



# GALLATIN PIKE



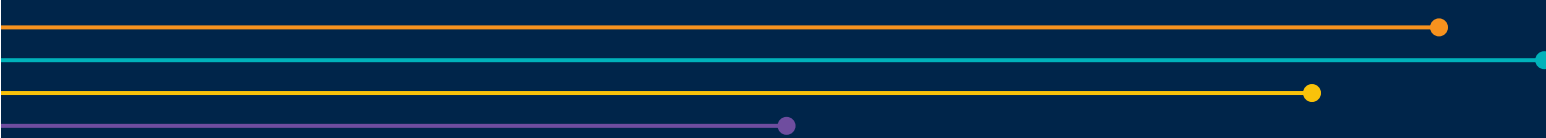
# & MAIN STREET

A NASHVILLE VISION ZERO INITIATIVE

Four horizontal lines of different colors (orange, teal, yellow, purple) with small circular dots at their right ends, extending across the bottom of the page.

Summary of December 2023/January 2024 Public Outreach | February 2<sup>nd</sup>, 2024

Note: This summary presents the results of input received during public engagement activities for the Gallatin Pike and Main Street Vision Plan in December 2023 and January 2024. The results are not scientific but are important and will be used in combination with other data and analysis to help inform the study recommendations.



# Four opportunities

1. December 6<sup>th</sup> public workshop
  - East Library
  - Noon to 1:30 PM
2. December 14<sup>th</sup> public workshop
  - East Park Community Center
  - 5 to 6:30 PM



# Four opportunities

## 3. WeGo rider outreach

- January 9<sup>th</sup> and 11<sup>th</sup> at WeGo Central
- January 10<sup>th</sup> on Route 56 on Gallatin Pike

## 4. Online



# Input activities

- Build Your Own Street
- Priority Pyramid
- Map Comments



# Build Your Own Street

- December 6<sup>th</sup> workshop: 23 responses
- December 14<sup>th</sup> workshop: 23 responses
- Online (StreetMix): 26 responses



## Build Your Own Street on Gallatin Pike

Step 1: Go to <https://streetmix.net/~2390286>

Step 4: Click "Share" to save your street as a JPG file and send via email it to [NDOTCommunications@nashville.gov](mailto:NDOTCommunications@nashville.gov).

Step 2: Delete street elements that you want to remove from Gallatin Pike.

Step 3: Drag and drop the street elements that you want to see on Gallatin Pike. Be sure to stay within the existing pavement width.

Want to go farther?

Try these sections:  
Main St. between 5<sup>th</sup> St. and 10<sup>th</sup> St. > [link](#)  
Gallatin Pike from Hart Ln. to Briley Pkwy. > [link](#)

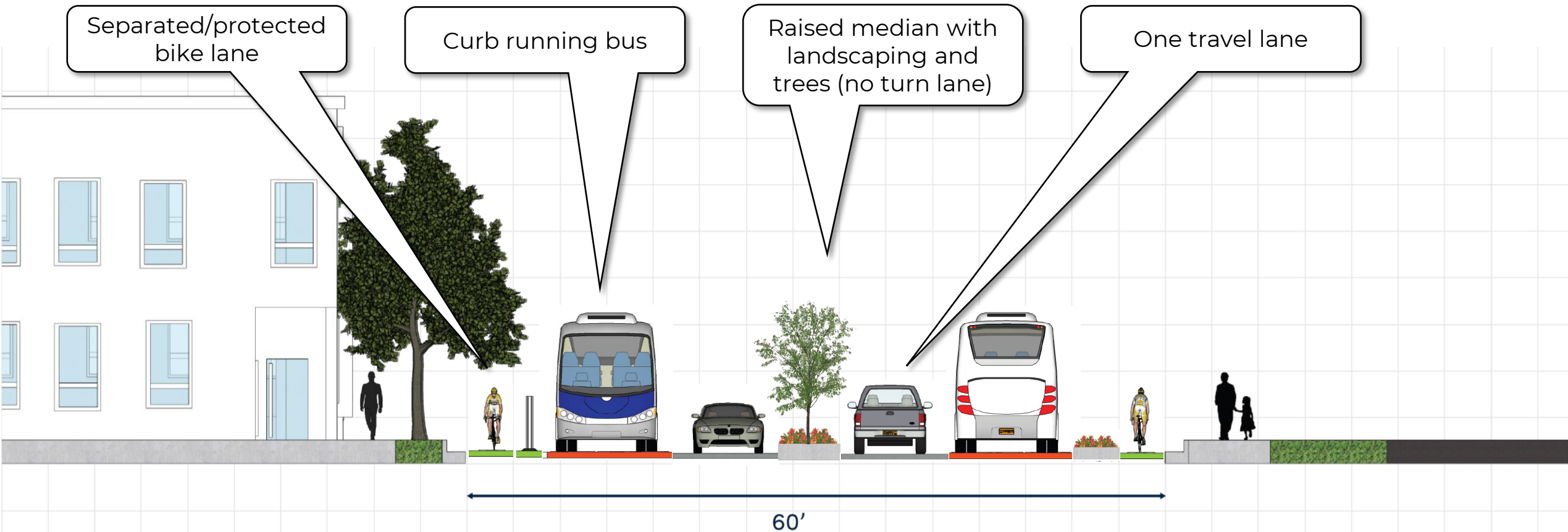


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# Main Street typical design

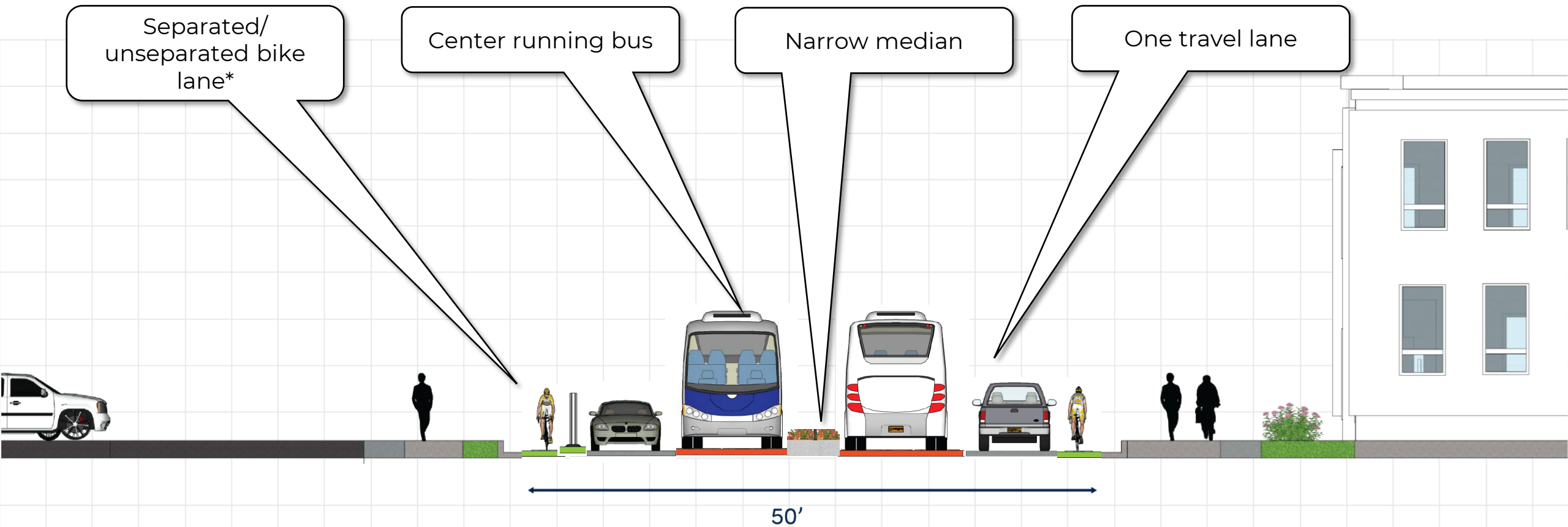
32  
responses





# 10<sup>th</sup> Street to Hart Lane typical design

25  
responses



\* Lots of "cheating."

