

1 0 3 5 West Eastland SP

Case No.: 2014SP-059-001

2014.07.31



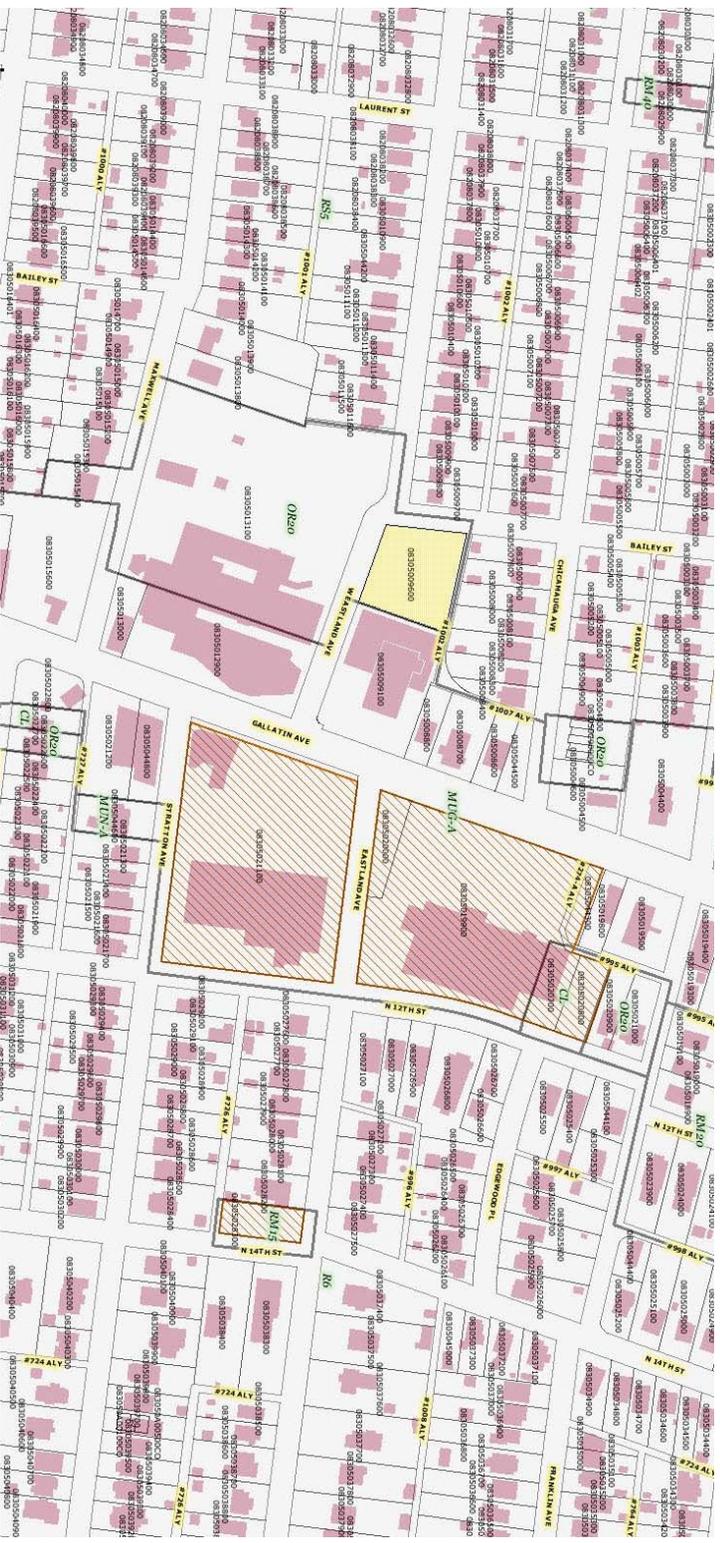
HASTINGS
ARCHITECTURE ASSOCIATES, LLC

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PURPOSE OF THE SP AND
COMPLIANCE WITH THE GENERAL PLAN



This Specific Plan will provide a transition between the commercial/mixed-use character of Gallatin Pike and the Maxwell Heights neighborhood. The SP will provide the transition in use, form, and character. In conformance with the guidance of T4 Community Center, the SP will “complement the adjacent neighborhoods that it serves and the infrastructure to which it has access.”

As Nashville continues to grow and demographics shift, housing choice will become increasingly important. 1035 West Eastland is an excellent location for multi-family housing. It is near the Gallatin Road BRT-lite line, near grocery stores, services, shops, restaurants, schools, and other civic institutions, and adjacent to a stable single-family neighborhood. Housing on this site will allow residents to use transit, walk and bike to many of their daily activities. As East Nashville continues to be a highly desirable neighborhood, the building designed by this SP will provide housing choices for existing residents who wish to stay in the neighborhood, those who need a different price-point than what single-family housing might provide, and those who desire the lifestyle provided by multi-family housing.

The Eastland SP is in-keeping with the design principles of T4 Community Center (T4 CC). The guidance of T4 CC specifically mentions that the transition between T4 CC and other lower intensity land uses should happen within the boundaries of T4 CC; this SP ensures that this property will act as that transition. The SP allows residential over the entire site, and allows commercial uses along West Eastland to the corner with Bailey Street. The SP allows a 4-story building, transitioning from the higher intensity uses and height allowed on Gallatin Road and the lower single-family character of Maxwell Heights. The SP utilizes a build-to zone, locates parking behind or beside the building, regulates the height of stoops, and regulates the glazing of the facade. Further, the SP utilizes shared parking with the adjacent property, provides bicycle parking, and utilizes on-street parking. These standards and agreements guarantee an urban character and contribute to a pedestrian friendly environment.

