

MEGAN BARRY
MAYOR



METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

Metropolitan Historic Zoning Commission
Sunnyside in Sevier Park
3000 Granny White Pike
Nashville, Tennessee 37204
Telephone: (615) 862-7970
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STAFF RECOMMENDATION

106 Lindsley Park Drive

May 16, 2018

Application: New construction - infill

District: Lockeland Springs-East End Neighborhood Conservation Zoning Overlay

Council District: 06

Map and Parcel Number: 08309044900

Applicant: Nathan Weinberg

Project Lead: Melissa Baldock, melissa.baldock@nashville.gov

Description of Project: Application is to construct a new single-family residence on a vacant lot.

Recommendation Summary: Staff recommends approval with the following conditions:

1. The finished floor height be consistent with the finished floor heights of the nearby historic houses, to be verified by MHZC staff in the field;
2. Staff approve a brick sample;
3. Staff approve a stone sample;
4. Staff approve the walkway and driveway materials;
5. The front door be a minimum of half glass, and staff approve the final details, dimensions and materials of all windows and doors;
6. Staff approve the roof shingle color and texture;
7. The HVAC be located behind the house or on either side, beyond the mid-point of the house.

With these conditions, staff finds that the project meets Section II.B. of the *Lockeland Springs-East End Neighborhood Conservation District: Handbook and Design Guidelines*.

Attachments

A: Photographs

B: Site Plan

C: Elevations

Applicable Design Guidelines:

II.B. New Construction

1. Height

New buildings must be constructed to the same number of stories and to a height which is compatible with the height of adjacent buildings.

The height of the foundation wall, porch roof, and main roofs should all be compatible with those of surrounding historic buildings.

For those lots located within the Residential Subdistrict of the Five Points Redevelopment District shall not exceed 3 stories .

2. Scale

The size of a new building and its mass in relation to open spaces; and its windows, doors, openings, and porches should be visually compatible with surrounding historic buildings.

Foundation lines should be visually distinct from the predominant exterior wall material. This is typically accomplished with a change in material.

3. Setback and Rhythm of Spacing

4. Since construction in an historic district has usually taken place continuously from the late nineteenth and early twentieth centuries, a variety of building types and styles result which demonstrate the changes in building tastes and technology over the years. New buildings should continue this tradition while complementing and being compatible with other buildings in the area.

In Lockeland Springs-East End, historic buildings were constructed between 1880 and 1950. New buildings should be compatible with surrounding houses from this period.

5. Reconstruction may be appropriate when it reproduces facades of a building which no longer exists and which was located in the historic district if: (1) the building would have contributed to the historical and architectural character of the area; (2) if it will be compatible in terms of style, height, scale, massing, and materials with the buildings immediately surrounding the lot on which the reproduction will be built; and (3) if it is accurately based on pictorial documentation.
6. Because new buildings usually relate to an established pattern and rhythm of existing buildings, both on the same and opposite sides of a street, the dominance of that pattern and rhythm must be respected and not disrupted.
7. New construction should be consistent with existing buildings along a street in terms of height, scale, setback, and rhythm; relationship of materials, texture, details, and color; roof shape; orientation; and proportion and rhythm of openings.

The setback from front and side yard property lines established by adjacent historic buildings must be maintained. When a definite rhythm along a street is established by uniform lot and building width, infill new buildings should maintain that rhythm.

The Commission has the ability to reduce building setbacks and extend height limitations of the required underlying base zoning for new construction, additions and accessory structures (ordinance no.

17.40.410).

Appropriate setback reductions will be determined based on:

- The existing setback of the contributing primary buildings and accessory structures found in the immediate vicinity;*
- Setbacks of like structures historically found on the site as determined by historic maps, site plans or photographs;*
- Shape of lot;*
- Alley access or lack thereof;*
- Proximity of adjoining structures; and*
- Property lines.*

Appropriate height limitations will be based on:

- Heights of historic buildings in the immediate vicinity*
- Existing or planned slope and grade*

Infill construction on the 1400 - 1600 blocks of Boscobel Street may have widths up to 40'.

4. Relationship of Materials, Textures, Details, and Material Colors

The relationship and use of materials, textures, details, and material color of a new building's public facades shall be visually compatible with and similar to those of adjacent buildings, or shall not contrast conspicuously.

T-1-11- type building panels, "permastone", E.F.I.S. and other artificial siding materials are generally not appropriate. However, pre-cast stone and cement fiberboard siding are approvable cladding materials for new construction; but pre-cast stone should be of a compatible color and texture to existing historic stone clad structures in the district; and cement fiberboard siding, when used for lapped siding, should be smooth and not stamped or embossed and have a maximum of a 5" reveal.

Shingle siding should exhibit a straight-line course pattern and exhibit a maximum exposure of seven inches (7").

Four inch (4") nominal corner boards are required at the face of each exposed corner.

Stud wall lumber and embossed wood grain are prohibited.

Belt courses or a change in materials from one story to another are often encouraged for large two-story buildings to break up the massing.

When different materials are used, it is most appropriate to have the change happen at floor lines.

Clapboard sided chimneys are generally not appropriate. Masonry or stucco is appropriate.

Texture and tooling of mortar on new construction should be similar to historic examples.

Asphalt shingle is an appropriate roof material for most buildings. Generally, roofing should not have strong simulated shadows in the granule colors which results in a rough, pitted appearance; faux shadow lines; strongly variegated colors; colors that are too light (e.g.: tan, white, light green); wavy or deep color/texture used to simulate split shake shingles or slate; excessive flared form in the shingle tabs; uneven or sculpted bottom edges that emphasize tab width or edges, unless matching the original roof.

Primary entrances should be 1/2 to full-light doors. Faux leaded glass is inappropriate.

Generally front doors should be 1/2 to full-light. Faux leaded glass is inappropriate.

5. Roof Shape

The roofs of new buildings shall be visually compatible, by not contrasting greatly, with the roof shape and orientation of surrounding buildings.

Roof pitches should be similar to the pitches found in the district. Historic roofs are generally between 6/12 and 12/12.

Roof pitches for porch roofs are typically less steep, approximately in the 3-4/12 range.

Generally, two-story residential buildings have hipped roofs.

Generally, dormers should be located on the roof. Wall dormers are not typical in the historic context and accentuate height so they should be used minimally and generally only on secondary facades. When they are appropriate they should be no wider than the typical window openings and should not project beyond the main wall.

Infill construction on the 1400 -1600 blocks of Boscobel Street may have flat roofs or roofs with a minimal slope.

6. Orientation

The site orientation of new buildings shall be consistent with that of adjacent buildings and shall be visually compatible. Directional expression shall be compatible with surrounding buildings, whether that expression is vertical, horizontal, or non-directional.

Porches

New buildings should incorporate at least one front street-related porch that is accessible from the front street.

Side porches or porte cocheres may also be appropriate as a secondary entrance, but the primary entrance should address the front.

Front porches generally should be a minimum of 6' deep, have porch racks that are 1'-3' tall and have posts that include bases and capitals.

Parking areas and Driveways

Generally, curb cuts should not be added.

Where a new driveway is appropriate it should be two concrete strips with a central grassy median. Shared driveways should be a single lane, not just two driveways next to each other. Sometimes this may be accomplished with a single lane curb cut that widens to a double lane deeper into the lot.

Duplexes

Infill duplexes shall have one or two doors facing the street, as seen on historic duplexes. In the case of corner lots, an entrance facing the side street is possible as long as it is designed to look like a secondary entrance.

In the case of duplexes, vehicular access for both units should be from the alley, where an alley exists. A new shared curb cut may be added, if no alley and no driveway exists, but the driveway should be no more than 12' wide from the street to the rear of the home. Driveways should use concrete strips where they are typical of the historic context. Front yard parking or driveways which end at the front of the house are not consistent with the character of the historic neighborhoods.

7. Proportion and Rhythm of Openings

The relationship of width to height of windows and doors, and the rhythm of solids (*walls*) to voids (*door and window openings*) in a new building shall be compatible, by not contrasting greatly, with surrounding *historic* buildings.

Window openings on the primary street-related or front façade of new construction should be representative of the window patterns of similarly massed historic structures within the district.

In most cases, every 8-13 horizontal feet of flat wall surface should have an opening (window or door) of at least 4 square feet. More leniencies can be given to minimally visible side or rear walls.

Double-hung windows should exhibit a height to width ratio of at least 2:1.

Windows on upper floors should not be taller than windows on the main floor since historically first floors have higher ceilings than upper floors and so windows were typically taller on the first floor.

Single-light sashes are appropriate for new construction. If using multi-light sashes, muntins should be fully simulated and bonded to the glass, and exhibit an interior bar, exterior bar, as well as a spacer between glass panes.

Four inch (nominal) casings are required around doors, windows and vents on non-masonry buildings. Trim should be thick enough to extend beyond the clapboard. Double or triple windows should have a 4" to 6" mullion in between. Brick molding is required around doors, windows and vents within masonry walls but is not appropriate on non-masonry buildings.

8. Outbuildings

b. Garages, if visible from the street, should be situated on the lot as historically traditional for the neighborhood.