# Hart Lane to Briley Parkway typical design

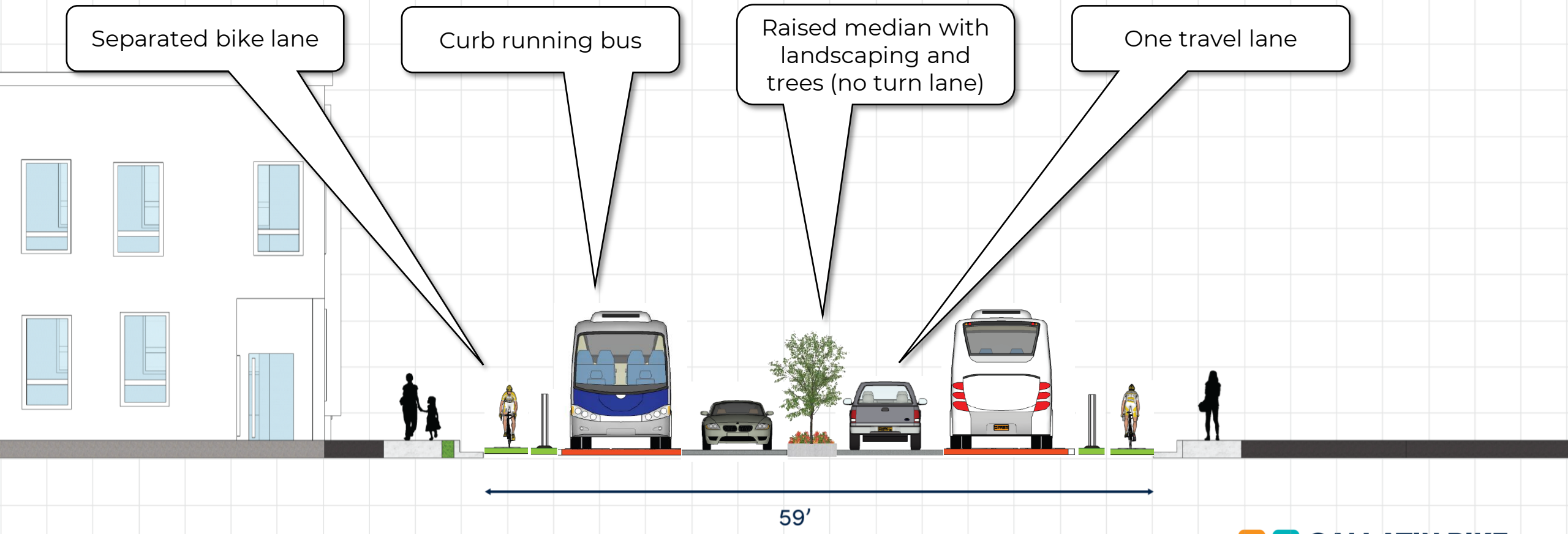
15  
responses

Separated bike lane

Curb running bus

Raised median with  
landscaping and  
trees (no turn lane)

One travel lane



59'

# Key findings

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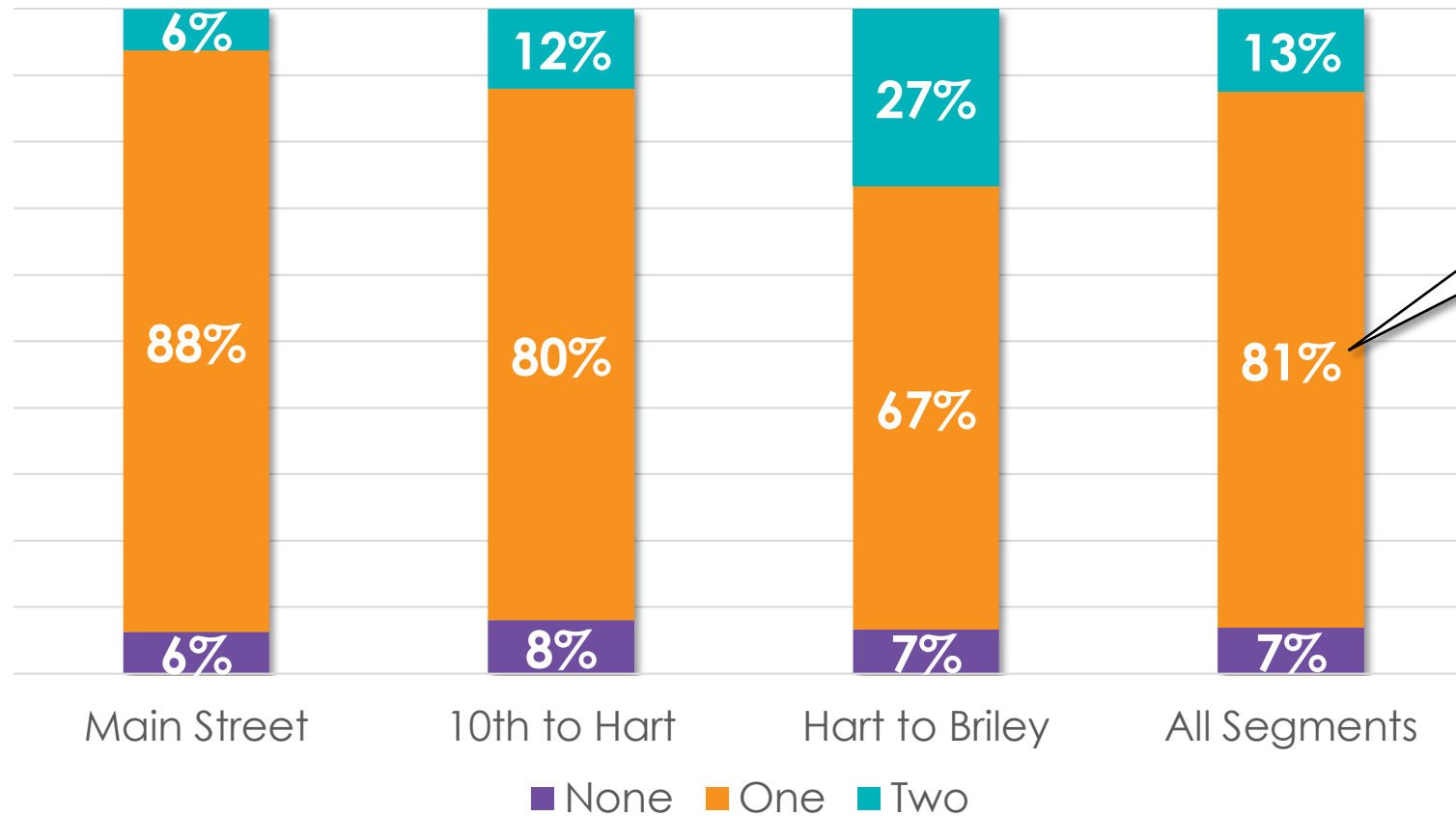
- Near unanimous agreement on lane reduction
- Most include bus lanes
  - Slight majority favor curb running
- Seven out of 10 responses include a bike lane

