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MAYOR



METROPOLITAN GOVERNMENT OF NASHVILLE AND DAVIDSON COUNTY

Metropolitan Historic Zoning Commission
Sunnyside in Sevier Park
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STAFF RECOMMENDATION

**3925 Cambridge Avenue
December 19, 2018**

Application: Partial Demolition; New Construction—Addition; Setback Determination

District: Cherokee Park Neighborhood Conservation Zoning Overlay

Council District: 24

Map and Parcel Number: 10308023200

Applicant: Van Pond, Architect

Project Lead: Sean Alexander, sean.alexander@nashville.gov

Description of Project: The proposal is for a side and rear addition to an historic house. The addition will be one-story and will include a front-facing garage. A rear setback determination is requested, reducing the rear setback from twenty feet (20') to five feet (5').

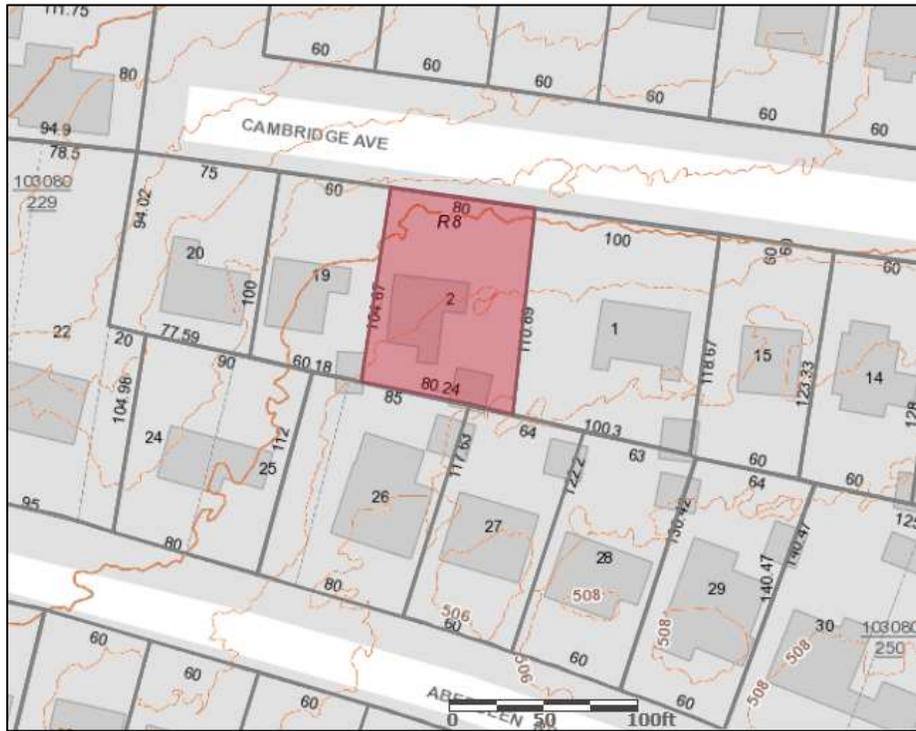
Recommendation Summary: Staff recommends approval of the proposed addition to 3925 Cambridge Avenue with the following conditions:

1. The rear setback shall be a minimum of ten feet (10'); and
2. The window and door selections and paving material are administratively approved.

With those conditions, Staff finds that the proposed addition would meet the design guidelines for additions in the Cherokee Park Neighborhood Conservation Zoning Overlay.

Attachments
A: Photographs
B: Site Plan
C: Elevations

Vicinity Map:



Aerial Map:



Applicable Design Guidelines:

II.B. GUIDELINES

B. NEW CONSTRUCTION

a. Height

The height of the foundation wall, porch roof(s), and main roof(s) of a new building shall be compatible, by not contrasting greatly, with those of surrounding historic buildings.

b. Scale

The size of a new building and its mass in relation to open spaces shall be compatible, by not contrasting greatly, with surrounding historic buildings.

Foundation lines should be visually distinct from the predominant exterior wall material. This is typically accomplished with a change in material.

c. Setback and Rhythm of Spacing

The setback from front and side yard property lines established by adjacent historic buildings should be maintained. Generally, a dominant rhythm along a street is established by uniform lot and building width. Infill buildings should maintain that rhythm.

The Commission has the ability to determine appropriate building setbacks and extend height limitations of the required underlying base zoning for new construction, additions and accessory structures (ordinance no. 17.40.410).

Appropriate setbacks will be determined based on:

- The existing setback of the contributing primary buildings and accessory structures found in the immediate vicinity;*
- Setbacks of like structures historically found on the site as determined by historic maps, site plans or photographs;*
- Shape of lot;*
- Alley access or lack thereof;*
- Proximity of adjoining structures; and*
- Property lines.*

Appropriate height limitations will be based on:

- Heights of historic buildings in the immediate vicinity*
- Existing or planned slope and grade*

In most cases, an infill duplex should be one building, as seen historically in order to maintain the rhythm of the street. Detached infill duplexes may be appropriate in the following instances:

- and width to the lot to accommodate two single-family dwellings in a manner that meets the design guidelines;*
- The second unit follows the requirements of a Detached Accessory Dwelling Unit; or*
- An existing non-historic building sits so far back on the lot that a building may be constructed in front of it in a manner that meets the rhythm of the street and the established setbacks.*
- There is not enough square footage to legally subdivide the lot but there is enough frontage*

d. Materials, Texture, Details, and Material Color

The materials, texture, details, and material color of a new building's public facades shall be visually compatible, by not contrasting greatly, with surrounding historic buildings. Vinyl and aluminum siding are not appropriate.

T-1-11- type building panels, "permastone", E.F.I.S. and other artificial siding materials are generally not appropriate. However, pre-cast stone and cement fiberboard siding are approvable cladding materials for new construction; but pre-cast stone should be of a compatible color and texture to existing historic stone clad structures in the district; and cement fiberboard siding, when used for lapped siding, should be smooth and not stamped or embossed and have a maximum of a 5" reveal. The reveal for lap siding should not exceed 5". Larger reveals may be possible but should not exceed 8" and shall have mitered corners.