Generally, attached garages are not appropriate; however, instances where they may be are:

- Where they are a typical feature of the neighborhood; or*
- When the location of the attached garage is in the general location of an historic accessory building, the new garage is located in the basement level, and the vehicular access is on the rear elevation.*

9. Appurtenances

Appurtenances related to new buildings, including driveways, sidewalks, lighting, fences, and walls, shall be visually compatible with the environment of the existing buildings and sites to which they relate.

Utilities

Utility connections such as gas meters, electric meters, phone, cable, and HVAC condenser units should be located so as to minimize their visibility from the street.

Generally, utility connections should be placed no closer to the street than the mid point of the structure.

Power lines should be placed underground if they are carried from the street and not from the rear or an alley.

Public Spaces

Landscaping, sidewalks, signage, lighting, street furniture and other work undertaken in public spaces by any individual, group or agency shall be presented to the MHZC for review of compatibility with the character of the district.

Generally, mailboxes should be attached to the front wall of the house or a porch post. In most cases, street-side mailboxes are inappropriate.

Background: The area that is now Lindsley Park Drive was initially the driveway to the Adrien V. S. Lindsley’s 1840, Italianate style, Springside Mansion. A Union supporter, Lindsley permitted his estate to serve as an unofficial headquarters for generals George Thomas and James Wilson during the Civil War. Beginning in 1887, owners of large estates in the area began to subdivide and sell off their land holdings. The property located at 106 Lindsley Park Drive is lot no. 25 of the Lindsley Park Addition Subdivision recorded in 1925. (Figure 1). At that time Lindsley Park Drive was Helen Drive.

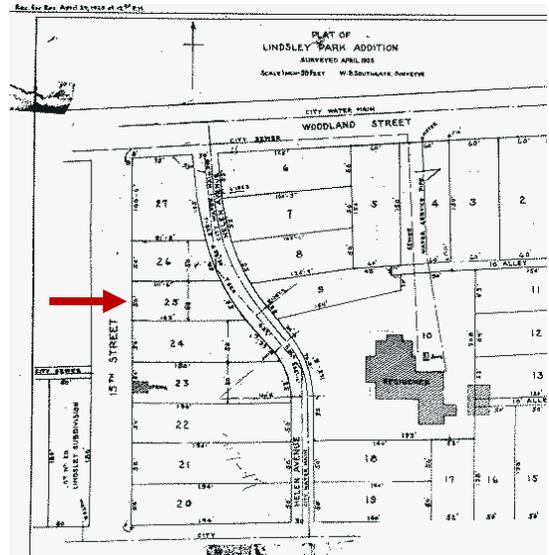


Figure 1: Lindsley Park Addition Subdivision (1925)

The site is vacant and has double frontage on Lindsley Park Drive and South Fifteenth Street. In addition, the lot slopes down approximately twenty-two feet (22’) from Lindsley Park Drive to South Fifteenth Street. (Figures 2 and 3).



Figure 2: View of lot from Lindsley Park Drive



Figure 3: View of lot from Lindsley Park Drive

In March 2017, the Historic Zoning Commission approved a design for an infill on this site. That design was never constructed.

Analysis and Findings: Application is to construct a new single-family residence on a vacant lot.

Height & Scale: The proposed infill is one and one-half (1.5) stories at the front with an overall height of approximately twenty-eight feet, eight inches (28’-8”) from grade. This includes foundation height of approximately eighteen inches (18”) at the front of the building. The overall height of the infill is slightly taller than historic homes on Lindsley Park Drive, which range from sixteen feet to twenty-eight feet (16’-28’) tall. However, staff finds the overall height to be appropriate since the infill will be only eight inches

(8") taller than the historic house across the street at 107 Lindsley Park Drive. When taking into account the change in grade, the proposed infill will sit lower than that historic house. MHZC previously approved a one-and-a-half story infill with a height of twenty-eight feet, eight inches (28'-8") for this site. The infill will have an eave height of thirteen feet (13'), which is appropriate for a one and one-half (1.5) story home.

The new infill will be thirty-five feet, four inches (35'4") wide at the front and bump out to thirty-seven feet, six inches (37'6") with a bay at the first floor and basement level. The bay will occur over twenty-four feet (24') back from the front wall of the house. The lot is slightly wider at the street than other lots with historic homes on Lindsley Park. This lot is fifty-nine feet (59') wide, whereas the historic houses are on lots that are fifty-five feet (55') wide. Historic buildings in the immediate vicinity on slightly narrower lots range from thirty to thirty-three feet (30'-33') wide. Given the width of the lot, staff finds the proposed primary width of thirty-five feet, four inches (35'4") to be appropriate. Staff also finds that it is appropriate for the footprint to widen to up to thirty-seven feet, six inches (37'6"), as the additional width occurs over twenty feet (20') behind the front wall, and the wider portion is only sixteen feet, eight inches (16'8") deep.

The infill will be approximately sixty-one feet (61') deep and will have a footprint of approximately two thousand, two hundred, and forty square feet (2,240 sq. ft.)

Staff finds that the infill's height and scale are appropriate and that the project meets sections II.B. 1 and 2 of the design guidelines.

Setback & Rhythm of Spacing: The proposed infill meets all base zoning setbacks. The side setbacks will be five feet (5') on the right side and seven feet, six inches (7'6") on the left side. The twenty foot (20') front setback will be consistent with the infill at 104 Lindsley Park Drive that was approved by the Commission in 2009. This is appropriate as there are no historic homes on this side of Lindsley Park Drive. The rear setback will be approximately thirty-one (31') feet.

Staff finds that the infill's setback and rhythm of spacing meet section II.B.3. of the design guidelines

Materials:

	Proposed	Color/Texture/Make/Manufacturer	Approved Previously or Typical of Neighborhood	Requires Additional Review
Foundation	Stone	Not indicated	Yes	Yes
Cladding	Brick	Not indicated	Yes	Yes
Secondary Cladding	Board and Batten	Smooth, stained trim	Yes	No
Additional Cladding	Cement Fiberboard lap siding	5" trim, smooth	Yes	No

Roofing	Asphalt Shingles	Not indicated	Yes	Yes
Trim	Fiber cement	Smooth	Yes	No
Sills and lintels within brick walls	Stone	Not indicated	Yes	Yes
Front Porch floor/steps	Concrete	Natural Color	Yes	No
Front Porch Posts	Wood	Stained, smooth	Yes	No
Front Porch Column base	Brick	Not indicated	Yes	Yes
Front porch railing	Wood	Typical	Yes	No
Windows	Not indicated	Unknown	Unknown	Yes
Principle Entrance	1/3 glass*	Unknown	Unknown	Yes
Side/rear doors	Not indicated	Unknown	Unknown	Yes
Rear Porch columns and railing	Wood	Typical	Yes	Yes
Driveway	Not indicated	Unknown	Unknown	Yes
Walkway	Not indicated	Unknown	Unknown	Yes
Garage Door	Not indicated	Unknown	Unknown	Yes

*The front façade drawing shows a front door that is 1/3 glass. The design guidelines state that, “*Generally front doors should be 1/2 to full-light.*” Staff recommends that the front door be at least half-glass.

Staff also recommends staff approval of a brick sample, stone sample, all windows and doors, the roof color and texture, and the driveway and walkway materials. With staff’s approval of all final material choices and with the condition that the front door be at least half glass, staff finds that the known materials meet Section II.B.4. of the design guidelines.

Roof form: The roof will be cross-gabled with pitches of 13/12 at the front and 8/12 on the side facades. The front dormer will be a shed dormer with a 3/12 pitch. The front porch will have a shed roof with a 4/12 pitch. The dormer is set back two feet (2’) from the wall below. The bay on the left façade will have a shed roof with a 5/12 pitch, and

the rear facing gable will have a 14/12 pitch. Staff finds that the roof forms and pitches are compatible with the historic context and meet Section II.B.5. of the design guidelines.

Orientation: The proposed infill is oriented toward Lindsley Park Drive, with a seven foot (7') deep partial width front porch. The infill includes a walkway that connects to the street. Vehicular access to the site will be via the rear, which is South 15th Street. Because of the steep slope of the lot and the lack of an alley, staff finds the driveway off of South 15th Street to be appropriate. Staff finds that the infill's orientation meets Section II.B.6. of the design guidelines.

Proportion and Rhythm of Openings: Most of the windows on the proposed infill are twice as tall as they are wide, thereby meeting the historic proportions of openings. There are no large expanses of wall space without a window or door opening. Staff finds the project's proportion and rhythm of openings to meet Section II.B.7.

Outbuilding: The plan also includes an attached garage that is accessed from a proposed driveway off South 15th Street at the rear. Because of the steep slope of the lot, the attached garage is located at basement level and the vehicular access is on the rear elevation, which meets the criteria for when attached garages may be appropriate. Staff finds that the proposed attached garage meets Section II.B.8. of the design guidelines.