# Key findings

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- Half include both bike and bus lanes
  - Curb + bike is most popular
- Almost all include median separation (but no turn lane)
- Almost all include trees or landscaping in some form

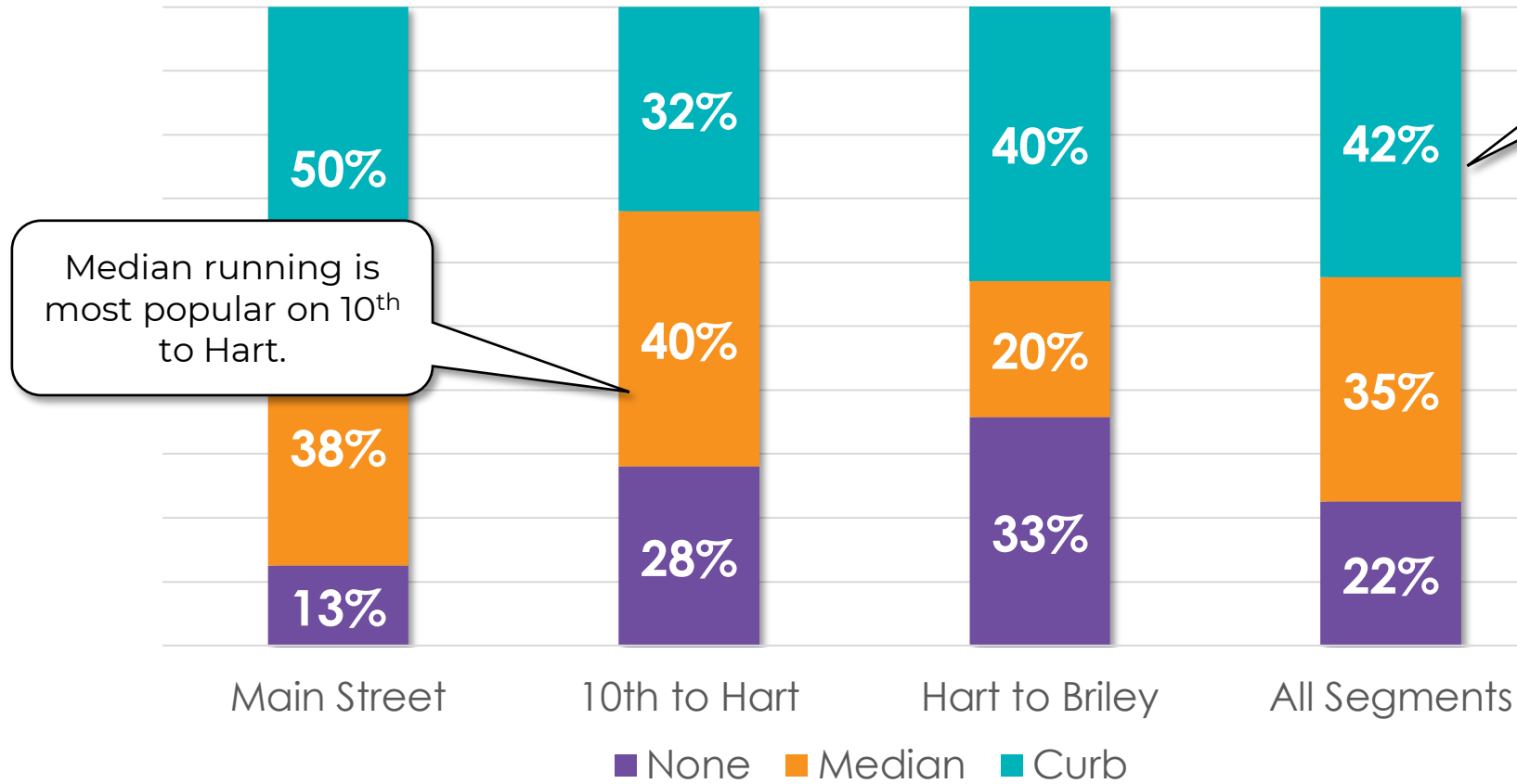
# Motor vehicle lanes



63 out of 72 responses reduced the number of motor vehicle lanes.



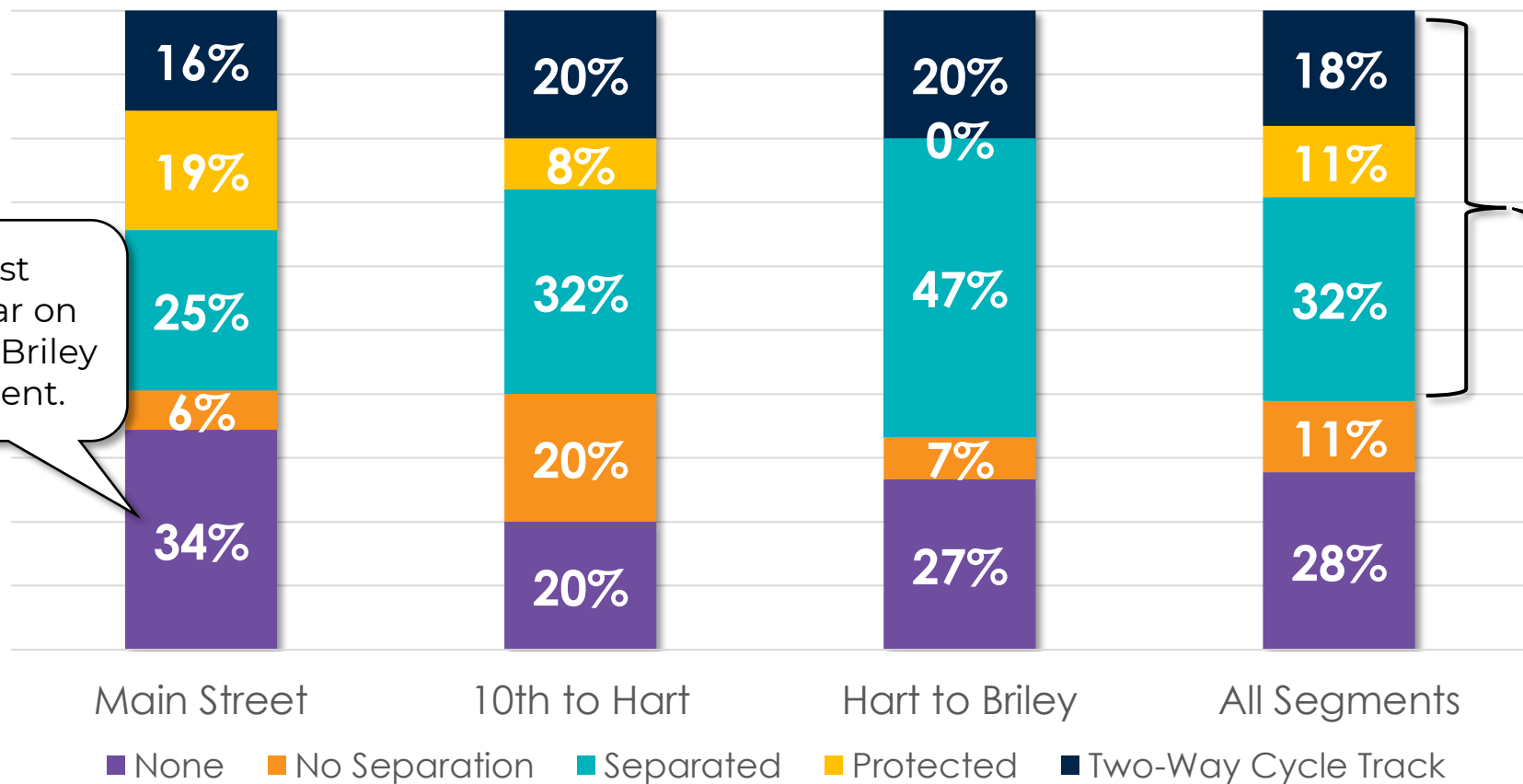
# Bus lanes



Slight majority favor curb running bus lanes.