With the exception of Cambridge Avenue which was developed earlier than all other streets within the historic district, brick and stone (primarily limestone) are the primary exterior claddings on historic structures in the neighborhood. Only on Cambridge Avenue is wood lap siding a primary exterior cladding on historic structures and considered compatible for infill construction.

Shingle siding should exhibit a straight-line course pattern and exhibit a maximum exposure of seven inches (7").

Four inch (4") nominal corner boards are required at the face of each exposed corner.

Stud wall lumber and embossed wood grain are prohibited.

Belt courses or a change in materials from one-story to another are often encouraged for large two-story buildings to break up the massing.

When different materials are used, it is most appropriate to have the change happen at floor lines.

Clapboard sided chimneys are generally not appropriate. Masonry or stucco is appropriate.

Texture and tooling of mortar on new construction should be similar to historic examples.

Asphalt shingle is an appropriate roof material for most buildings. Generally, roofing should not have strong simulated shadows in the granule colors which results in a rough, pitted appearance; faux shadow lines; strongly variegated colors; colors that are too light (e.g.: tan, white, light green); wavy or deep color/texture used to simulate split shake shingles or slate; excessive flared form in the shingle tabs; uneven or sculpted bottom edges that emphasize tab width or edges, unless matching the original roof.

Generally front doors should be 1/2 to full-light. Faux leaded glass is inappropriate.

e. Roof Shape

The roof(s) of a new building shall be visually compatible, by not contrasting greatly, with the roof shape, orientation, and pitch of surrounding historic buildings.

Roof pitches should be similar to the pitches found in the district. Historic roofs are generally between 6/12 and 12/12.

Roof pitches for porch roofs are typically less steep, approximately in the 3-4/12 range.

Generally, two-story residential buildings have hipped roofs.

Generally, dormers should be located on the roof. Wall dormers are not typical in the historic context and accentuate height so they should be used minimally and generally only on secondary facades. When they are appropriate they should be no wider than the typical window openings and should not project beyond the main wall.

f. Orientation

The orientation of a new building's front facade shall be visually consistent with surrounding historic buildings.

Porches

New buildings should incorporate at least one front street-related porch that is accessible from the front street.

Side porches or porte cocheres may also be appropriate as a secondary entrance, but the primary entrance should address the front.
Front porches generally should be a minimum of 6' deep, have porch racks that are 1'-3' tall and have posts that include bases and capitals.

Parking areas and Driveways
Generally, curb cuts should not be added.

Where a new driveway is appropriate it should be two concrete strips with a central grassy median. Shared driveways should be a single lane, not just two driveways next to each other. Sometimes this may be accomplished with a single lane curb cut that widens to a double lane deeper into the lot.

Duplexes

Infill duplexes shall have one or two doors facing the street, as seen on historic duplexes. In the case of corner lots, an entrance facing the side street is possible as long as it is designed to look like a secondary entrance.

In the case of duplexes, vehicular access for both units should be from the alley, where an alley exists. A new shared curb cut may be added, if no alley and no driveway exists, but the driveway should be no more than 12' wide from the street to the rear of the home. Driveways should use concrete strips where they are typical of the historic context. Front yard parking or driveways which end at the front of the house are not consistent with the character of the historic neighborhoods.

Multi-unit Developments

For multi-unit developments, interior dwellings should be subordinate to those that front the street. Subordinate generally means the width and height of the buildings are less than the primary building(s) that faces the street.
For multi-unit developments, direct pedestrian connections should be made between the street and any interior units. The entrances to those pedestrian connections generally should be wider than the typical spacing between buildings along the street.

g. Proportion and Rhythm of Openings

The relationship of width to height of windows and doors, and the rhythm of solids (walls) to voids (door and window openings) in a new building shall be compatible, by not contrasting greatly, with surrounding historic buildings.

Window openings on the primary street-related or front façade of new construction should be representative of the window patterns of similarly massed historic structures within the district.
In most cases, every 8-13 horizontal feet of flat wall surface should have an opening (window or door) of at least 4 square feet. More leniencies can be given to minimally visible side or rear walls.
Double-hung windows should exhibit a height to width ratio of at least 2:1.
Windows on upper floors should not be taller than windows on the main floor since historically first floors have higher ceilings than upper floors and so windows were typically taller on the first floor.
Single-light sashes are appropriate for new construction. If using multi-light sashes, muntins should be fully simulated and bonded to the glass, and exhibit an interior bar, exterior bar, as well as a spacer between glass panes.
Four inch (nominal) casings are required around doors, windows and vents on non-masonry buildings. Trim should be thick enough to extend beyond the clapboard. Double or triple windows should have a 4" to 6" mullion in between.
Brick molding is required around doors, windows and vents within masonry walls but is not appropriate on non-masonry buildings.

h. Outbuildings

(Although the MHZC does not review use itself there are additional ordinance requirements for buildings that have are or have a Detached Accessory Dwelling Unit (DADU) required by ordinance 17.16.030 that are reviewed by the MHZC. This information is provided for informational purposes only and does not replace ordinance 17.16.030.)

- 1) A new garage or storage building should reflect the character of the period of the house to which the outbuilding will be related. The outbuilding should be compatible, by not contrasting greatly, with surrounding historic outbuildings in terms of height, scale, roof shape, materials, texture, and details.

Historically, outbuildings were either very utilitarian in character, or (particularly with more extravagant houses) they repeated the roof forms and architectural details of the houses to which they related. Generally, either approach is appropriate for new outbuildings.