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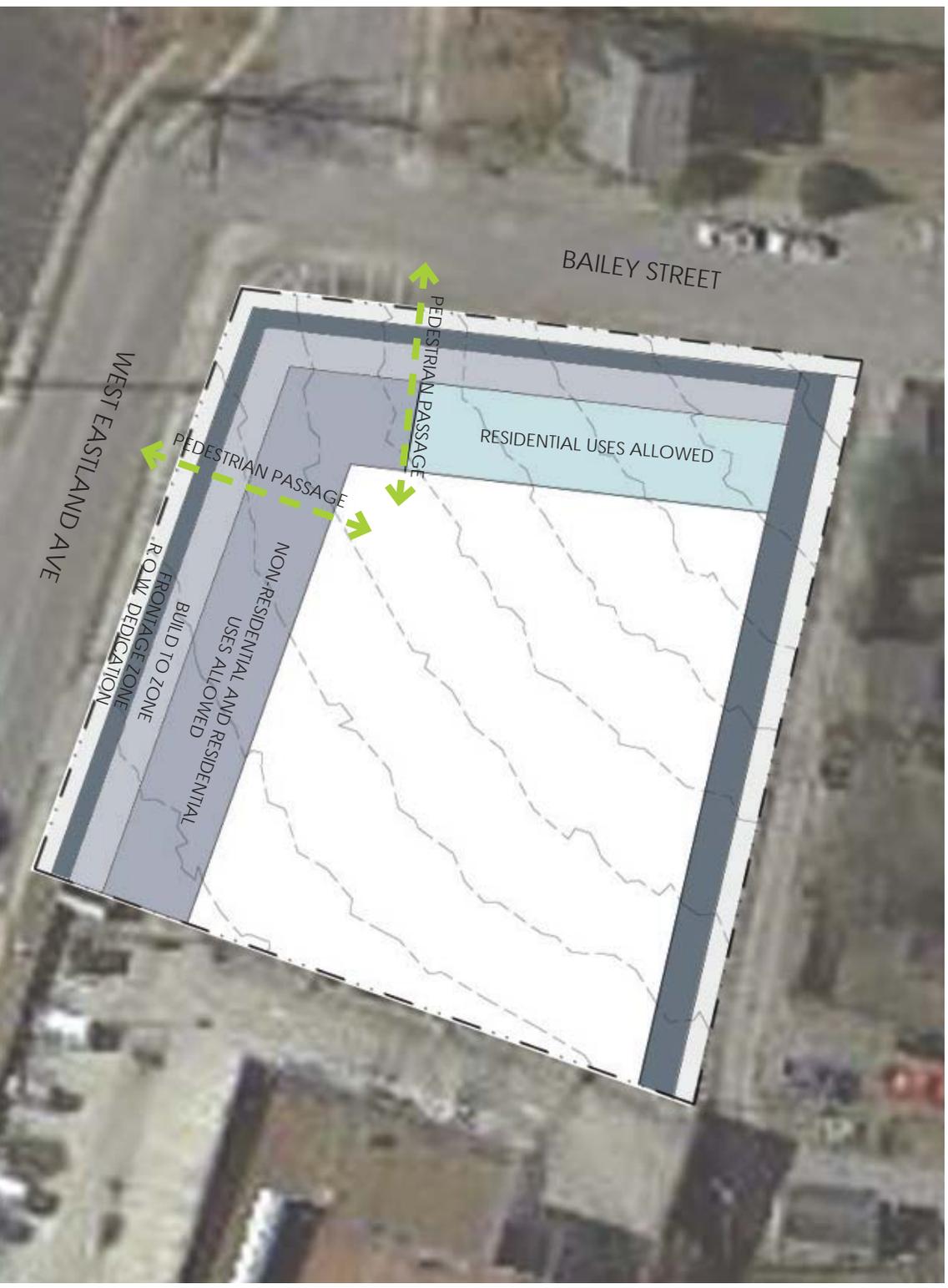
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LAND USE DIAGRAM



The land uses allowed by the SP are based on the land uses allowed in MUL-A zoning. Uses shown in red with a strikethrough (~~example-text~~) are not allowed in the SP. All other uses are permitted, permitted with conditions, or permitted as an accessory use. All conditions and regulations of use shall be according to the standards of the zoning code. Commercial uses shall be limited to the ground floor only. Land Use locations shall be according to the Use Location Diagram.

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LAND USE TABLE

Communication Uses	
Amateur radio antenna	P
Audio/video tape transfer	P
Multi-media production	P
Printing and publishing	P
Radio/TV/satellite tower	SE
Radio/TV studio	
Satellite dish	P
Telephone services	PC
Industrial Uses	
Artisan Distillery	
Building contractor supply	
Distributive business/wholesale	PC
Fuel storage	
Heavy equipment sales & service	
Hazardous operation	
Manufacturing, heavy	
Manufacturing, medium	
Manufacturing, light	
Microbrewery	
Research service	
Scrap operation	
Tank farm	
Tasting Room	
Warehouse	PC
Transportation Uses	
Airport/heliport	
Boatdock (commercial)	
Bus station/landport	
Bus transfer station	SE
Commuter rail	SE
Helistop	
Motor freight	
Park and ride lot	
Railroad station	
Railroad yard	
Water taxi station	
Utility Uses	
Power/gas substation	PC
Power plant	
Reservoir/water tank	PC
Safety services	P
Waste water treatment	SE
Water/sewer pump station	P
Water treatment plant	SE
Wind energy facility (small)	PC
Wind energy facility (utility)	
Waste Management Uses	
Collection center	
Construction/demolition landfill	
Construction/demolition waste processing (project-specific)	PC
Medical waste	
Recycling collection center	A
Recycling facility	
Sanitary landfill	
Waste transfer	
Recreation and Entertainment Uses	
Adult entertainment	
After-hours establishment	
Camp	
Club	P
Commercial amusement (inside)	P
Commercial amusement (outside)	P
Country club	
Dive-in movie	
Dive-in range	PC
Fairground	
Golf course	P
Greenway	P
Park	P
Racetrack	
Recreation center	P
Rehearsal hall	P
Stadium arena/convention center	
Temporary festival	SE
Theater	P
Zoo	
Other Uses	
Agricultural activity	
Cemetery	
Domestic animals / wildlife	
Mineral extraction	
On-Site Agricultural Sales	
Pond/like	P

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DEVELOPMENT STANDARDS

Bulk standards of this SP are based on the zoning code standards for MLU-A. Standards not adjusted or addressed in this SP shall be regulated by MLU-A. Standards are illustrated in graphics throughout this document.