# Bike lanes



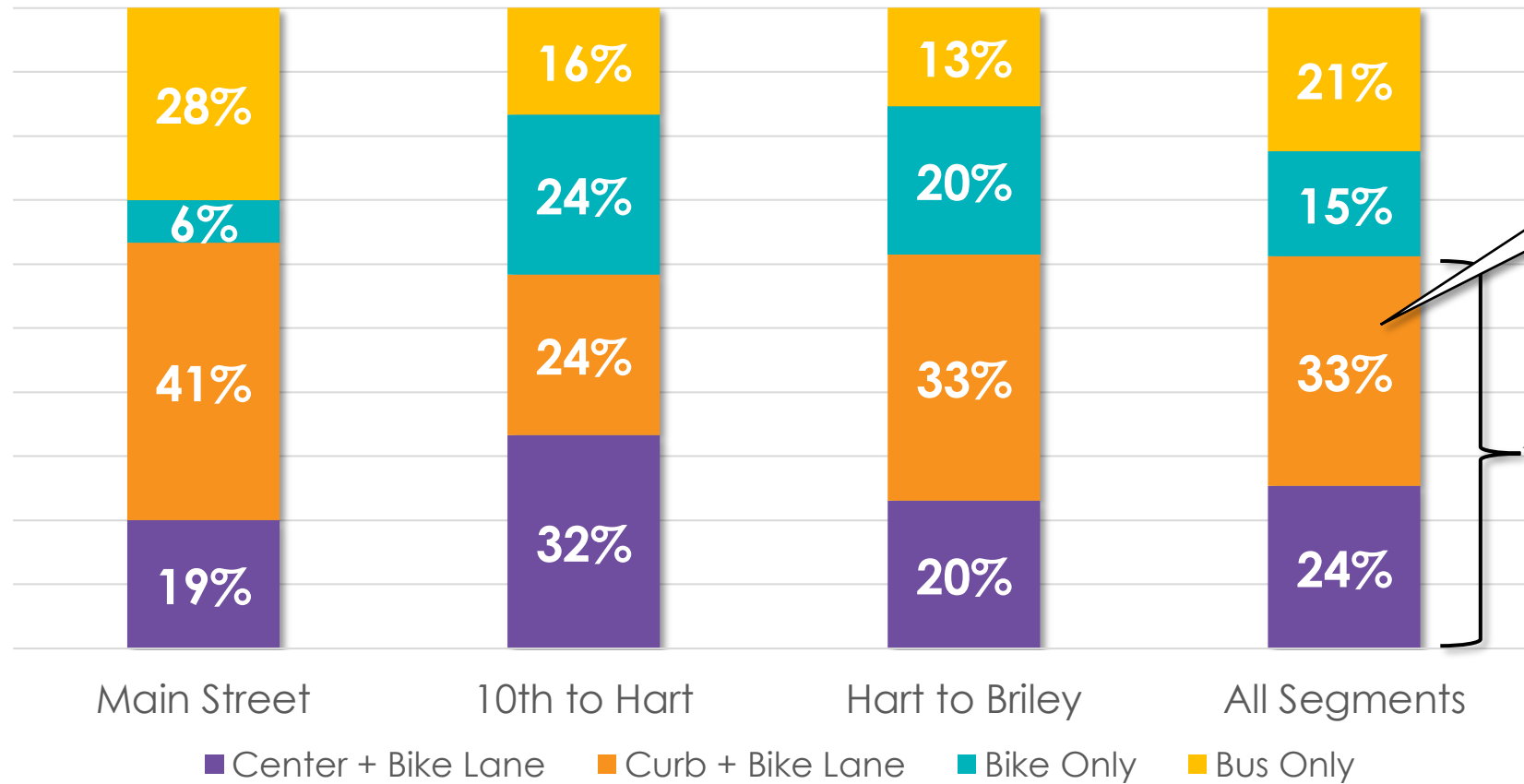
Least popular on Hart to Briley segment.

About 8 out of 10 responses include a bike facility.

Most include some form of separation/protection.