Outbuildings: Height & Scale

- *On lots less than 10,000 square feet, the footprint of a DADU or outbuilding shall not exceed seven hundred fifty square feet or fifty percent of the first floor area of the principal structure, whichever is less.*
- *On lots 10,000 square feet or greater, the footprint of a DADU or outbuilding shall not exceed one thousand square feet.*
- *The DADU or outbuilding shall maintain a proportional mass, size, and height to ensure it is not taller or wider than the principal structure on the lot. The DADU or outbuilding height shall not exceed the height of the principal structure, with a maximum eave height of 10' for one-story DADU's or outbuildings and 17' for two-story DADUs or outbuildings. The roof ridge height of the DADU or outbuilding must be less than the principal building and shall not exceed 25' feet in height.*

Outbuildings: Character, Materials and Details

- *Historically, outbuildings were either very utilitarian in character, or (particularly with more extravagant houses) they repeated the roof forms and architectural details of the houses to which they related. Generally, either approach is appropriate for new outbuildings. DADUs or out buildings located on corner lots should have similar architectural characteristics, including roof form and pitch, to the existing principal structure.*
- *DADUs or outbuildings with a second story shall enclose the stairs interior to the structure and properly fire rate them per the applicable life safety standards found in the code editions adopted by the Metropolitan Government of Nashville.*

Outbuildings: Roof

- *Roof slopes on simple, utilitarian buildings do not have to match the roof slopes of the main structure, but generally should maintain at least a 4/12 pitch.*
- *The DADU or outbuilding may have dormers that relate to the style and proportion of windows on the DADU and shall be subordinate to the roof slope by covering no more than fifty percent of the roof plane and should sit back from the exterior wall by 2'.*

Outbuildings: Windows and Doors

- *Publicly visible windows should be appropriate to the style of the house.*
- *Double-hung windows are generally twice as tall as they are wide and of the single-light sash variety.*
- *Publicly visible pedestrian doors must either be appropriate for the style of house to which the outbuilding relates or be flat with no panels.*
- *Metal overhead doors are acceptable on garages when they are simple and devoid of overly decorative elements typical on high-style wooden doors. Decorative raised panels on publicly visible garage doors are generally not appropriate.*
- *For street-facing facades, garages with more than one-bay should have multiple single doors rather than one large door to accommodate more than one bay.*

Outbuildings: Siding and Trim

- *Brick, weatherboard, and board-and-batten are typical siding materials. Outbuildings with*

weatherboard siding typically have wide cornerboards and window and door casings (trim).

- *Exterior siding may match the existing contributing building's original siding; otherwise, siding should be wood or smooth cement-fiberboard lap siding with a maximum exposure of five inches (5"), wood or smooth cement-fiberboard board-and-batten or masonry.*
 - *Four inch (4" nominal) corner-boards are required at the face of each exposed corner.*
 - *Stud wall lumber and embossed wood grain are prohibited.*
 - *Four inch (4" nominal) casings are required around doors, windows, and vents within clapboard walls. Trim should be thick enough to extend beyond the clapboard. Double or triple windows should have a 4" to 6" mullion in between.*
- Brick molding is required around doors, windows, and vents within masonry walls but is not appropriate on non-masonry clad buildings.*

2) Outbuildings should be situated on a lot as is historically typical for surrounding historic buildings.

Generally new garages should be placed close to the alley, at the rear of the lot, or in the original location of an historic accessory structure.

Lots without rear alleys may have garages located closer to the primary structure. The appropriate location is one that matches the neighborhood or can be documented by historic maps.

Attached garages may be appropriate when:

- *The garage doors face the rear of the lot; or*
- *The garage doors face the side of the lot and are setback a minimum of 10' from the existing sidewall of the building; and*
- *The garage does not result in an inappropriately massed addition.*

Setbacks & Site Requirements.

- *To reflect the character of historic outbuildings, new outbuildings for duplexes should not exceed the requirements for outbuildings for the entire lot and should not be doubled. The most appropriate configuration would be two 1-bay buildings with or without parking pads for additional spaces or one 2-bay building.*
- *A DADU or outbuilding may only be located behind the principal structure in the established rear yard. The DADU or outbuilding is to be subordinate to the principal structure and therefore should be placed to the rear of the lot.*
- *There should be a minimum separation of 20' between the principal structure and the DADU or outbuilding.*

At least one side setback a DADU or outbuilding on an interior lot, should generally be similar to the principle dwelling but no closer than 3' from each property line. The rear setback may up to 3' from the rear property line. For corner lots, the DADU or outbuilding should match the context of homes on the street. If there is no context, the street setback should be a minimum of 10'.

Driveway Access.

- *On lots with no alley access, the lot shall have no more than one curb-cut from any public street for driveway access to the principal structure as well as the detached accessory dwelling or outbuilding.*
- *On lots with alley access, any additional access shall be from the alley and no new curb cuts shall be provided from public streets.*

Parking accessed from any public street shall be limited to one driveway for the lot with a maximum width of twelve feet.

Additional Requirements for DADUs from Ordinance 17.16.030. See requirements for outbuildings for additional requirements.

- *The lot area on which a DADU is placed shall comply with Table 17.12.020A.*
- *The DADU may not exceed the maximums outlined previously for outbuildings.*
- *No additional accessory structure shall exceed two hundred square feet when there is a DADU on the lot.*
- *Density. A DADU is not allowed if the maximum number of dwelling units permitted for the lot has been*

met.

· *Ownership.*

· *a. No more than one DADU shall be permitted on a single lot in conjunction with the principal structure.*

· *b. The DADU cannot be divided from the property ownership of the principal dwelling.*

o *The DADU shall be owned by the same person as the principal structure and one of the two dwellings shall be owner-occupied.*

o *Prior to the issuance of a permit, an instrument shall be prepared and recorded with the register's office covenanting that the DADU is being established accessory to a principal structure and may only be used under the conditions listed here.*

Bulk and Massing. The living space of a DADU shall not exceed seven hundred square feet.

i. Utilities

Utility connections such as gas meters, electric meters, phone, cable, and HVAC condenser units should be located so as to minimize their visibility from the street.

Generally, utility connections should be placed no closer to the street than the mid point of the structure.

Power lines should be placed underground if they are carried from the street and not from the rear or an alley.