Zoning District	Min. Lot Area	Max. Density	Max FAR (see note B)	Max ISR (see note C)	Min. Rear Set-back
SP	none	doesn't apply	1.5	0.90	10 ft
Zoning District	Min. Side Set-back	Max. Height in the Build-to Zone	Min. Step-back	Max. Height	Build-to Zone (see Note D)
SP	none	4 stories in 52 feet	none	4 stories in 52 feet	3-15 on Eastland; 5-15 on Bailey

Note A: Height shall be measured finished first floor to the top of the parapet, along street frontages.

Note B: The area of any parcel dedicated as right-of-way as part of a related development process may be used to calculate FAR or density for the applicable property.

Note C: Any development that has an impervious surface ratio of less than the maximum permitted by the zoning may increase the base FAR or the base density by the same amount. For example, a nine percent decrease in ISR would permit up to a nine percent increase in FAR.
 The maximum number of residential units shall not exceed 65.
 The maximum square footage for commercial space on the ground floor is 8000 SF.

Note D: The following standards apply to the location of a building and its associated parking:

- The build-to zone shall be measured from the standard right-of-way line as established by the table entitled "Standard Street Right-of-Way Widths" in the Major and Collector Street Plan. If the existing sidewalk and planting strip, or the sidewalk and planting strip proposed by the final site plan, meet the standard of the Major and Collector Street Plan, then the build-to zone shall be 0'-15' from the standard right-of-way line.
- If a driveway from a street frontage is needed, an opening of up to twenty-six feet wide shall be permitted.

- A building shall occupy the corner of the parcel at the intersection of West Eastland Avenue and Bailey Street.
- Minimum Building Depth: 25 feet, from face of building along street frontages.
- The front façade of the building shall extend across at least fifty percent of the parcel's frontage on West Eastland.

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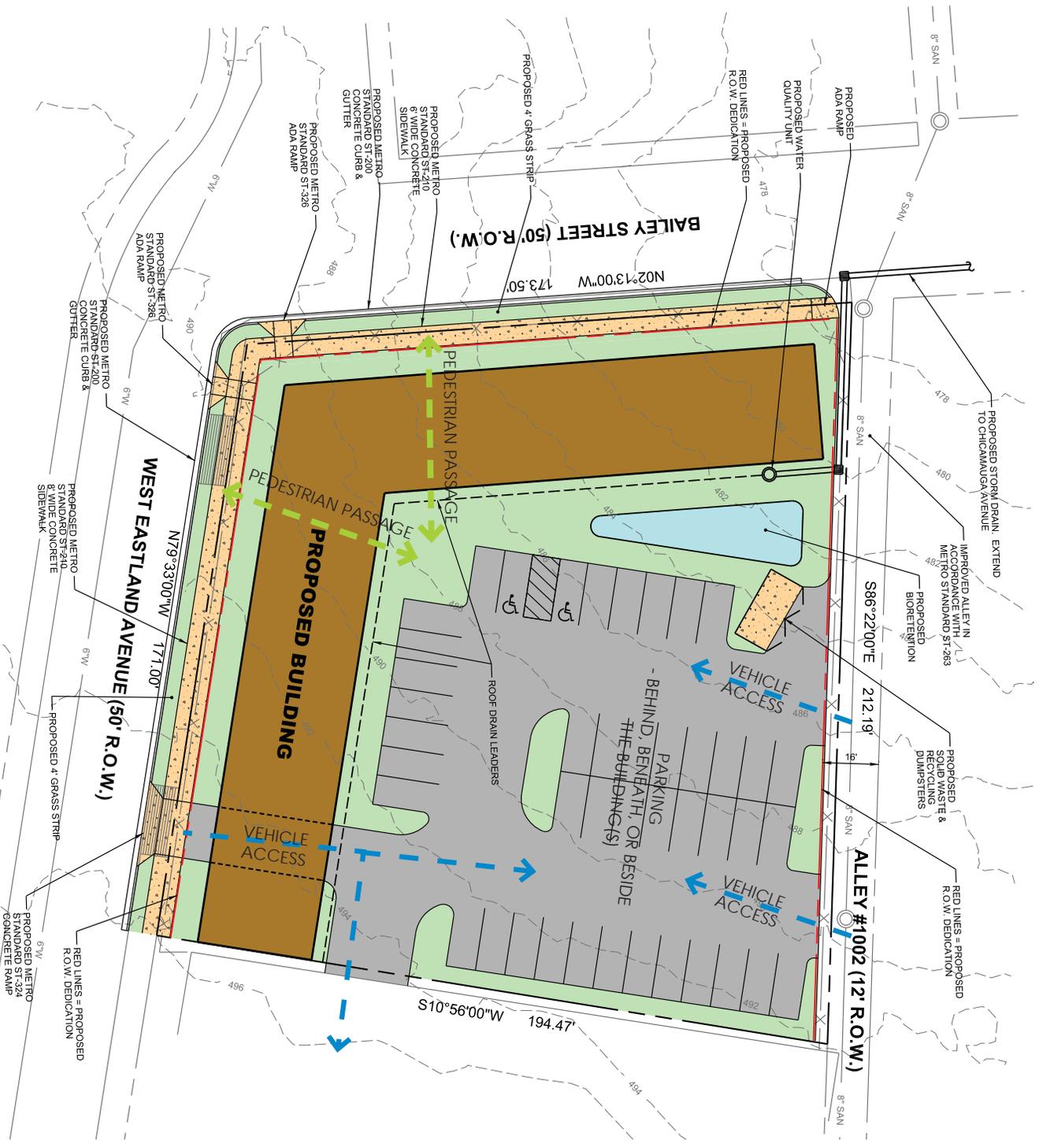
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4. Raised foundation, on street-facing facades
 - Pedestrian entrances to the building shall be located along the building façade within the build-to zone on both streets. The ground floor along West Eastland may utilize a continuous stoop for single access to all commercial space or flex space. The ground floor along Bailey Street shall have a stoop frontage with stoop access limited to no more than two units per stoop.
 - Minimum raised foundation: In order to mitigate the dramatic topography and keep the maximum raised foundation appropriately scaled, no minimum raised foundation is required.
 - Maximum raised foundation: 7'; any raised foundation higher than 3.5' above the sidewalk shall be designed with landscaping in beds raised above the sidewalk but lower than the finished first floor.
5. Glazing, on street-facing facades
 - Commercial space: 40% minimum, as measured from finished floor to finished ceiling, for the cumulative length of the commercial use.
 - Residential space: 25%, as measured from finished floor to finished ceiling, for the length of the cumulative residential use.
 - Flex space (ground floor areas that may be used for either commercial or residential over time): 25%, as measured from finished floor to finished ceiling, for the length of the cumulative flex space.
6. Parking
 - Requirements: According to the UZO standards.
 - Shared Parking: According to the zoning code standards.
 - Location: Parking shall be permitted behind, beneath, or beside buildings.
 - Landscaping: According to the zoning code standards.
7. Access
 - Primary vehicular access shall be provided on the alley.
 - In order to allow continued access to the adjacent parking structure from West Eastland, a curb cut shall remain on West Eastland.
8. Buffering
 - Buffering requirements between MUL-A and MUG-A shall be waived.
9. Street-Facing Facade Articulation
 - Street-facing facades shall be articulated in a manner consistent with the illustrative examples shown on pages 13-16. Tools for articulation will include at least one of the following but preferably all: changes in plane, material, or finish. Unarticulated massing shall not exceed 40' in width. In addition, the building facade cannot exceed 60' in width without a change in plane or vertical break.