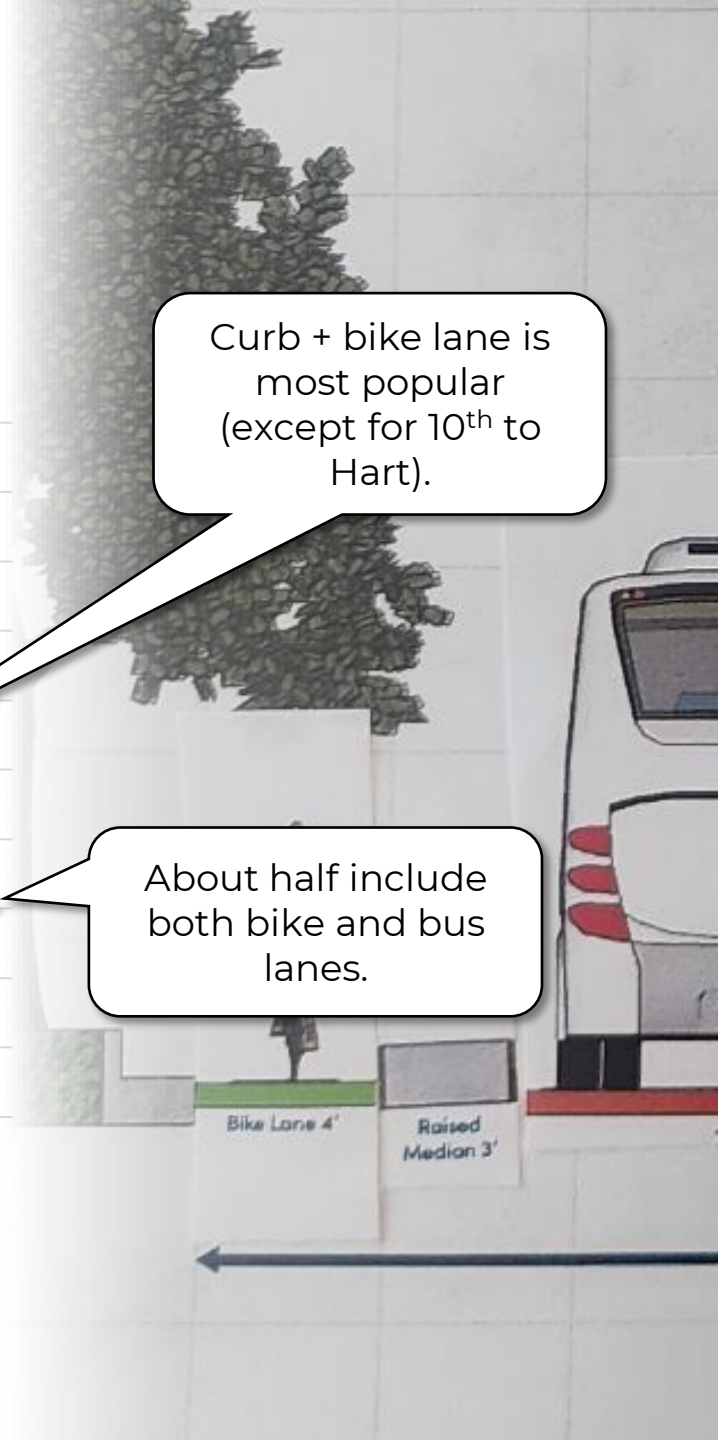


# Bike lanes + bus lanes



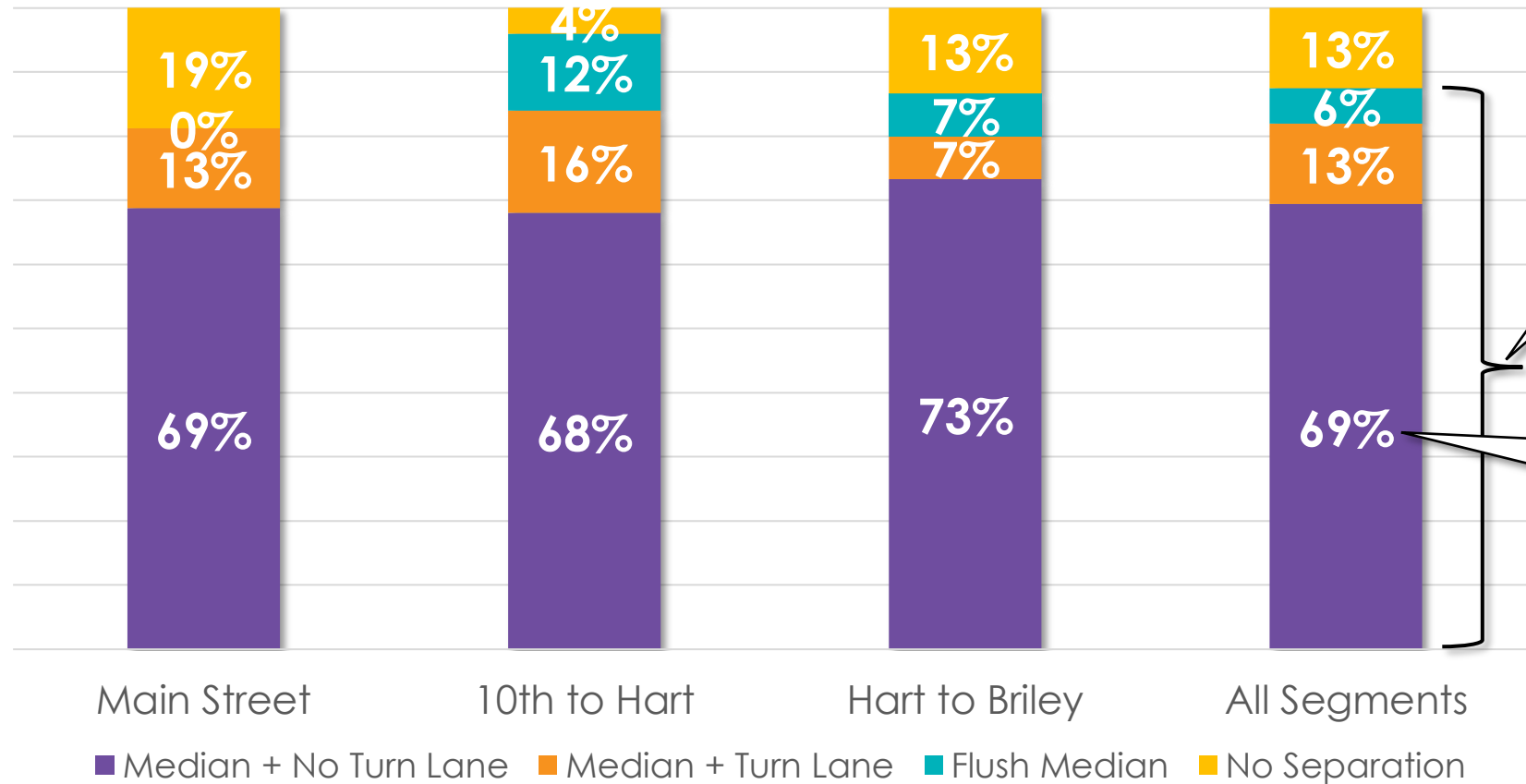
Curb + bike lane is most popular (except for 10<sup>th</sup> to Hart).

About half include both bike and bus lanes.