Appurtenances & Utilities: The infill includes a walkway leading from the street to the front porch. The driveway will be located from the rear of the lot with a curb cut from South Fifteenth Street. The location of the HVAC and other utilities was not noted on the plans. Staff asks that the HVAC be located on the rear façade, or on a side façade beyond the midpoint of the house, to ensure that the project meets Section II.B.9. of the design guidelines.

Recommendation Summary: Staff recommends approval with the following conditions:

1. The finished floor height be consistent with the finished floor heights of the nearby historic houses, to be verified by MHZC staff in the field;
2. Staff approve a brick sample;
3. Staff approve a stone sample;
4. Staff approve the walkway and driveway materials;
5. The front door be a minimum of half glass, and staff approve the final details, dimensions and materials of all windows and doors;
6. Staff approve the roof shingle color and texture;
7. The HVAC be located behind the house or on either side, beyond the mid-point of the house.

With these conditions, staff finds that the project meets Section II.B. of the *Lockeland Springs-East End Neighborhood Conservation District: Handbook and Design Guidelines*.

Context Photos:



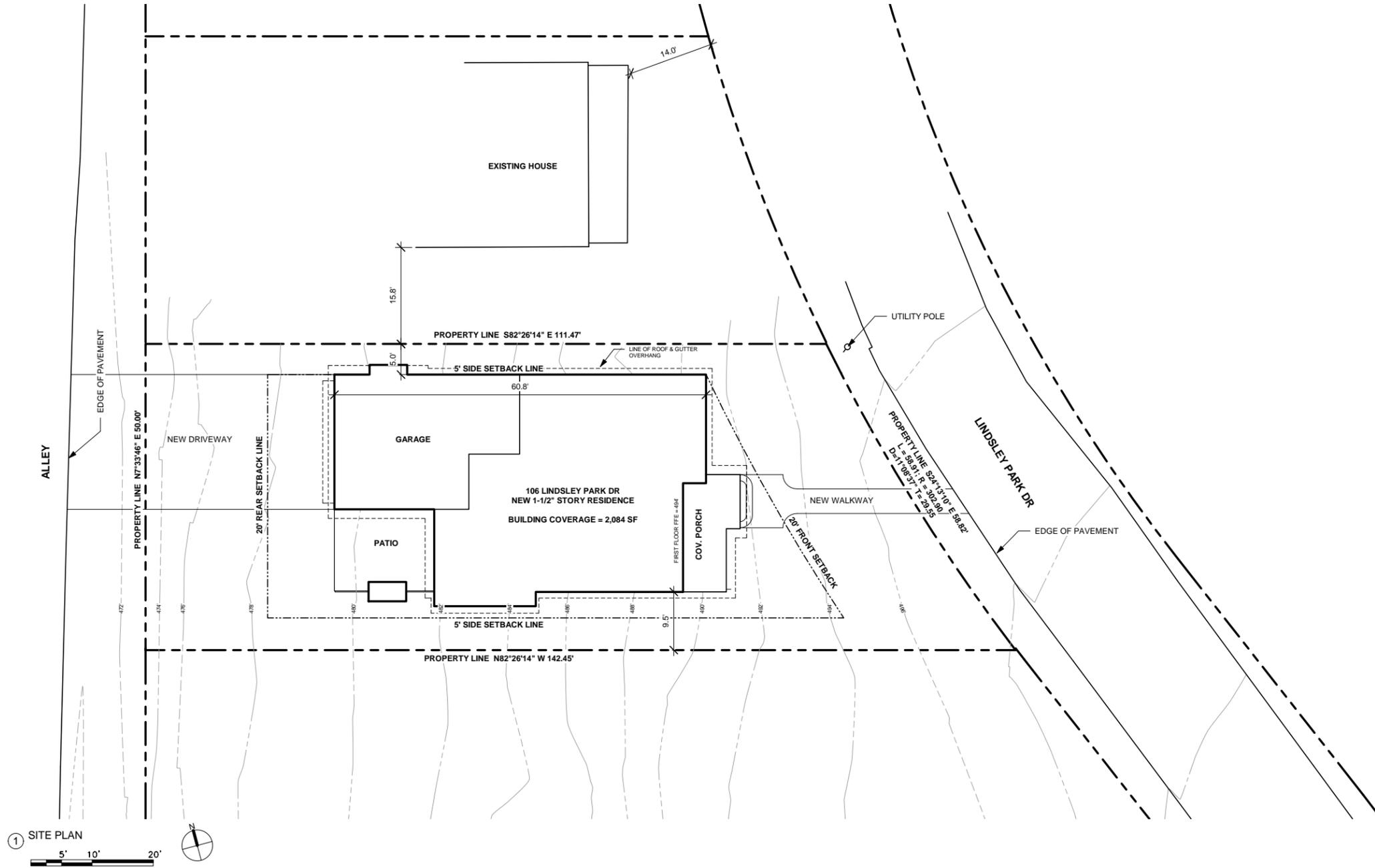
104 Lindsley Park Drive – non-contributing (infill approved by MHZC in 2009)



105 Lindsley Park Drive – contributing



107 Lindsley Park Drive - contributing



PROJECT DATA:
 106 LINDSLEY PARK DRIVE
 NASHVILLE, TN 37206

ZONING INFORMATION:
 ZONE R6
 MAX. HEIGHT - 3 STORIES
 ACTUAL HEIGHT - 1.5 STORIES

TOTAL LOT AREA:
 6,292 S.F.

BUILDING AREA CALCULATIONS:
 BASEMENT PLAN = 599 SF
 FIRST FLOOR = 1,858 GSF
 SECOND FLOOR = 1,040 GSF
 TOTAL AREA = 3,497 GSF
 EXTERIOR PORCHES = 150 GSF
 GARAGE = 588 GSF

TOTAL BUILDING COVERAGE:
 2,084 SF / 6,292 SF = 0.33

MHZC SHEET INDEX	
NUM.	SHEET NAME
A0.01H	SITE PLAN
A1.03H	ROOF PLAN
A2.02H	BUILDING ELEVATIONS
A4.00H	3D VIEWS
A4.02H	3D VIEWS
A4.03H	3D VIEWS
A4.04H	3D VIEWS



REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MHZC SUBMITTAL SET

Project Number: **106**

Date: **5.4.2018**

SITE PLAN

A0.01H

106 LINDSLEY PARK DRIVE

NORTH BY NORTHEAST DEVELOPMENT

CUSTOM SINGLE FAMILY RESIDENCE

Designed For:

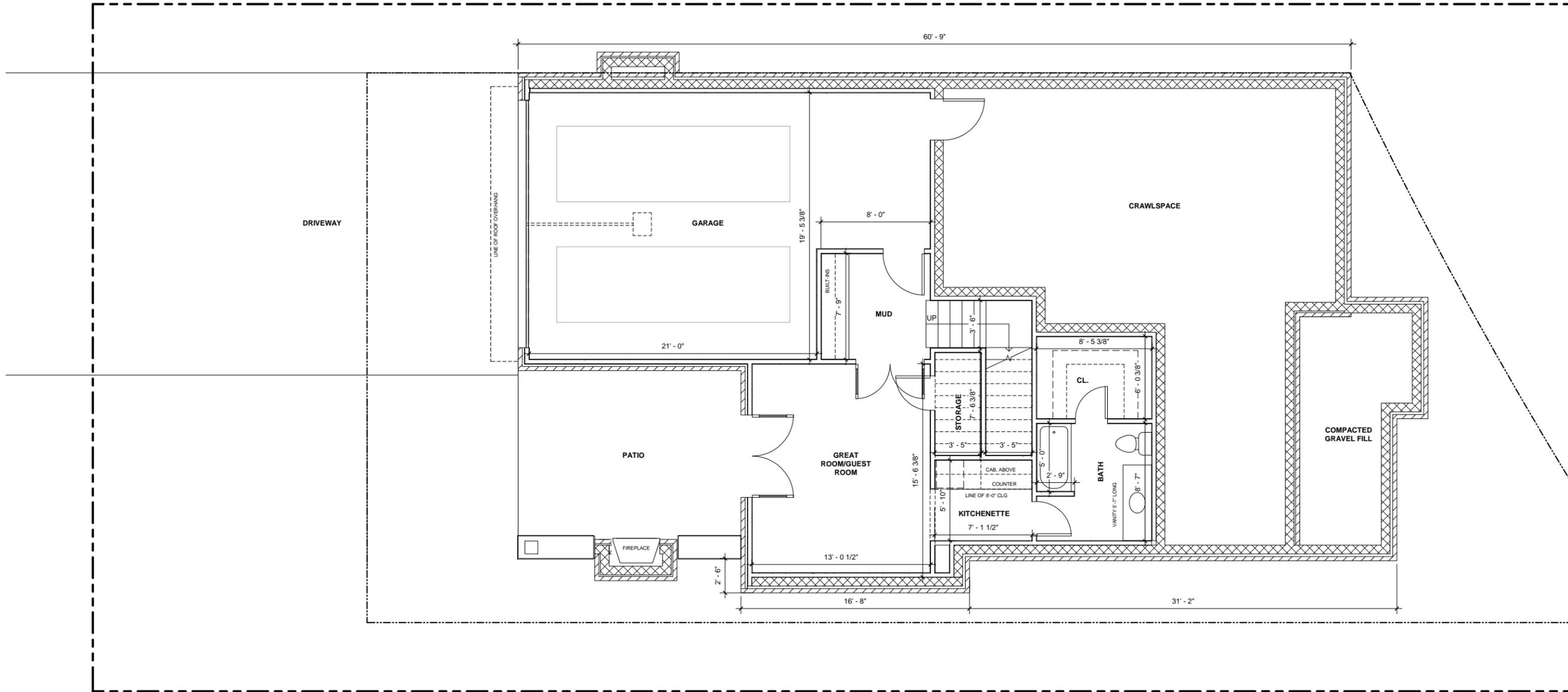
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Building Ideas*

