Old Hickory Boulevard
- » Clarksville Pike-Briley Parkway-McGavock Pike from Kings Lane to Lebanon Pike

Regional Need

- » Northeast Corridor to Gallatin