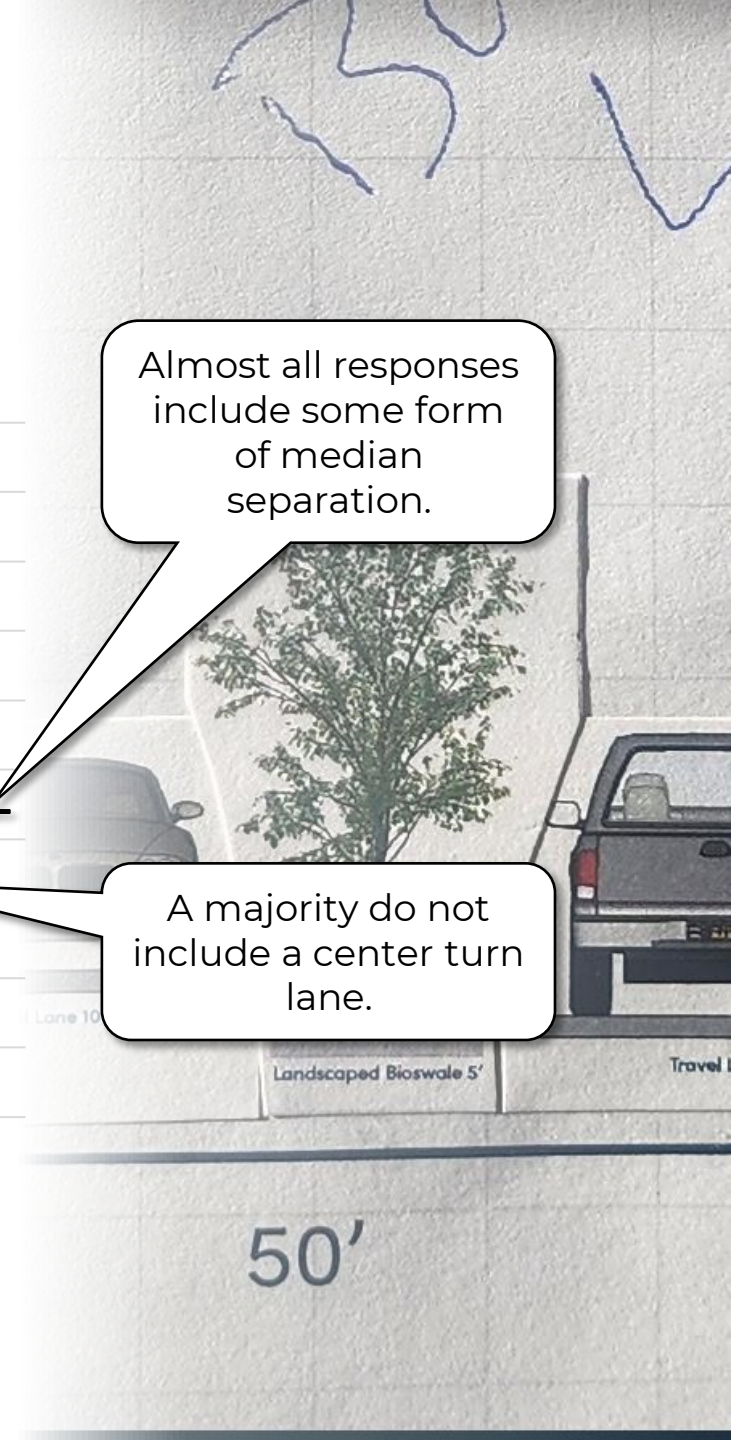


# Median separation

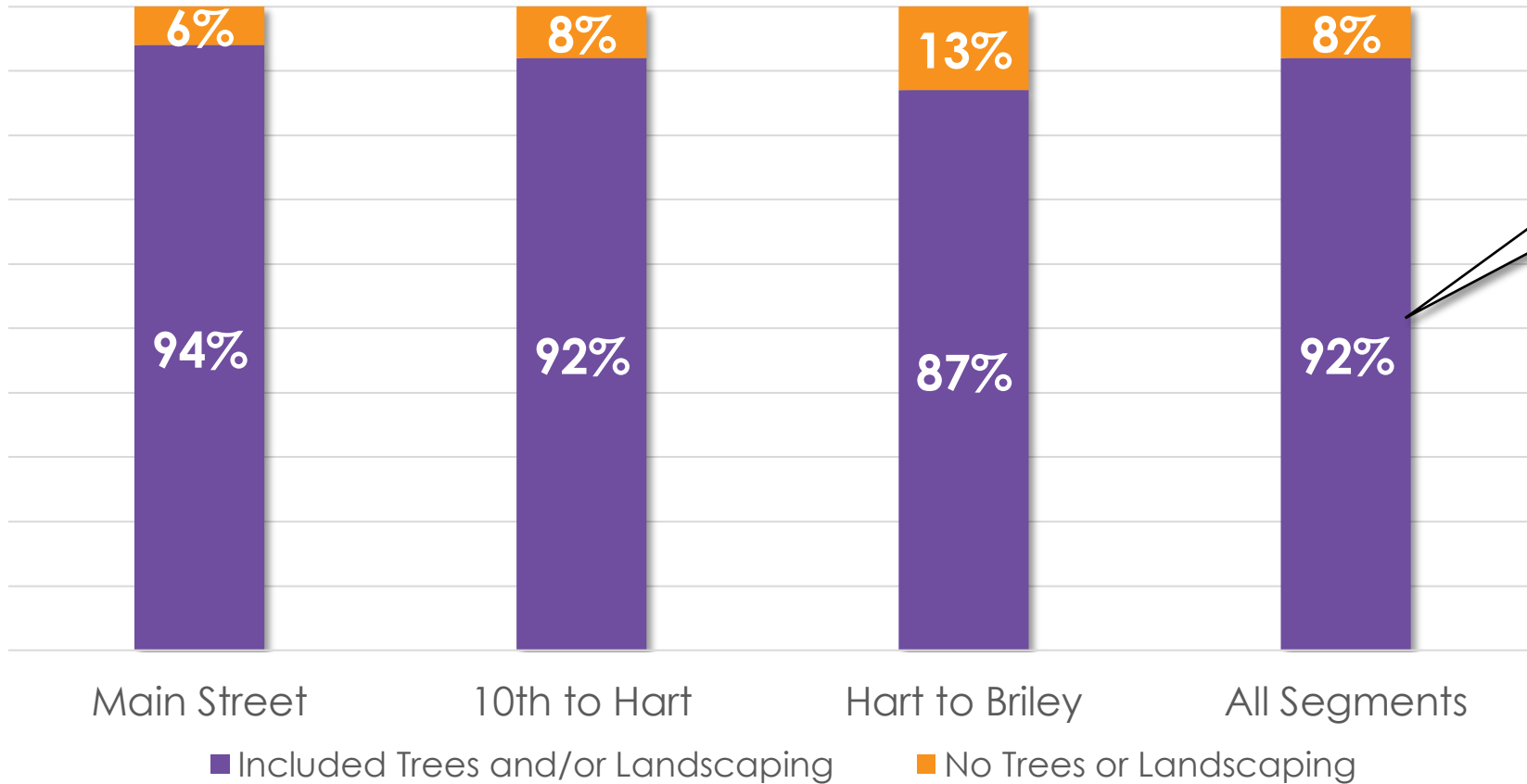


Almost all responses include some form of median separation.

A majority do not include a center turn lane.



# Trees and landscaping

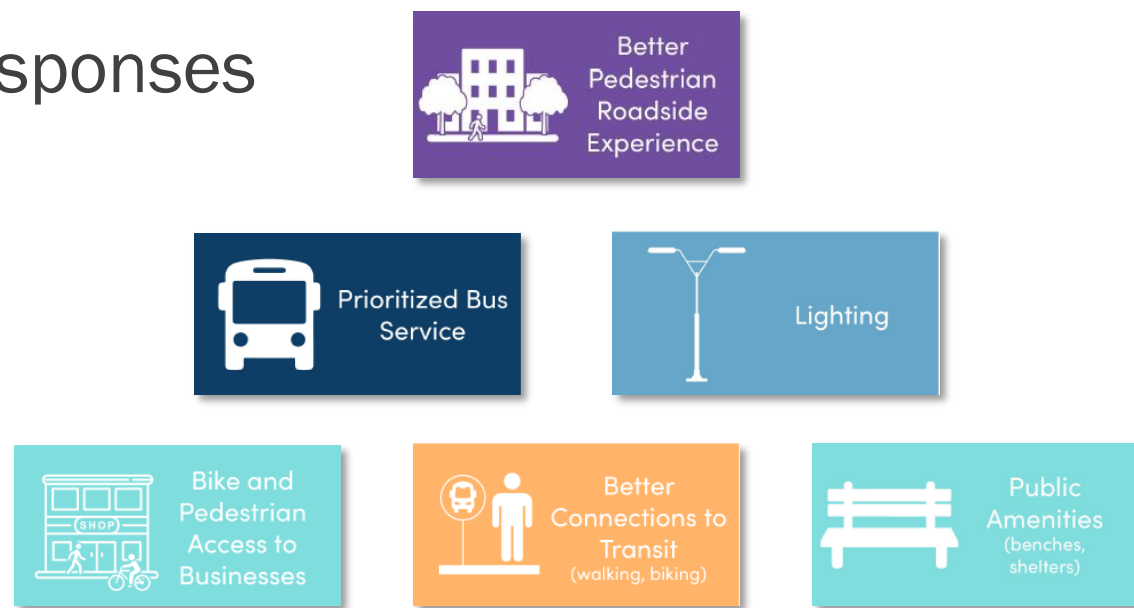


Most include trees or landscaping in some form.