DEVELOPMENT PROPOSAL



The site plan illustrates the standards of this document.

As the design of the project continues to progress, the owner may make adjustments to items such as building footprint, building height, or other items necessitated by codes, structural, mechanical or electrical systems, provided that such adjustments are done within the standards of this SP. The developer will coordinate such adjustments with Planning staff and other agencies as necessary.

If adjustments to the SP are necessary to accommodate building code requirements and other regulations,

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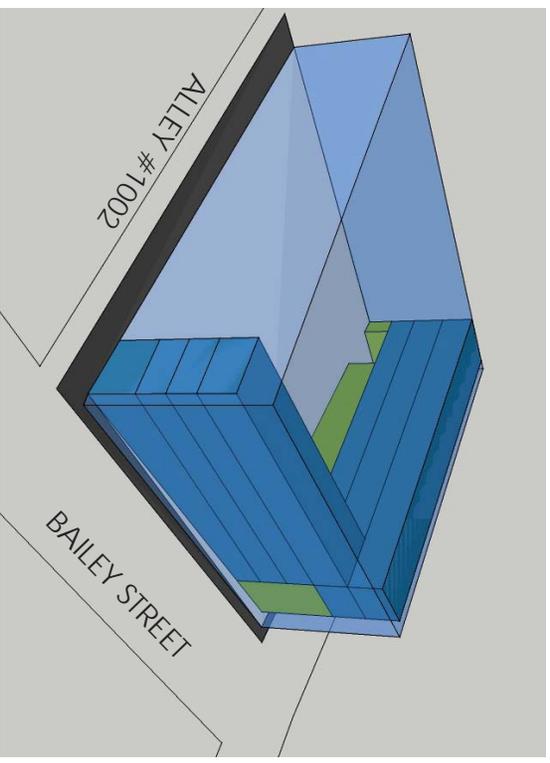
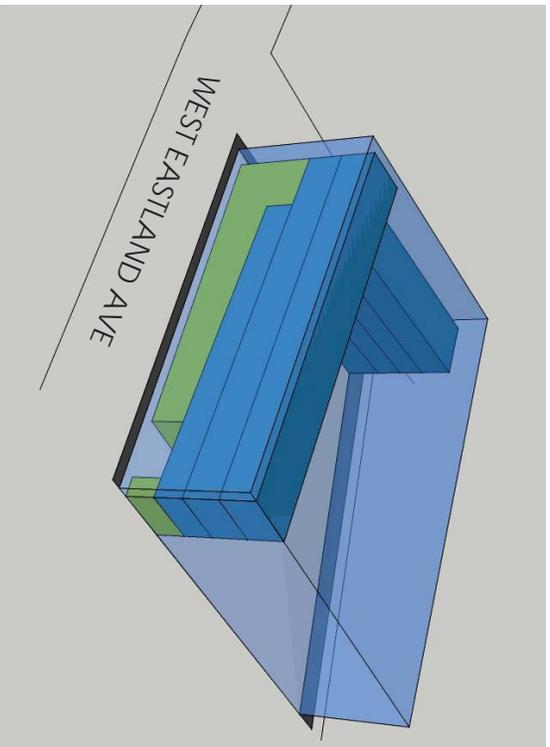
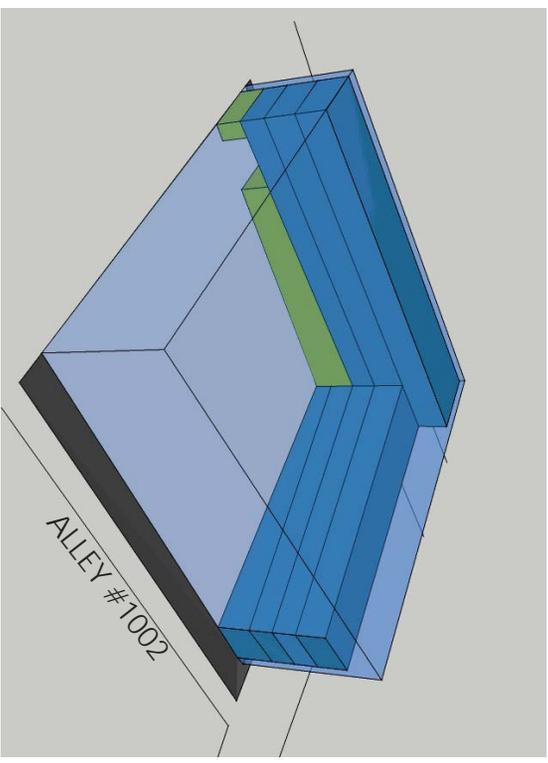
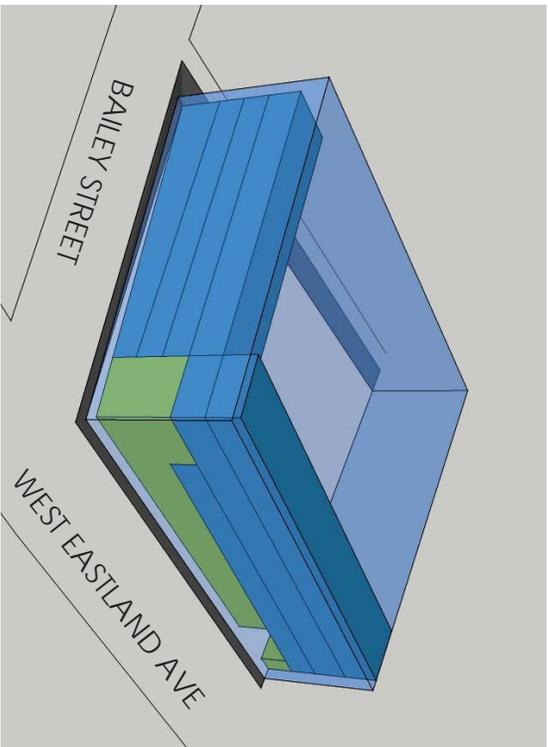
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PROPOSED MASSING

the owner will coordinate such adjustments with Planning staff and other agencies as necessary.