Architecture Interior Design Planning

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 dbaird@building-ideas.net



① BASEMENT PLAN

BASEMENT	606 SF
FIRST FLOOR	1,858 SF
SECOND FLOOR	1,040 SF
TOTAL	3,535 SF



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Designed For:
NORTH BY NORTHEAST DEVELOPMENT

106 LINDSLEY PARK DRIVE
 CUSTOM SINGLE FAMILY RESIDENCE

REVISIONS		
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Project Phase:
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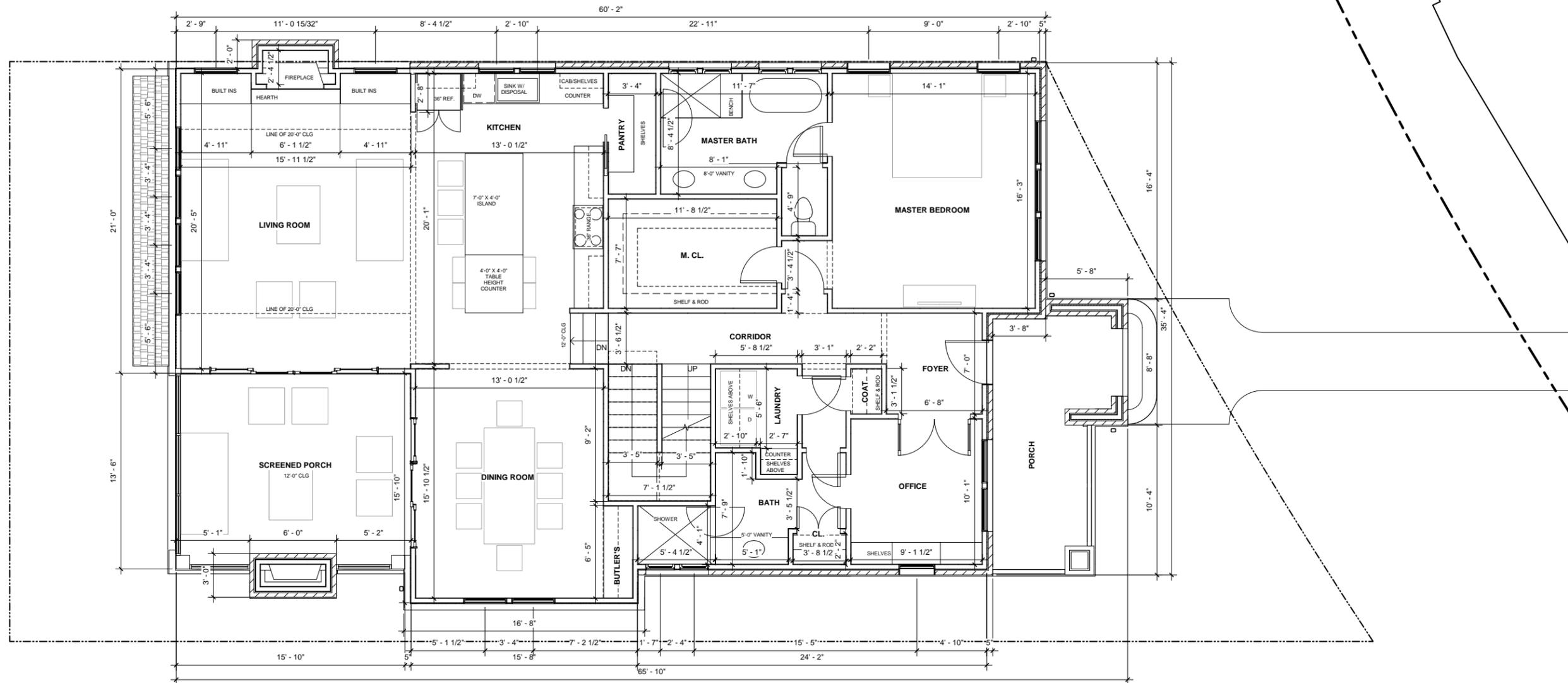
Project Number: **106**

Date: **5.4.2018**

BASEMENT PLAN

A1.00H

① FIRST FLOOR PLAN-OPTION 1

BASEMENT	606 SF
FIRST FLOOR	1,858 SF
SECOND FLOOR	1,040 SF
TOTAL	3,535 SF



REVISIONS		
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Project Phase:
MHZC SUBMITTAL SET

Project Number: **106**

Date: **5.4.2018**

FIRST FLOOR PLAN

A1.01H

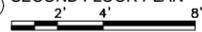
106 LINDSLEY PARK DRIVE

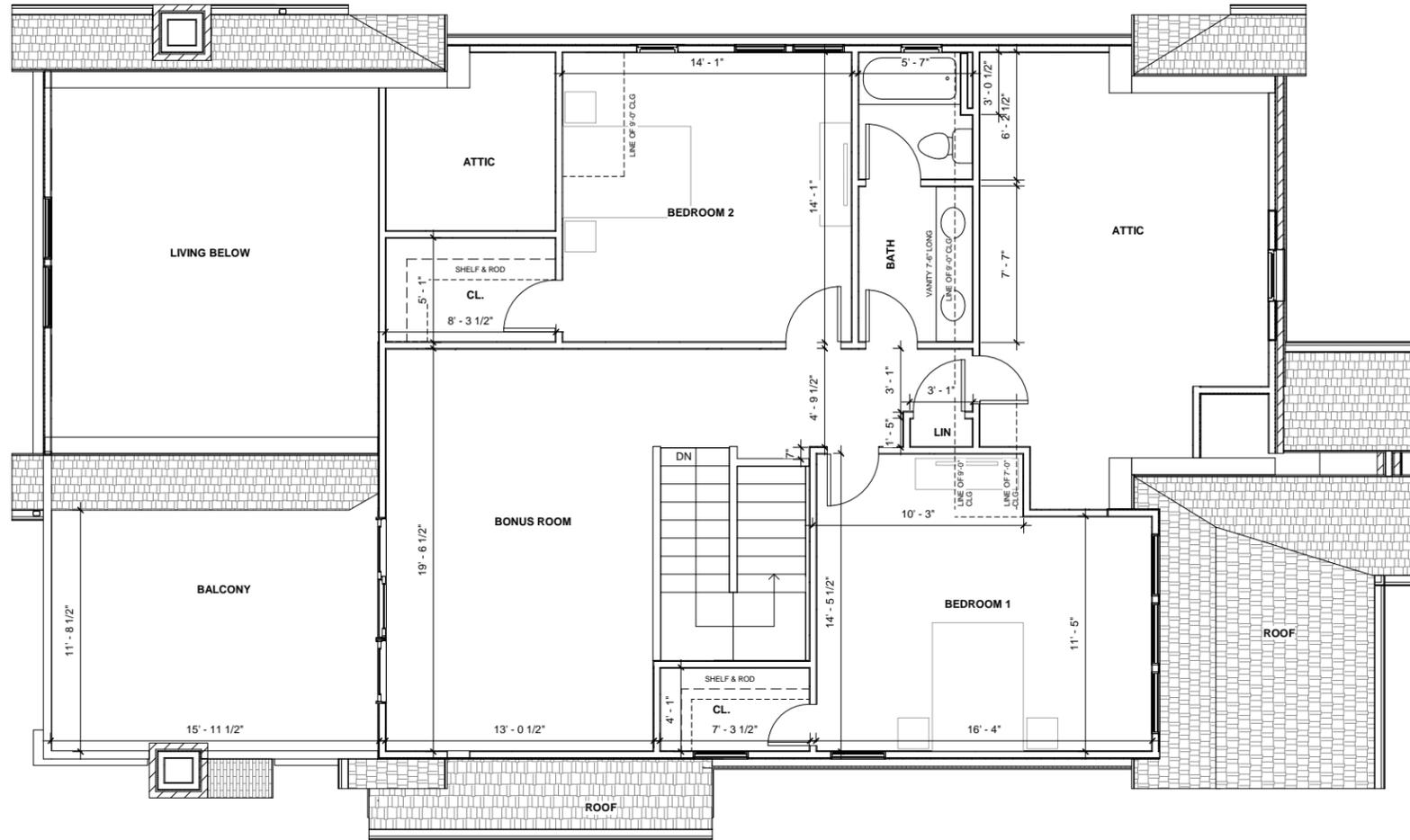
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① SECOND FLOOR PLAN