# Priority Pyramid

- December 6<sup>th</sup> workshop: 39 responses
- December 14<sup>th</sup> workshop: 27 responses
- Online: 785 responses to date



# Composite Pyramid



# Key findings

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- Better crossing opportunities is first across the board
  - Most appearances within priority ranking list
  - Most Priority 1 selections
  - Highest weighted score
- Pedestrian priorities are most prominent
- Beautification is next most prominent

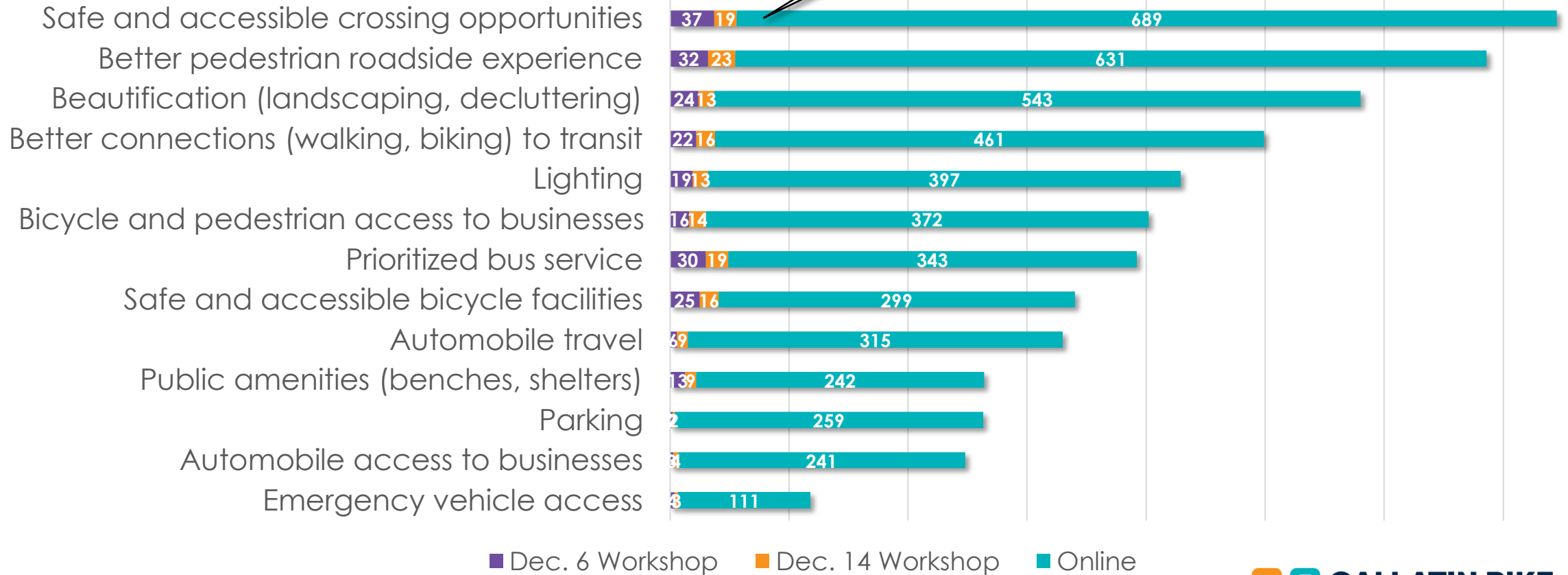
# Key findings

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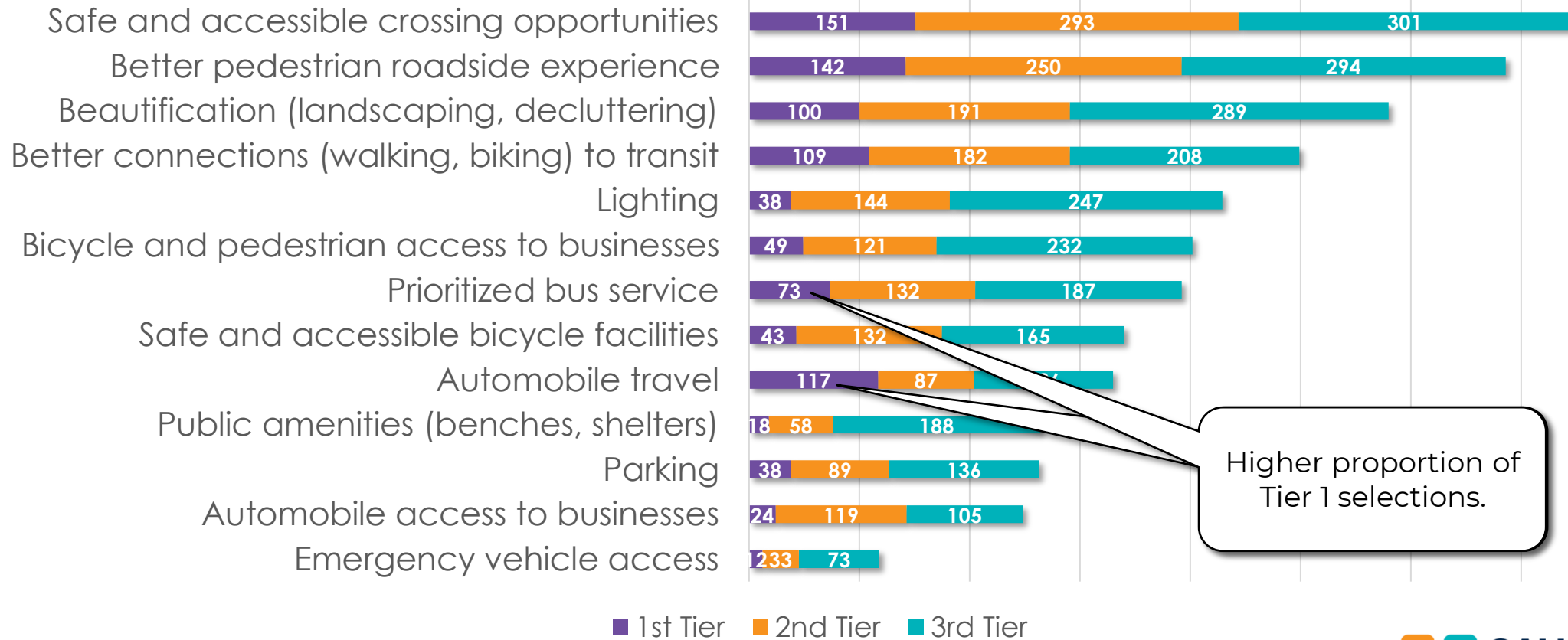
- More important in-person vs. online
  - Transit prioritization
  - Bike facilities
- More important online vs. in-person
  - Auto travel
  - Parking
  - Auto access

# Total appearances

Most appearances and Tier 1 selections.