Landscape and hardscape improvements illustrated herein beyond the building facade and those within the public right of way are conceptual and will require coordination with Metro Public Works and other agencies having jurisdiction over the project. Revisions to such, whether due to unforeseen conditions, existing conditions, or basic development of design intent, shall be reviewed by Planning staff to ensure consistency with the intent of the SP.



The massing model illustrates the standards of this document.

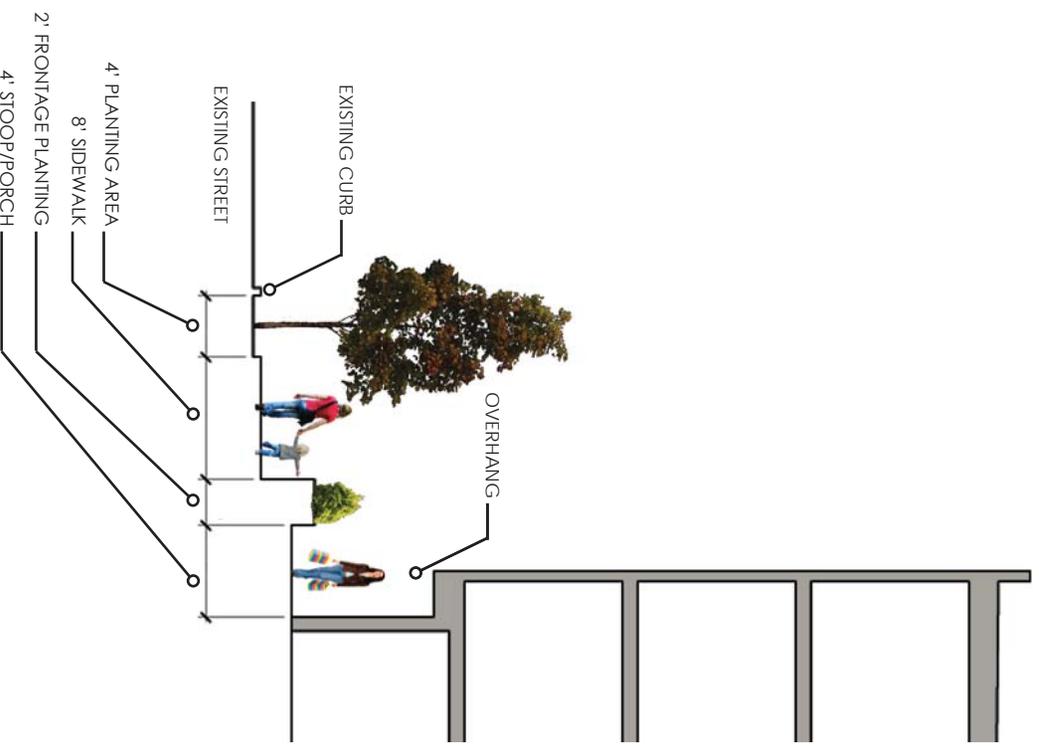
R I G H T - O F - W A Y C O M P L I A N C E

Compliance with the Major and Collector Street Plan and other Metro Public Works standards provides the site with needed auto and pedestrian infrastructure. The means of proposed compliance with these standards is outlined below. Should adjustments to the proposal for compliance be necessary as the design progresses, the owner will coordinate such adjustments with Planning staff and other agencies as necessary.