2. ADDITIONS

- a. Generally, an addition should be situated at the rear of a building in such a way that it will not disturb either front or side facades. Additions normally not recommended on historic structures may be appropriate for non-historic structures in Cherokee Park. Front or side alterations to non-historic buildings that increase habitable space or change exterior height should be compatible, by not contrasting greatly, with the adjacent historic buildings.

Placement

Additions should be located at the rear of an existing structure.

Connections to additions should, as much as possible, use existing window and door openings rather than remove significant amounts of rear wall material.

Generally, one-story rear additions should inset one foot, for each story, from the side wall.

Additions should be physically distinguished from the historic building and generally fit within the shadow line of the existing building.

In order to assure that an addition has achieved proper scale, the addition should:

· *No matter their use, not be larger than the existing house, not including non-historic additions, in order to achieve compatibility in scale. This will allow for the retention of small and medium size homes in the neighborhood. The diversity of housing type and size is a character defining feature of the historic districts.*

· *Additions which are essentially a house-behind-a-house with a long narrow connector are not appropriate, as the form does not exist historically. Short or minimal connections that do not require the removal of the entire back wall of a historic building are preferred.*

· *Generally be shorter and thinner than the existing building. Exceptions may be made when unusual constraints make these parameters unreasonable, such as:*

· *An extreme grade change*

· *Atypical lot parcel shape or size*

In these cases, an addition may rise above or extend wider than the existing building; however, generally the addition should not higher and extend wider.

When an addition needs to be taller:

Whenever possible, additions should not be taller than the historic building; however, when a taller

addition is the only option, additions to single story structures may rise as high as 4' above the shadow line of the existing building at a distance of 40' from the front edge of the existing building. In this instance, the side walls and roof of the addition must set in as is typical for all additions. The portion of the roof that can be seen should have a hipped, side gable or clipped gable roof to help decrease the visual mass of the addition.

When an addition needs to be wider:

Rear additions that are wider than an existing historic building may be appropriate when the building is narrower than 30' or shifted to one side of the lot. In these instances, a structural alcove or channel must separate the existing building from the new addition. The structural alcove should sit in a minimum of 1' and be at least twice as long as it is deep.

In addition, a rear addition that is wider should not wrap the rear corner.

Ridge raises

Ridge raises are most appropriate for one-story, side-gable buildings, (without clipped gables) and that require more finished height in the attic. The purpose of a ridge raise is to allow for conditioned space in the attic and to discourage large rear or side additions. The raised portion must sit in a minimum of 2' from each side wall and can be raised no more than 2' of total vertical height within the same plane as the front roof slope.

Sunrooms

Metal framed sunrooms, as a modern interpretation of early green houses, are appropriate if they are mostly glass or use appropriate cladding material for the district, are located at the rear in a minimally visible location, are minimally attached to the existing structure, and follow all other design guidelines for additions.

Foundation

Foundation walls should set in from the existing foundation at the back edge of the existing structure by one foot for each story or half story. Exception: When an addition is a small one-room deep (12' deep or less) addition that spans the width of the structure, and the existing structure is masonry with the addition to be wood (or appropriate substitute siding). The change in material from masonry to wood allows for a minimum of a four inch (4") inset.

Foundation height should match or be lower than the existing structure.

Foundation lines should be visually distinct from the predominant exterior wall material. This is generally accomplished with a change in materials.

Roof

The height of the addition's roof and eaves must be less than or equal to the existing structure.

Visually evident roof slopes should match the roof slopes of the existing structure, and roof planes should set in accordingly for rear additions.

Skylights should not be located on the front-facing slope of the roof. Skylights should be flat (no bubble lenses) with a low profile (no more than six inches tall) and only be installed behind the midpoint of the building).

Side Additions

b. When a lot width exceeds 60 feet or the standard lot width on the block, it may be appropriate to add a side addition to a historic structure. The addition should set back from the face of the historic structure and should be subservient in height, width and massing to the historic structure.

Side additions should be narrower than half of the historic building width and exhibit a height of at least 2' shorter than the historic building.

To deemphasize a side addition, the roofing form should generally be a hip or side-gable roof form.

c. The creation of an addition through enclosure of a front porch is not appropriate. The creation of an addition through the enclosure of a side porch may be appropriate if the addition is constructed in such a way that original form and openings on the porch remain visible and undisturbed.

Side porch additions may be appropriate for corner building lots or lots more than 60' wide.

d. Contemporary designs for additions to existing properties are not discouraged when such additions do not destroy significant historical, architectural, or cultural material; and when such design is compatible, by not contrasting greatly, with the size, scale, color, material, and character of the property, neighborhood, or environment.

e. A new addition should be constructed in such a manner that if the addition were to be removed in the future, the essential form and integrity of the original structure would be unimpaired.

Connections should, as much as possible, use existing window and door openings rather than remove significant amounts of rear wall material.

f. A new addition should be constructed in such a manner that if the addition were to be removed in the future, the essential form and integrity of the original structure would be unimpaired.

g. Additions should follow the guidelines for new construction.

III.B.1 Demolition is Not Appropriate

- a. if a building, or major portion of a building, is of such architectural or historical interest and value that its removal would be detrimental to the public interest; or
- b. if a building, or major portion of a building, is of such old or unusual or uncommon design and materials that it could not be reproduced or be reproduced without great difficulty and expense.

III.B.2 Demolition is Appropriate

- a. if a building, or major portion of a building, has irretrievably lost its architectural and historical integrity and significance and its removal will result in a more historically appropriate visual effect on the district;
- b. if a building, or major portion of a building, does not contribute to the historical and architectural character and significance of the district and its removal will result in a more historically appropriate visual effect on the district; or
- c. if the denial of the demolition will result in an economic hardship on the applicant as determined by the MHZC in accordance with section 17.40.420 (Historic Zoning Regulations), Metropolitan Comprehensive Zoning Ordinance.

Background: The structure at 3925 Cambridge Avenue is a one-story brick house, constructed in the 1920s. The building has a hipped roof and small central front stoop and a side porch. The house is an example of the Craftsman architectural style, and it is contributing to the historic character of the district.