BASEMENT	606 SF
FIRST FLOOR	1,858 SF
SECOND FLOOR	1,040 SF
TOTAL	3,535 SF



REVISIONS		
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Project Phase:

MHZC SUBMITTAL SET

Project Number: **106**

Date: **5.4.2018**

SECOND FLOOR PLAN

A1.02H

Designed For:

**NORTH BY
 NORTHEAST
 DEVELOPMENT**

**106 LINDSLEY PARK
 DRIVE**

CUSTOM SINGLE FAMILY RESIDENCE

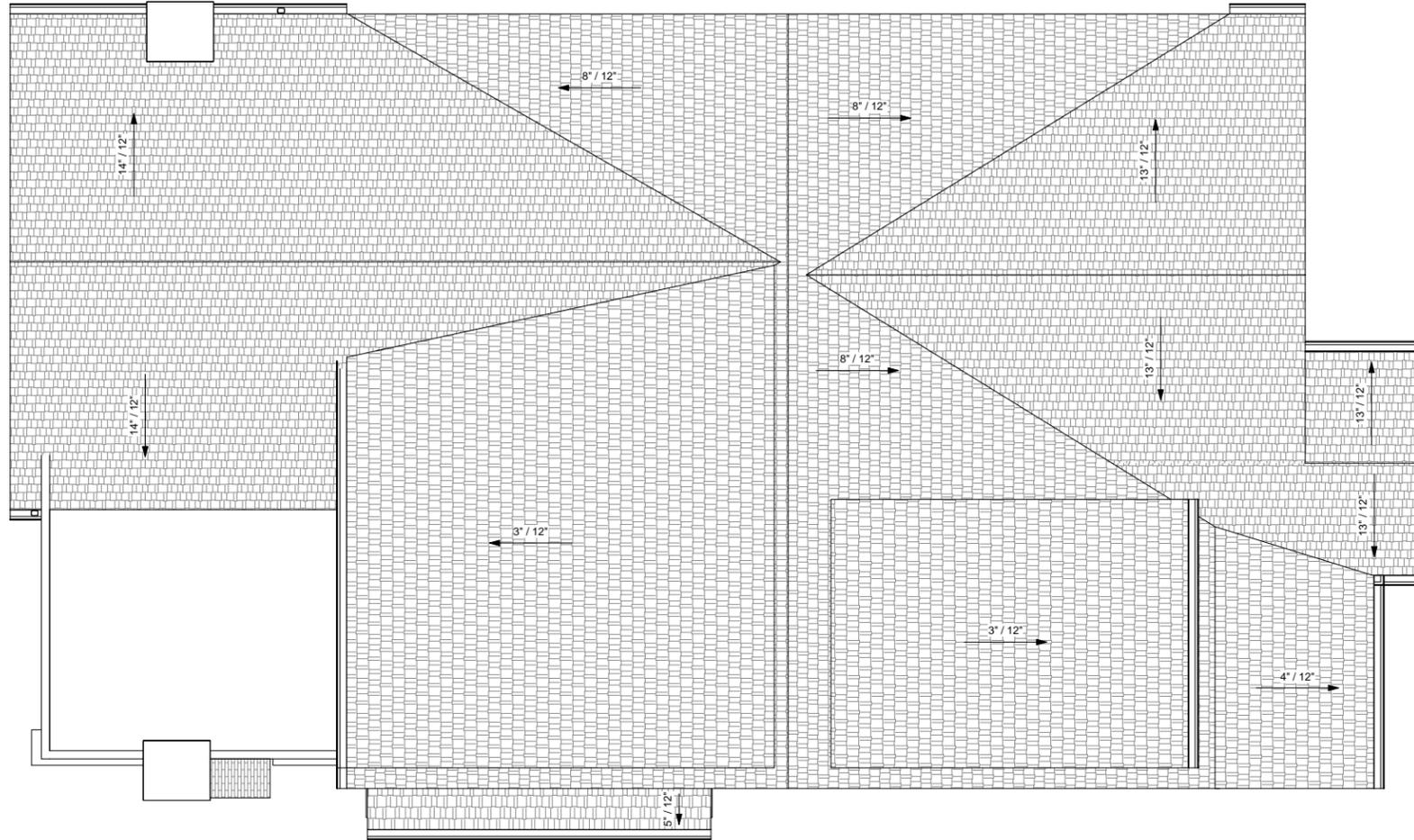
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① ROOF PLAN

REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:

MHZC SUBMITTAL SET

Project Number: 106

Date: 5.4.2018

ROOF PLAN

A1.03H

106 LINDSLEY PARK DRIVE

NORTH BY NORTHEAST DEVELOPMENT

Designed For:

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David Baird, Architect

NCARB, LEED-AP

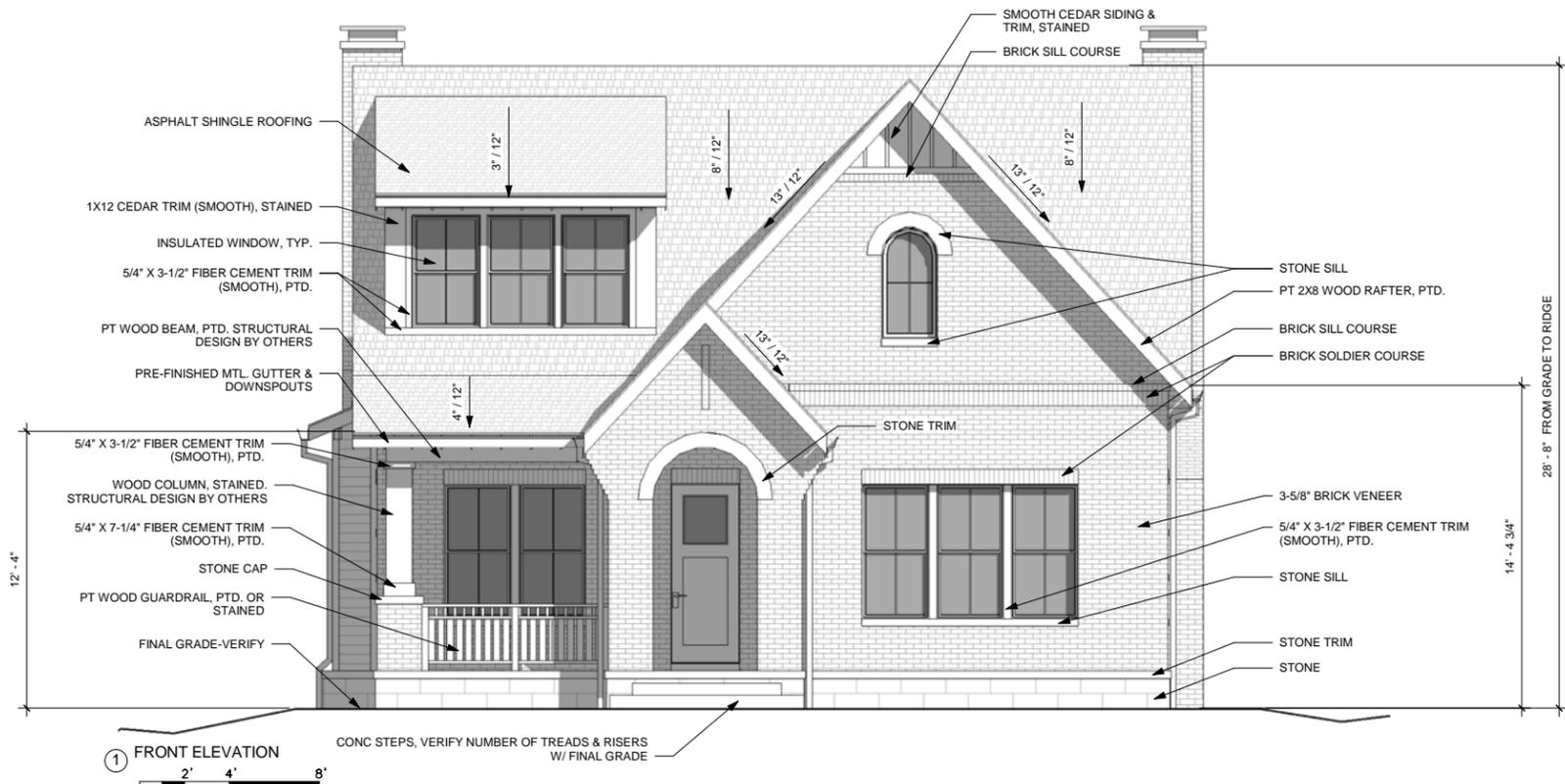
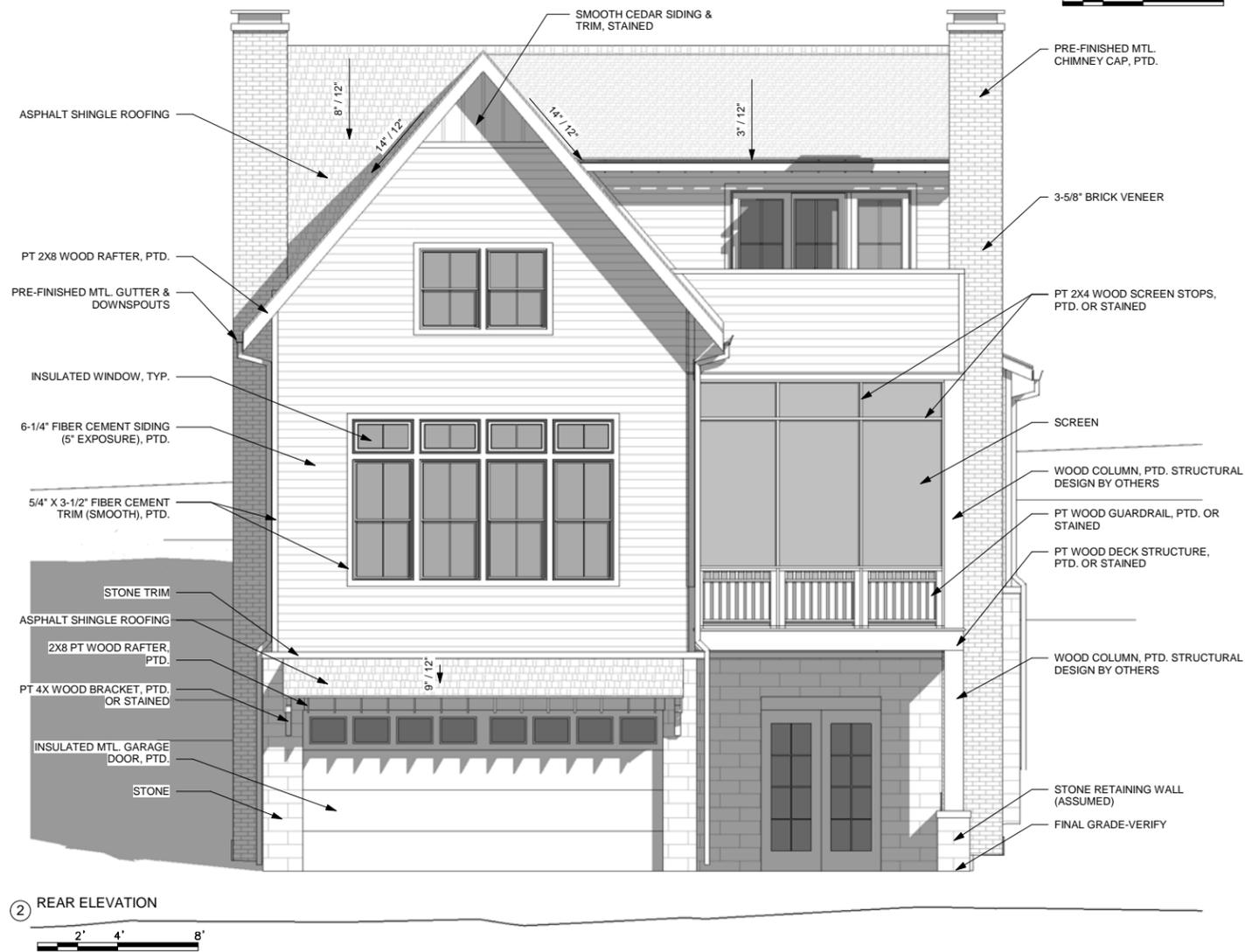
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CUSTOM SINGLE FAMILY RESIDENCE