West Eastland, T4-M-CA-2:

Required Minimum:

- Keep existing curb location.
- Add street trees in a 4' planting area.
- Add an 8' sidewalk.
- Include a front yard zone as the frontage zone. The frontage zone shall include a planting zone (min. 2'), and a stoop/porch (min. 4') to transition along the grade change.
- If an overhang is used as an architectural element, the maximum dimension shall be 2'.



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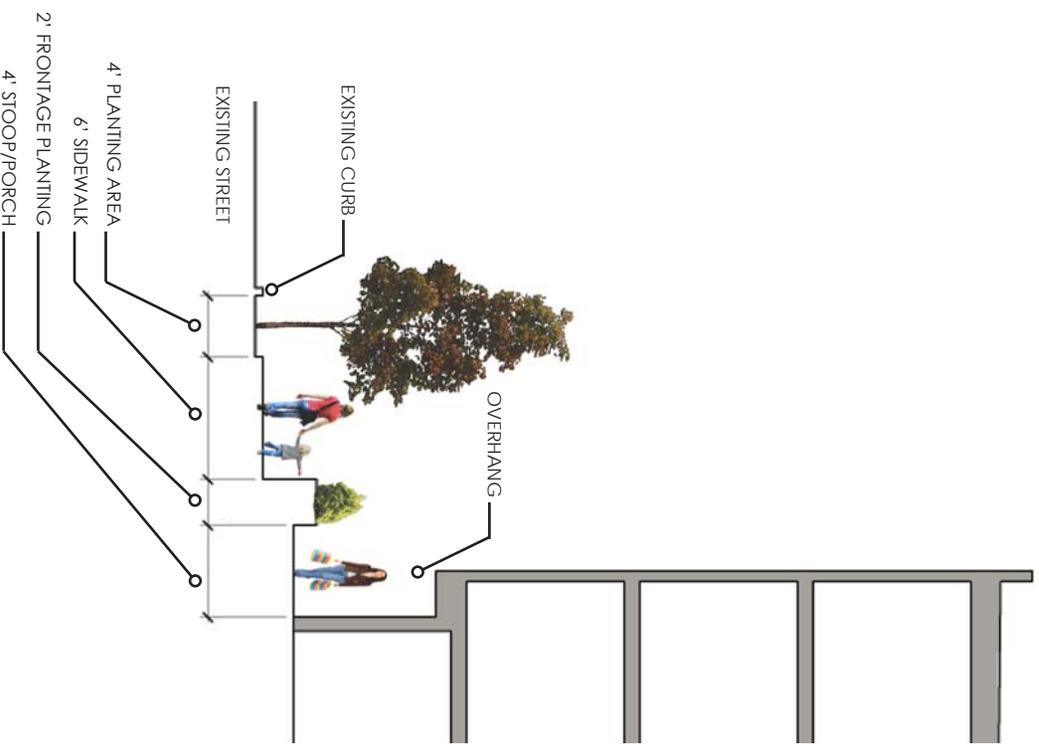
Bailey Street, Local Street:

Required Minimum:

- Keep existing curb location.
- Add street trees in a 4' planting area.
- Add a 6' sidewalk.
- Include a front yard zone as the frontage zone. The frontage zone shall include a planting zone (min. 2'), and a stoop/porch (min. 4') to transition along the grade change.
- If an overhang is used as an architectural element, the maximum dimension shall be 2'.

Alley:

- The currently alley is approximately 12'.
- Proposed: Dedicate 10' of ROW from centerline and improve to MPW standards



ILLUSTRATIVE PHOTOS

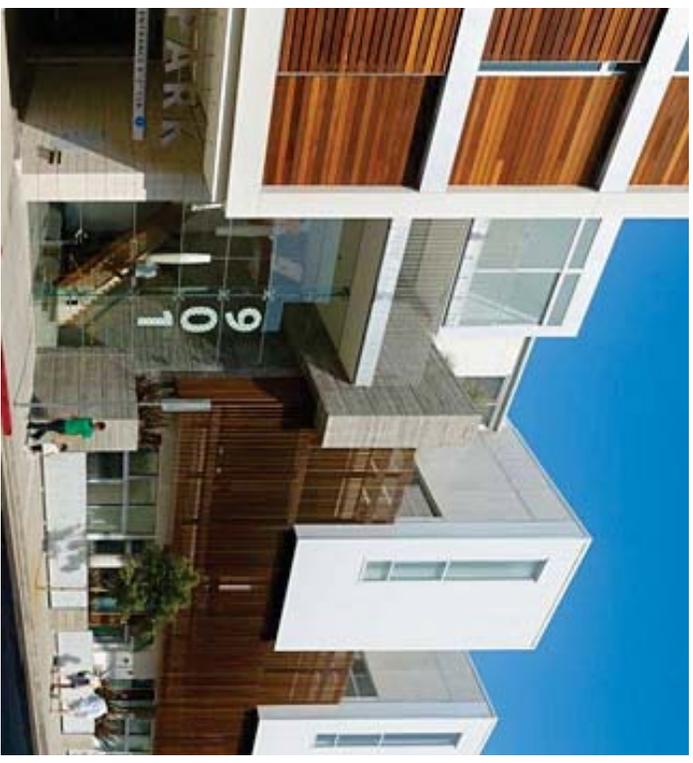
These photos show the intent of particular characteristics of the proposed development - as described in the caption of each photo. These photos are for illustrative purposes only - to indicate a commitment to neighborhood-scaled commercial and multi-family residential on this transitional site. The SP limits height to 4 stories. Photos of buildings with heights greater than 4 stories are used to illustrate important design concepts, not height.