Figure 1: 3925 Cambridge

The building is shifted to the right on a lot that is eighty feet (80') wide, which is twenty feet (20') wider than the average lot on the street, giving the house a large left side yard. The lot is also only one hundred feet, five (105') deep with no alley at the rear.

The applicant proposed a similar addition at the November 2018 public hearing, which was disapproved. Commissioners expressed concern over how close the garage doors on the addition would be to the front of the house and the greatly reduced rear setback. The current proposal shows the garage component of the addition pushed further back and retains other elements of the previous proposal that Commissioners said met the design guidelines. The proposed rear setback remains at five feet (5').

Demolition: The proposed addition would “wrap” the rear-left corner of the house, and involves demolishing portions of the house at the rear and left side of the original building. In general, additions are most appropriate at the rear or in certain situations they may be appropriate on the side, but wrapping a corner is generally not appropriate because doing so requires more demolition of the original building and makes the addition less reversible.

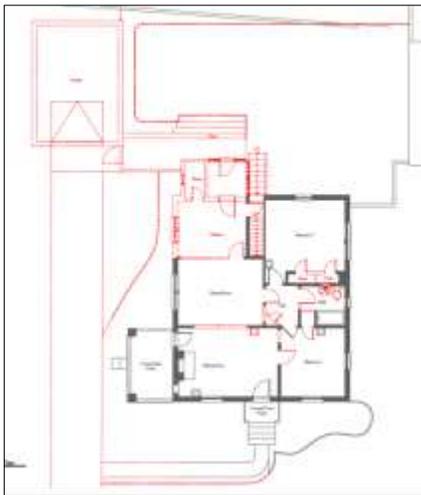


Figure 2: Demolition plan, November 2018 Submittal

Staff finds that the circumstances of this lot, however, could justify an addition wrapping the corner. Because the lot is eighty feet (80') wide and only one hundred, five feet (105') deep, the buildable area is more to the side than the rear, unlike the typical situation for most houses. Because of the atypical lot dimensions at 3925 Cambridge Avenue, Staff finds that this partial demolition at the corner would meet section III.B.2 of the guidelines for appropriate demolition,

The proposal also includes demolition of a non-contributing outbuilding, which meets section III.B.2 of the guidelines for appropriate demolition,

Location & Removability: The proposed addition will attach to the historic house at the midpoint of the rear-wall and on the left side twenty-five feet (25') back from the front, wrapping the rear-left corner but leaving the front two-thirds of the left wall unaltered. The front and right side will not be impacted by the addition.

Although an addition that wraps the around the side and rear of an historic house is generally not appropriate, the shallowness of the lot and the setback requirements make an addition attached at the rear-only impractical. Considering the conditions of the lot, Staff finds that the location and corner attachment of an addition may be appropriate and meets section II.B.2.a and II.B.2.e of the design guidelines.

Because of the substantial impact wrapping the corner would have on an historic house, it's important that additional care is given to ensure that the scale and setbacks are appropriate to avoid compromising the house's form and integrity. This is discussed further and in greater detail in the analysis for "Height & Scale" and "Setback & Rhythm of Spacing."

The proposal also includes enclosing the side porch with full-light multi-pane doors. Staff finds that the side porch enclosure would meet II.B.2.e of the design guidelines because it is planned to be constructed in such a way that original form and openings on the porch remain visible and undisturbed.

Design: The addition will have hipped roofs with wide eaves, in keeping with the simple Craftsman character of the historic house. The form of the addition will be distinguished from the original building with a different but compatible exterior cladding.

Staff finds that the design of the addition does not contrast with the historic house; therefore meets sections II.B.2.a and II.B.2.f of the design guidelines.

Height & Scale: The addition will have two components, both one-story tall. The primary component will consist of a garage and master suite, connected to the historic house by a central "hyphen" component containing a great room. The garage/master component will have a hipped roof with a ridge peak two feet (2') lower than the peak of the historic house's roof. The hyphen's roof will tie in one foot, six inches (1'-6") below the ridge of the garage and three feet (3') lower than the peak of the house. The eaves of both components will align with the eaves of the historic house.

The hyphen component of the addition will attach to the left side of the house twenty-five feet (25') back from the front and will be nineteen feet (19') wide, extending eleven feet (11') past the house's eight foot (8') deep side porch. The garage component will extend another twenty-three feet (23') wide, stepping forward seven feet (7') from the hyphen up to the midpoint of the house. The width of the addition, thirty-four feet (34') wide from the edge of the porch will be nearly equal to that that of the thirty-nine foot (39') wide historic house. The left side of the garage component will be forty-two feet (42') deep, six feet (6') deeper than the historic house.

When appropriate, side additions are typically approved to add up to half the width of an historic house and are typically required to be kept at or behind the house’s midpoint.

The location and width of the proposed addition is appropriate given the unusual dimensions of the lot, and that the height of the addition is subordinate to that of the existing roof. Staff finds that the proposal would meet section II.B.1.b of the design guidelines.

Setback & Rhythm of Spacing: The lot is one hundred, five feet (105’) deep on the right and one hundred, eleven feet (111’) on the left and the house is located only thirty-two feet (32’) from the rear property line. With a standard rear setback requirement of twenty feet (20’), the buildable area behind the house would be limited to approximately twelve feet (12’) deep.

The applicant is proposing an addition that would extend the depth of the house by twenty-eight feet (28’), coming to five feet (5’) of the rear property line. The Commission has the authority to determine setbacks when doing so is appropriate for the historic context, and given the size of the lot. Staff finds that the reduction of the rear setback may be appropriate but is concerned with the amount of reduction for a lot with no rear alley. In the past, the Commission has reduced the rear setback to ten feet (10’) and sometimes less in cases where the property backed up to a railroad or high-density development. In this case, the property backs up to the rear of two properties with the same residential use and zoning, have developed in the same manner, and that are also located in the Cherokee Park Neighborhood Conservation Zoning Overlay. The lots behind are also less than the typical depth of one-hundred and fifty square feet (150’) and would also have minimal opportunity for rear additions. Because the circumstances between 3925 Cambridge Avenue and 4020 and 4018 Aberdeen Road are similar, the same rear setback approved here would be appropriate for all three lots. The result could be a large stretch of area with just ten feet (10’) between the rear walls of buildings in a situation where they would normally have at least forty feet (40’). For this reason, Staff does not recommend that the rear setback is reduced to be less than ten feet (10’).