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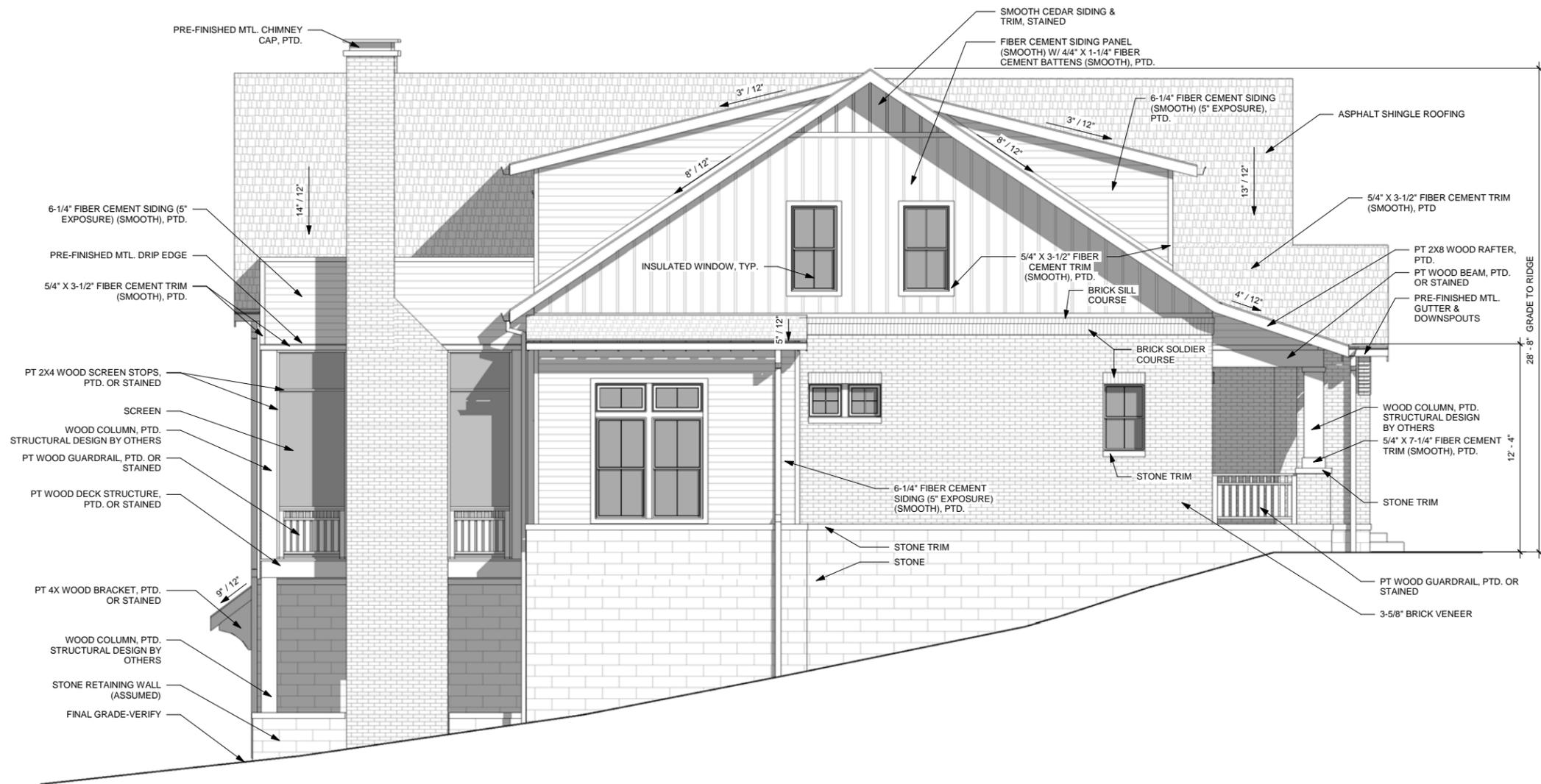
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BUILDING ELEVATIONS

A2.00H





① LEFT SIDE ELEVATION



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Project Phase:
MHZC SUBMITTAL SET