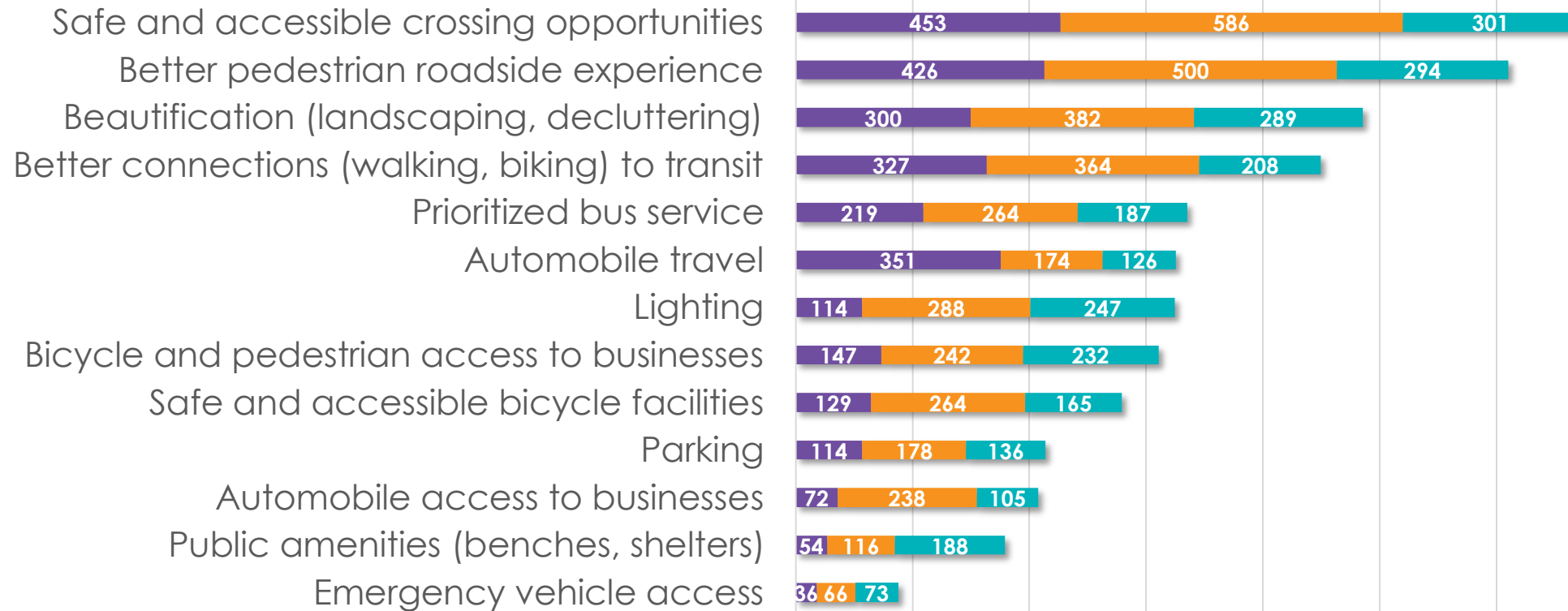


# Total appearances by tier





# Weighted score\*

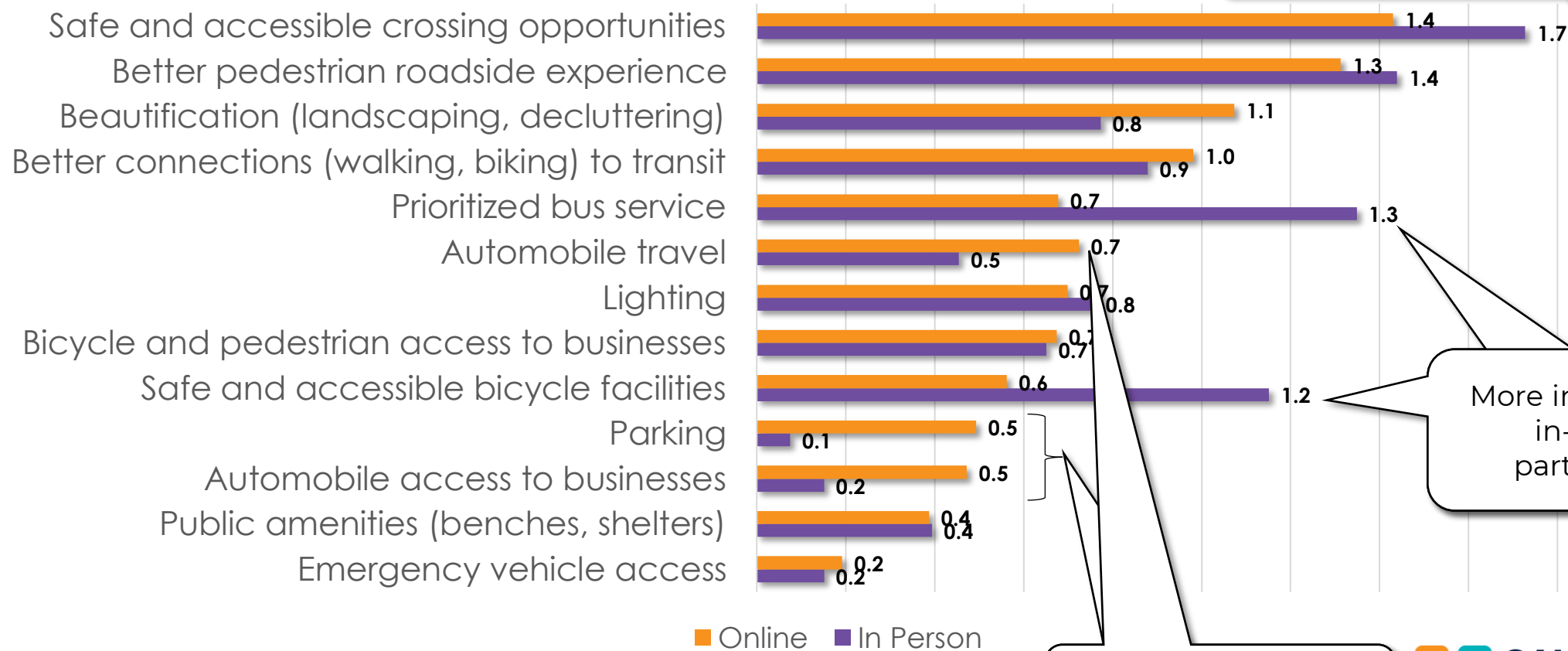


Ped-focused priorities are most prominent. Clear # 1 and 2

■ 1st Tier ■ 2nd Tier ■ 3rd Tier

\* (Tier 1 selections x 3) + (Tier 2 selection x 2) + (Tier 3 selections x 1) = Weighted Score

# Weighted score\*: In-person vs. online



Although a priority for both, in-person still scores higher for ped/bike

More important to in-person participants.

More important to online participants.

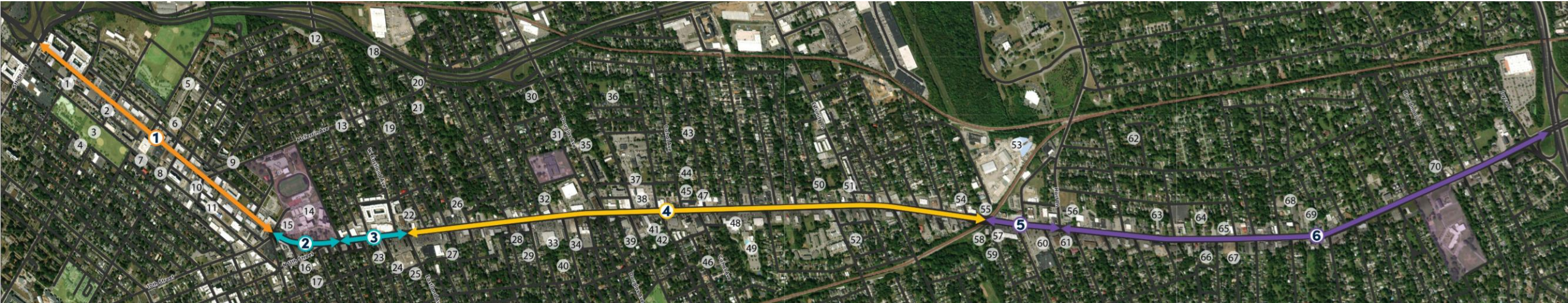
\* Normalized by the total number of responses.

# Map comments

- Open ended feedback
- Sticky notes



# Map comments



# Map comments by category