With a condition that the rear setback is reduced to be no less than ten feet (10’), Staff finds that the proposed addition would meet section II.B.1.c of the design guidelines.

Materials:

	Proposed	Color/Texture/ Make/ Manufacturer	Typical of Neighborhood	Requires Additional Review
Foundation	Concrete Block	Split Face	Yes	
Cladding	Cement-fiber Clapboard	Smooth, Match Existing Reveal	Yes	
Trim	Wood		Yes	
Roofing	Asphalt	Match Existing	Yes	

	Shingles	Color		
Windows	Wood		Yes	X
Doors	Fiberglass	Needs Final Approval	Yes	X
Garage doors	Steel		Yes	
Side Porch Floor/steps	No Change			
Side Porch Posts	No Change			
Side Porch Doors	Fiberglass, Divided Light	Needs Final Approval		X
Side Porch Roof	No Change			
Driveway	Front Drive, Parking Pad	Needs Final Approval	Unknown	X
Chimney	Brick	Needs Final Approval	Yes	X
Terrace	Unknown	Needs Final Approval		X
Rear Deck and Railing	Wood		Yes	
Walkways	Unknown	Needs Final Approval		X

With a condition that the windows, doors, and paving material are administratively approved Staff finds that the project meets section II.B.1.d of the design guidelines.

Roof form: The roof of the addition will be hipped on the garage components with a 6:12 pitch, and a matching pitch on the hyphen. These roof forms and pitches match the hipped roof of the historic house. Staff finds that the roof forms of the addition are compatible with the historic house and the project would meet section II.B.1.e of the design guidelines.

Proportion and Rhythm of Openings: The windows on the proposed addition are vertically oriented and, with the exception of three square windows on the left side of the garage, are generally twice as tall as they are wide or taller. These proportions are compatible with the proportions of openings on the historic house. There are no large expanses of wall space without a window or door opening. Staff finds the project’s proportion and rhythm of openings to meet section II.B.1.g of the design guidelines.

Appurtenances & Utilities: There is currently a driveway to the left side of the house leading to an existing garage at the rear of the lot. The site plan indicates that the existing drive will be widened near the garage entrance. There are no alleys in the Cherokee Park neighborhood, driveways are common. Two walkways will be added. One will lead

from the front door to the street and the other from the front door to the driveway, an area which is currently pavers. The location of the HVAC units is shown to be located to the left of the garage, beyond the midpoint of the building. This location is appropriate and meets section II.B.1.i of the design guidelines.

Outbuildings: Although the proposal includes a garage, it was reviewed under the design guidelines for additions rather than section II.B.1.h of the design guidelines because it is attached and integral to the form of the addition.

Recommendation: Staff recommends approval of the proposed addition to 3925 Cambridge Avenue with the following conditions:

1. The rear setback shall be a minimum of ten feet (10'); and
2. The window and door selections and paving material shall be administratively approved.

Staff finds that the proposed addition would then meet the design guidelines for additions in the Cherokee Park Neighborhood Conservation Zoning Overlay.