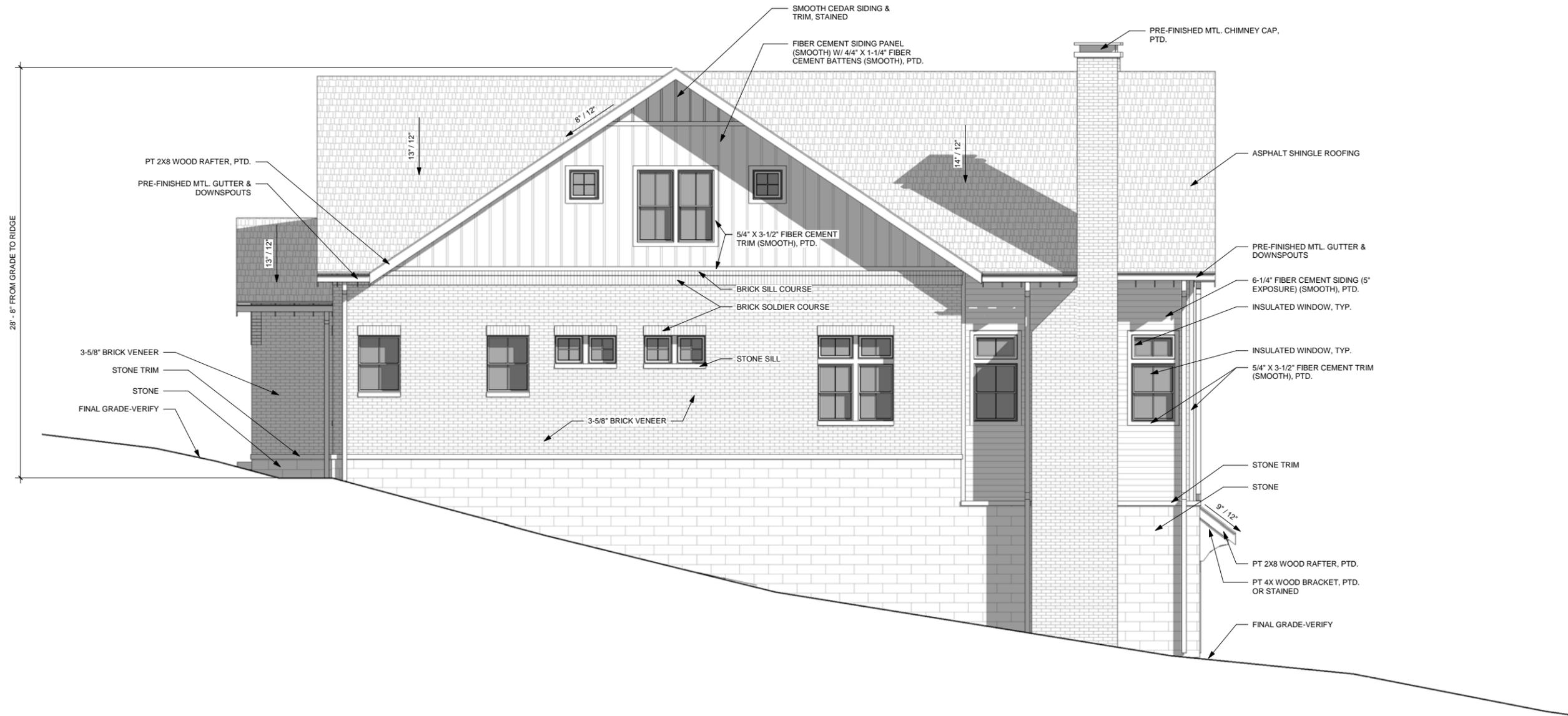
Project Number: **106**

Date: **5.4.2018**

BUILDING ELEVATIONS

A2.01H





① RIGHT SIDE ELEVATION
 2' 4' 8'

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Designed For:
NORTH BY NORTHEAST DEVELOPMENT

106 LINDSLEY PARK DRIVE
 CUSTOM SINGLE FAMILY RESIDENCE

REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MHZC SUBMITTAL SET

Project Number: **106**

Date: **5.4.2018**

BUILDING ELEVATIONS

A2.02H



5/4/2018 9:37:00 AM



② FRONT LEFT 3D VIEW



① FRONT RIGHT 3D VIEW

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Designed For:

**NORTH BY
NORTHEAST
DEVELOPMENT**

**106 LINDSLEY PARK
DRIVE**

CUSTOM SINGLE FAMILY RESIDENCE

REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MH2C SUBMITTAL SET

Project Number: **106**

Date: **5.4.2018**

3D VIEWS

A4.00H





② REAR RIGHT 3D VIEW



① REAR LEFT 3D VIEW

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Designed For:
NORTH BY NORTHEAST DEVELOPMENT