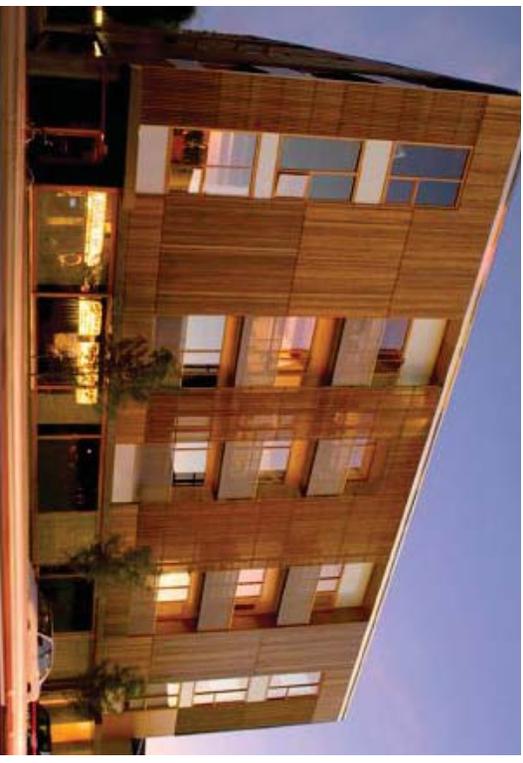
A wide sidewalk, street trees, and landscaping will be provided on the site and in the right-of-way adjacent to the site.



The uses along Bailey Street are limited to residential. The articulation of the facade will reflect the residential use. Pedestrian access to and through the building will be provided on Bailey Street.



The ground floor on West Eastland will be active with residential and/or commercial uses, will contribute positively to the street life, and will create a transition between the commercial/mixed-use character of Gallatin Road and the residential neighborhood.



The architecture of the building will be of its time, with materials and facade articulation that respond to the unique location of the site - the transition point between the adjacent neighborhood and Gallatin Road.

ILLUSTRATIVE PHOTOS

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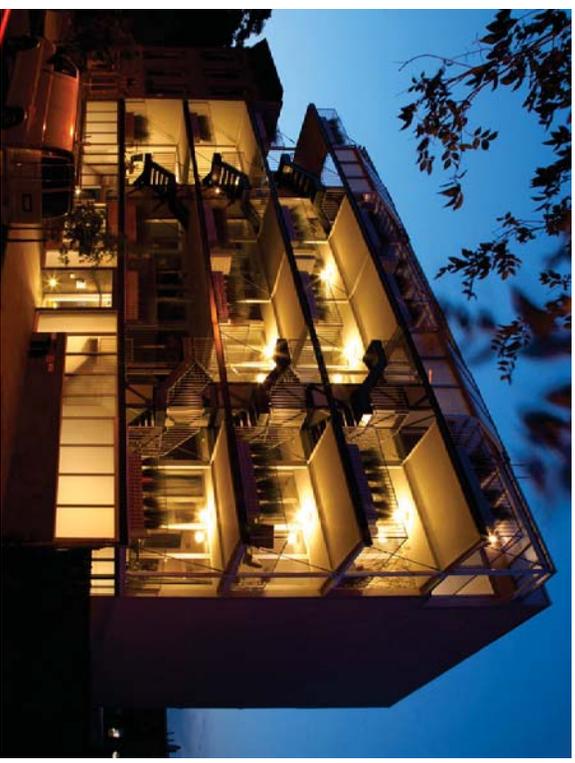
The facades of the building will respond to the existing and envisioned context on both West Eastland and Bailey Street.



The building will utilize private and public open space as an organizing tool and design element.



Non-street facing facades: the exterior spaces of the building may be used for circulation, and private or public gathering spaces.