ATTACHMENT A: PHOTOGRAPHS



3925 Cambridge Avenue, front.



3925 Cambridge Avenue, left showing side yard.



3925 Cambridge Avenue, right.



Extensions + Renovations to:
3925 Cambridge Avenue
Nashville, Tennessee 37205

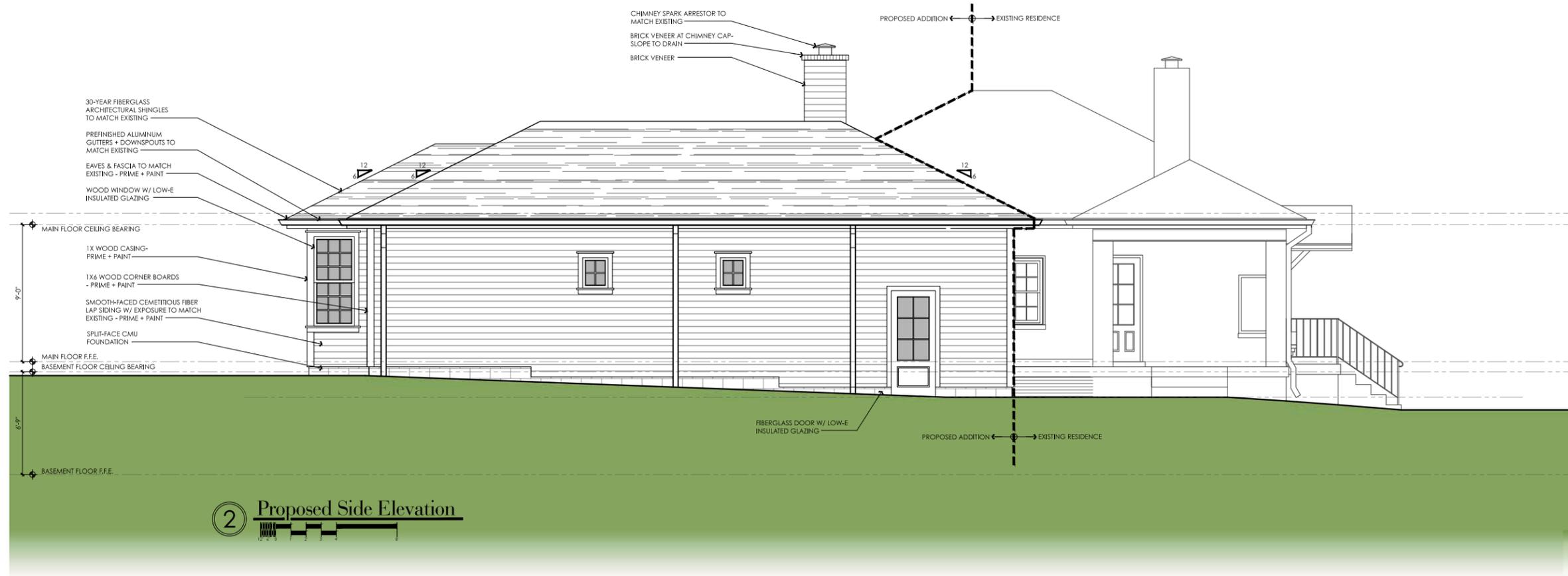
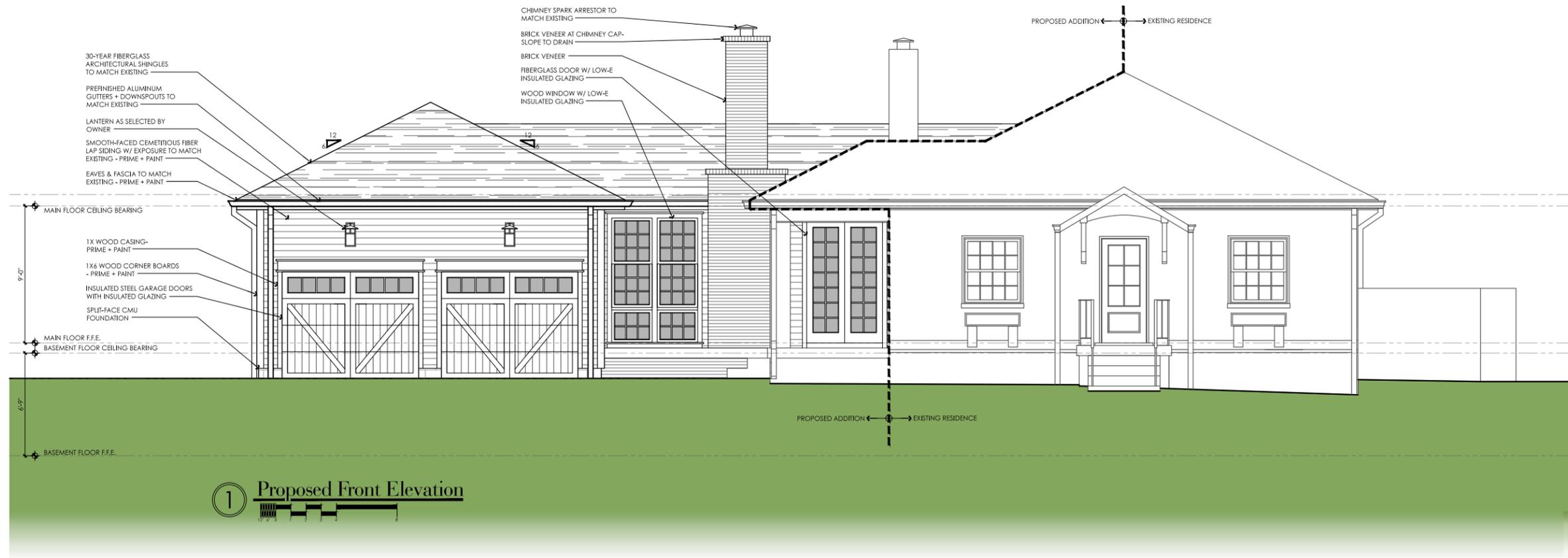
**HISTORIC ZONING COMMISSION SUBMITAL
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DATE OF ISSUANCE:
24 October 2018
REVISED: 03 December 2018
EXISTING SITE PLAN