106 LINDSLEY PARK DRIVE
 CUSTOM SINGLE FAMILY RESIDENCE

REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MHZC SUBMITTAL SET

Project Number: **106**

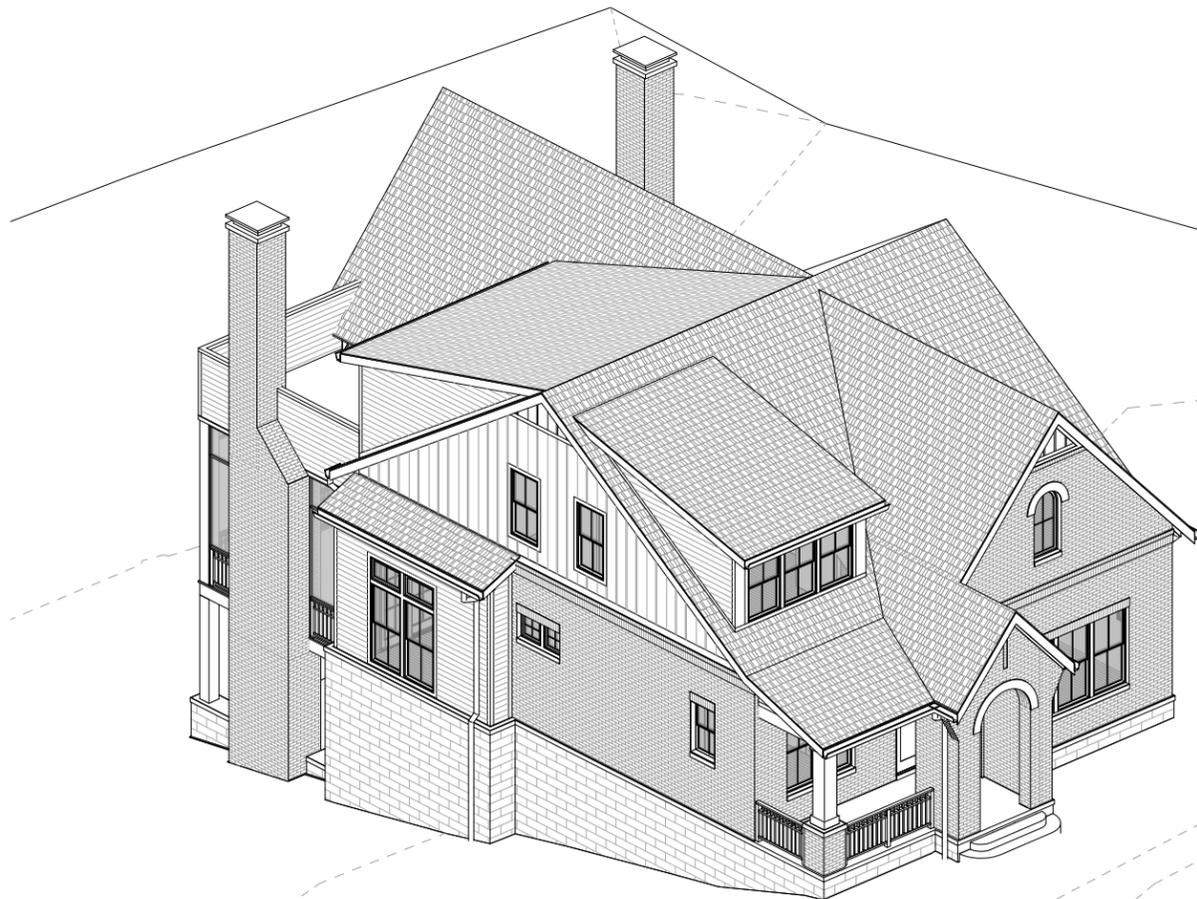
Date: **5.4.2018**

3D VIEWS

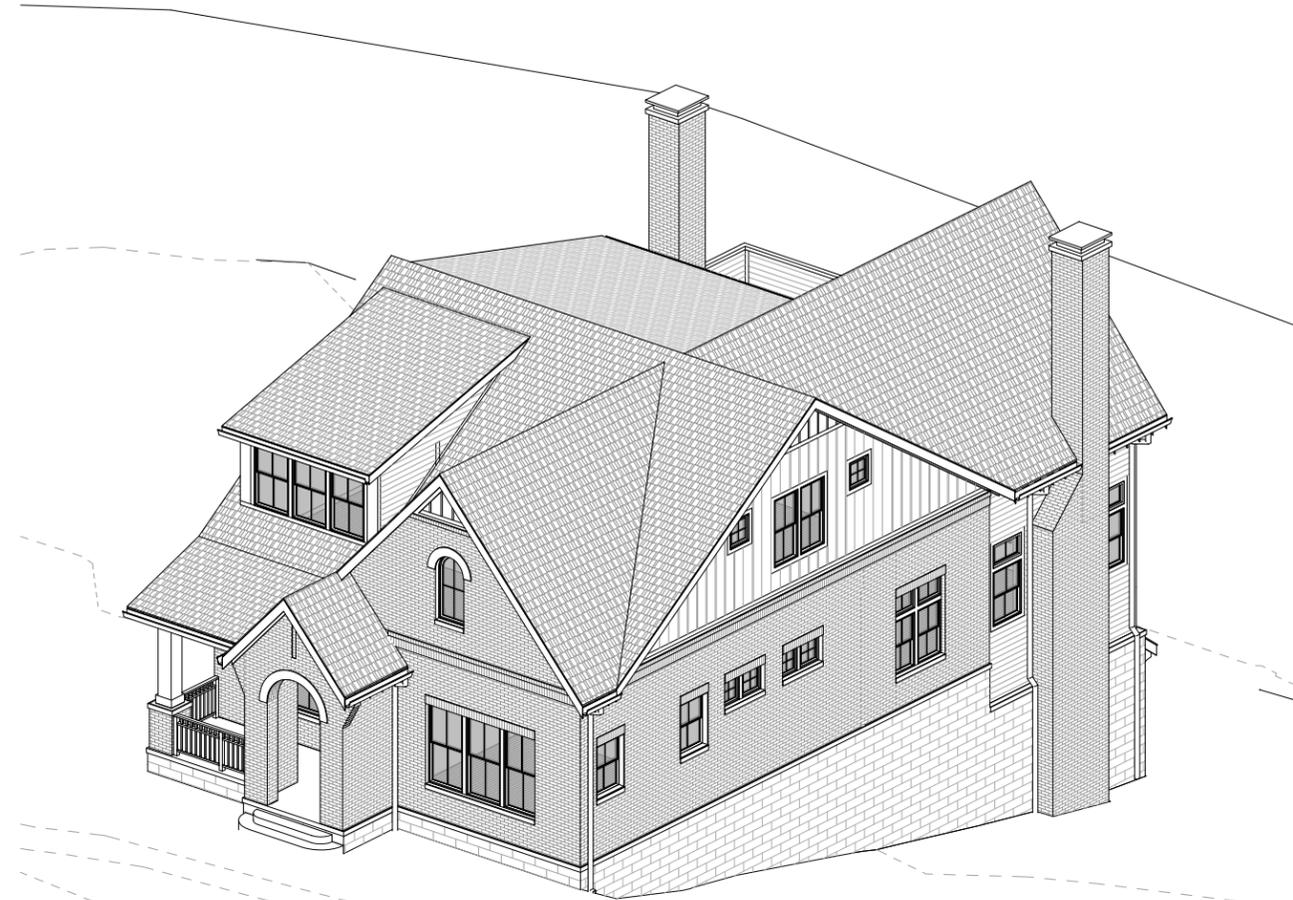


5/4/2018 9:37:13 AM

② FRONT LEFT BIRDS EYE VIEW



① FRONT RIGHT BIRDS EYE VIEW



REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MHZC SUBMITTAL SET

Project Number: 106

Date: 5.4.2018

3D VIEWS

A4.03H

106 LINDSLEY PARK DRIVE

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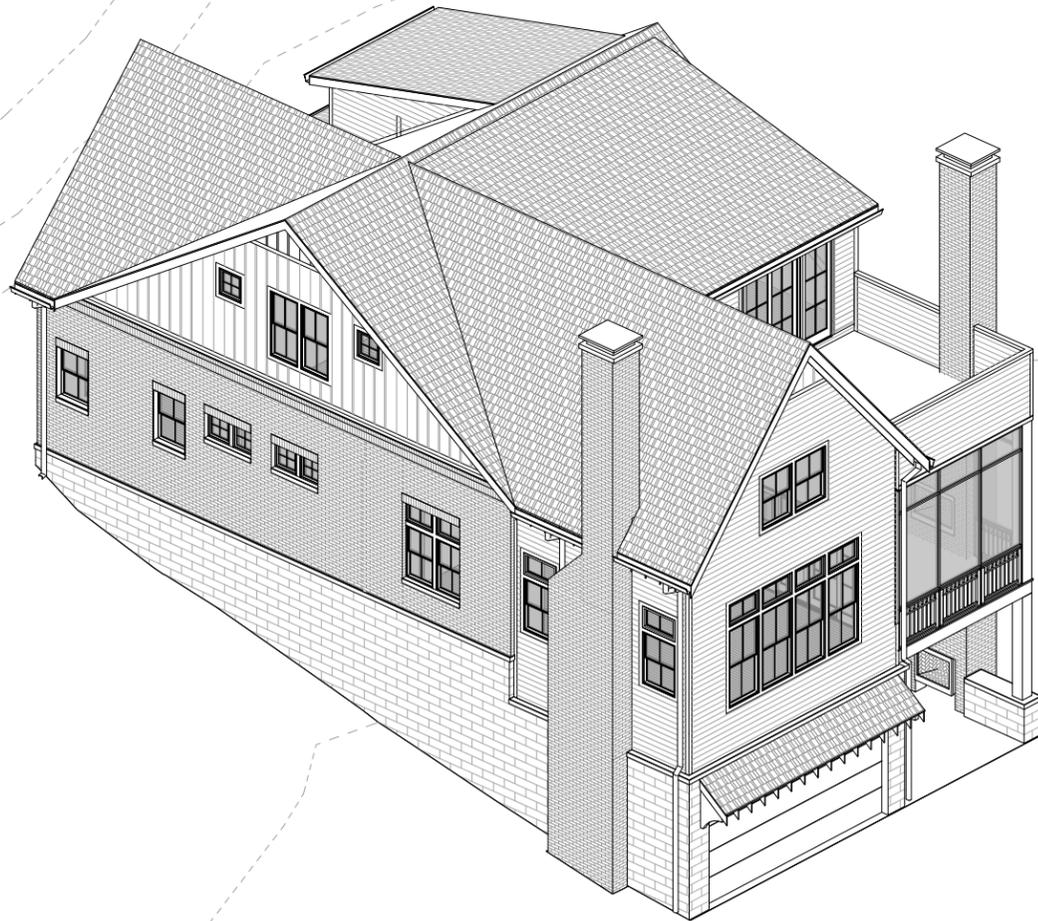
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Architecture Interior Design Planning

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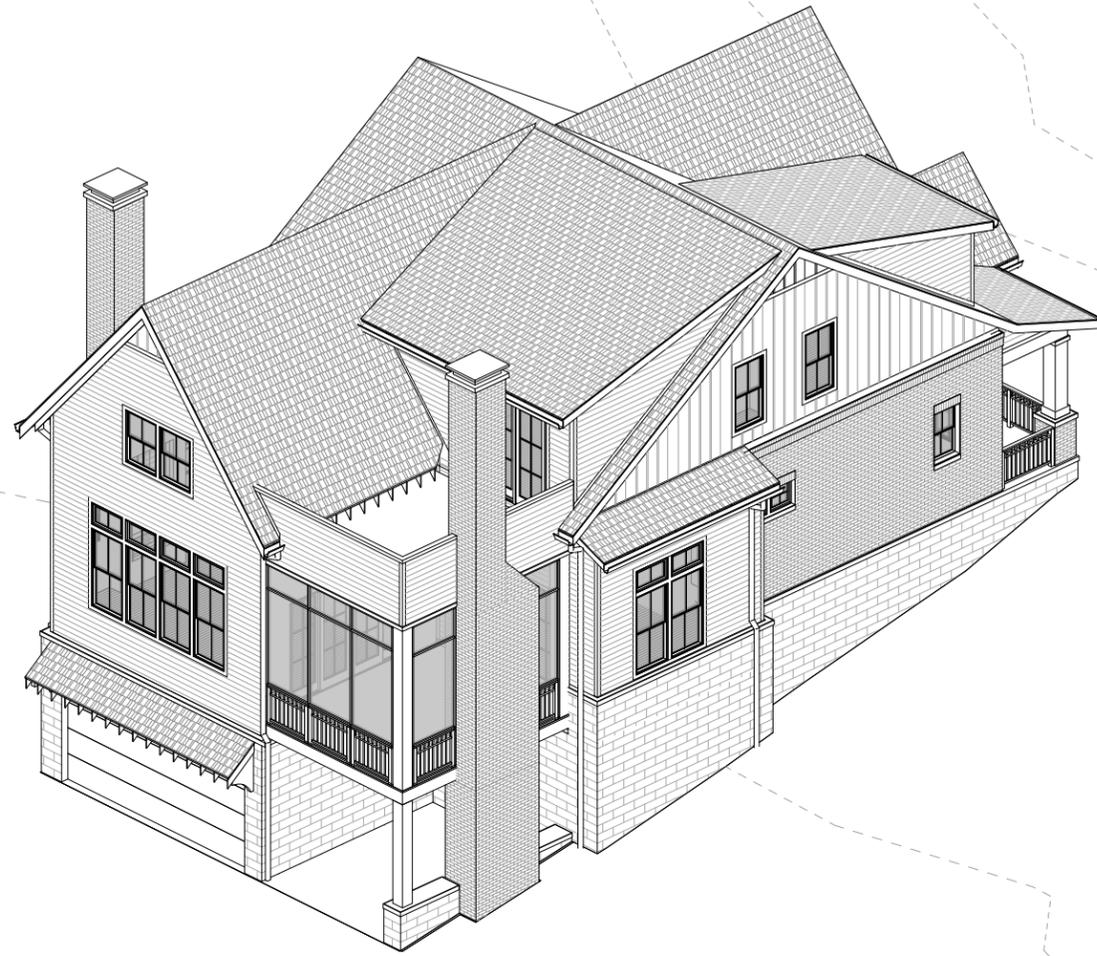


5/4/2018 9:37:22 AM

② BACK RIGHT BIRDS EYE VIEW



① BACK LEFT BIRDS EYE VIEW



REVISIONS		
NUM.	DESCRIPTION	DATE

Project Phase:
MHZC SUBMITTAL SET

Project Number: 106

Date: 5.4.2018

3D VIEWS

A4.04H



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Designed For:

**NORTH BY
NORTHEAST
DEVELOPMENT**

**106 LINDSLEY PARK
DRIVE**

CUSTOM SINGLE FAMILY RESIDENCE

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