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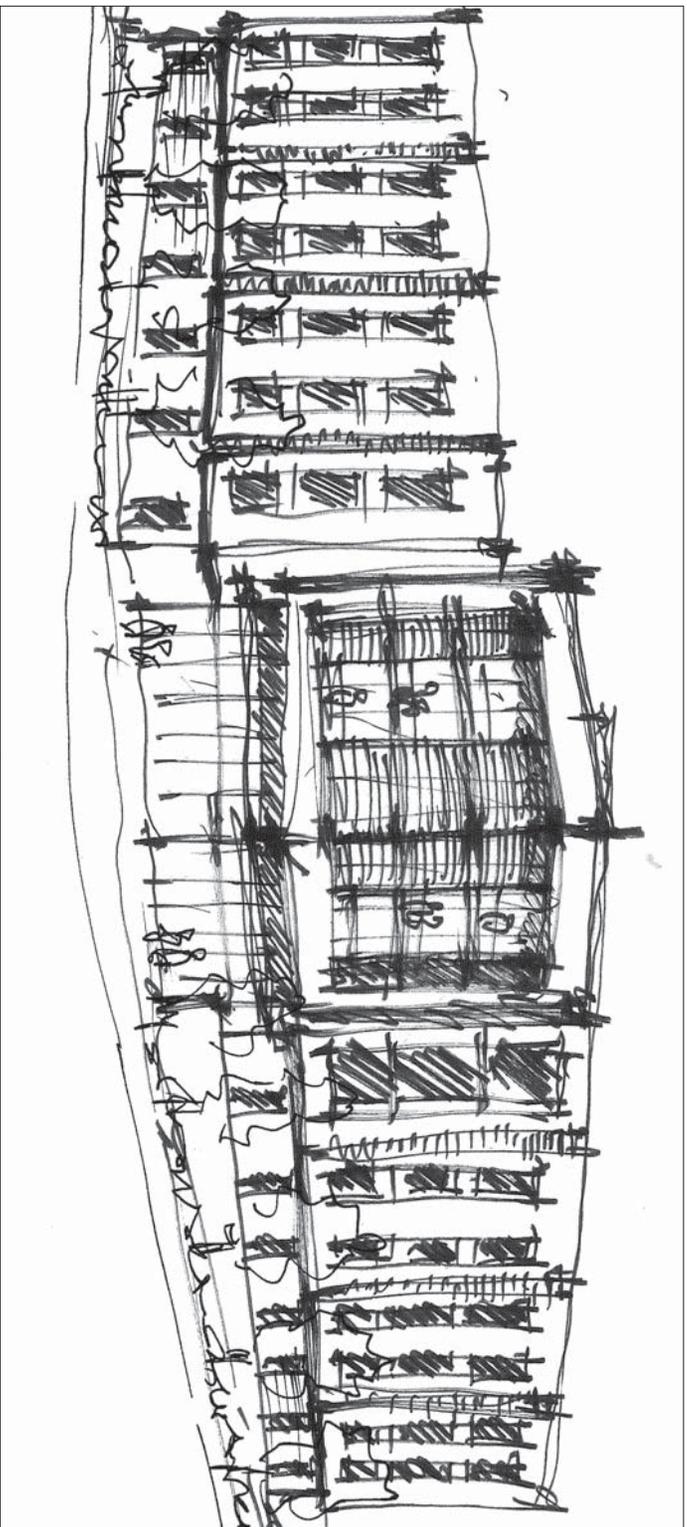
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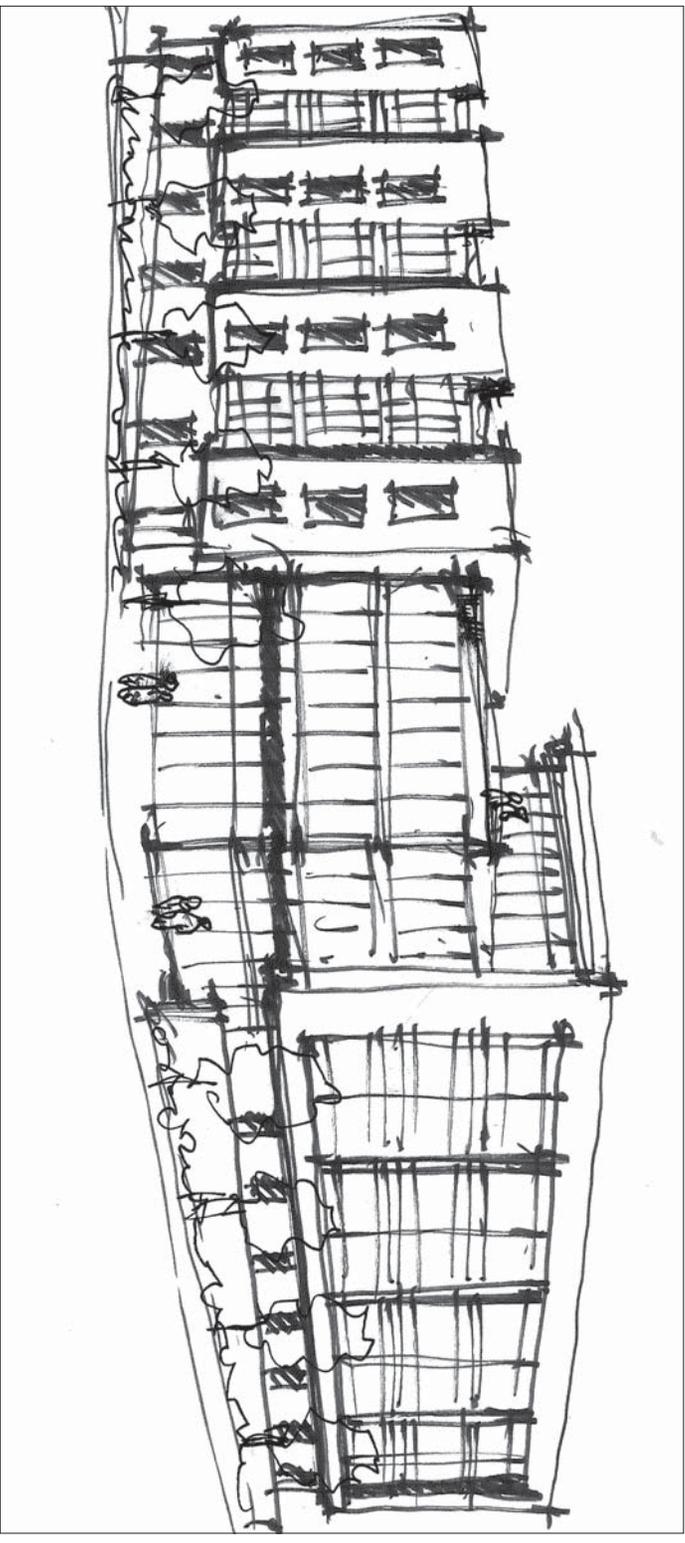
ILLUSTRATIVE SKETCHES

These sketches show the potential character of the architecture of the building, and are for illustrative purposes only. The sketches indicate a commitment to neighborhood-scaled commercial and multi-family residential on this transitional site, and show potential articulations of the building massing.



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