L0



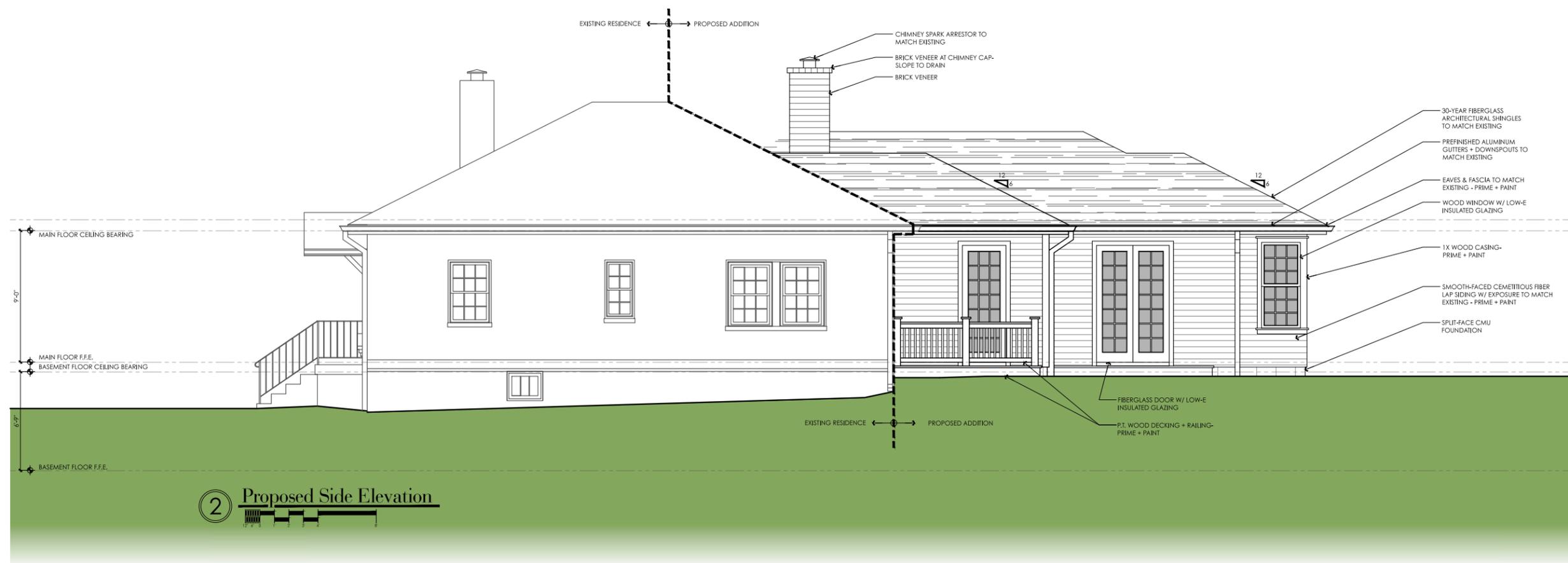
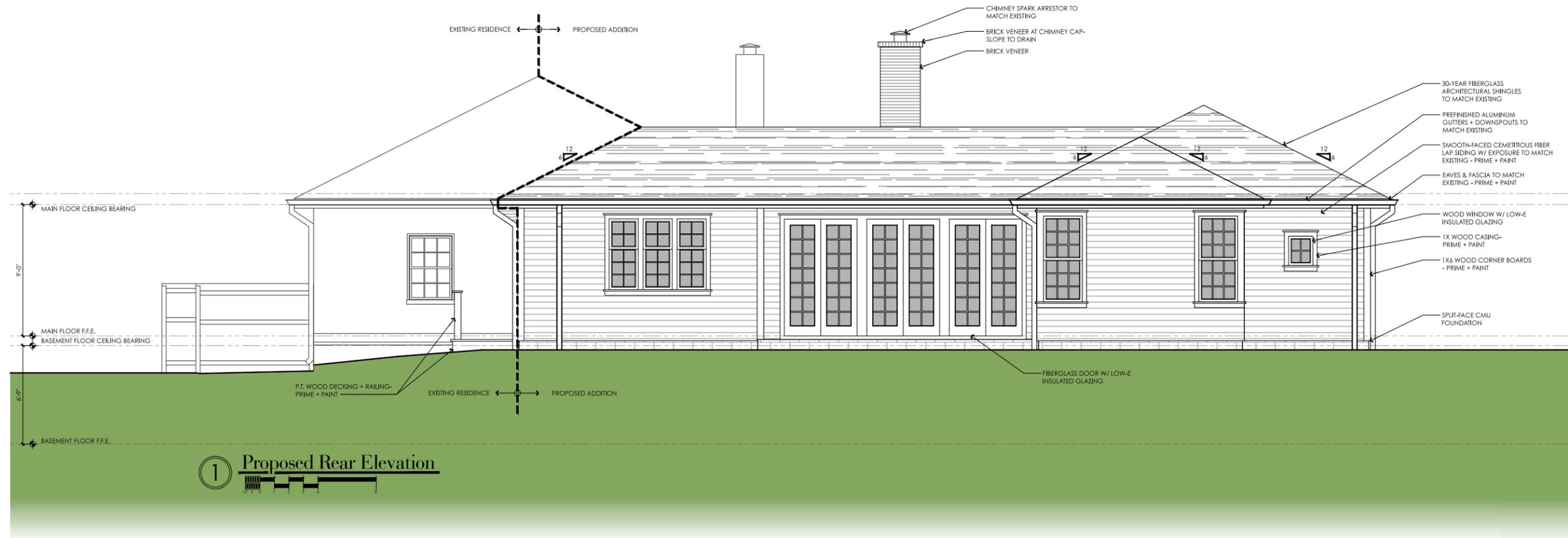
CAMBRIDGE AVENUE



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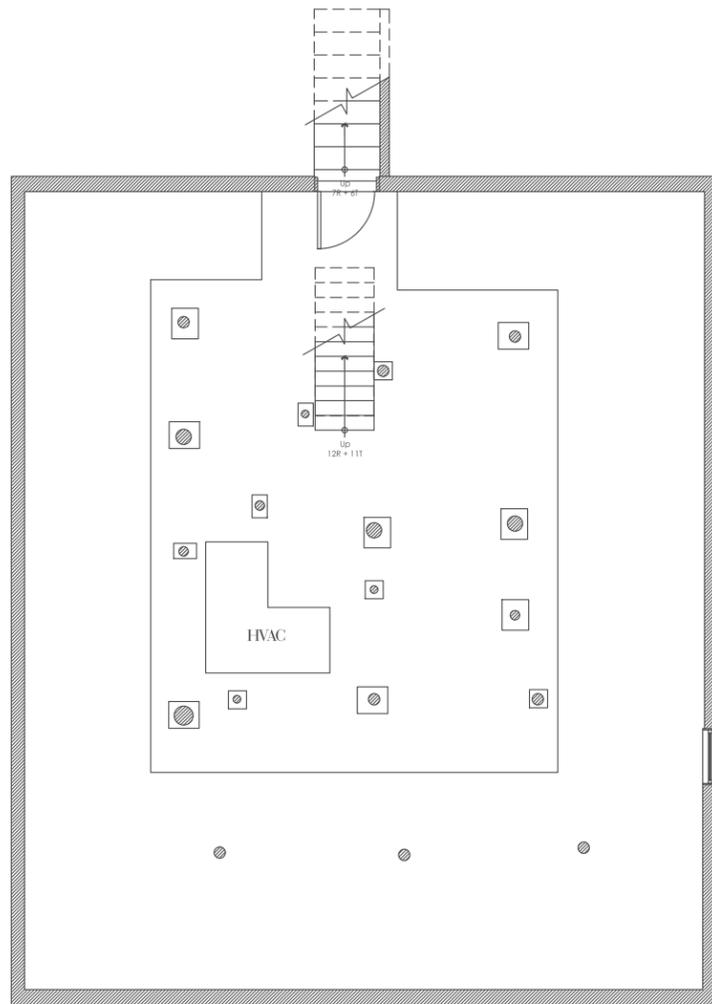
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PROPOSED ELEVATIONS



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PROPOSED ELEVATIONS



①

Existing Basement Plan



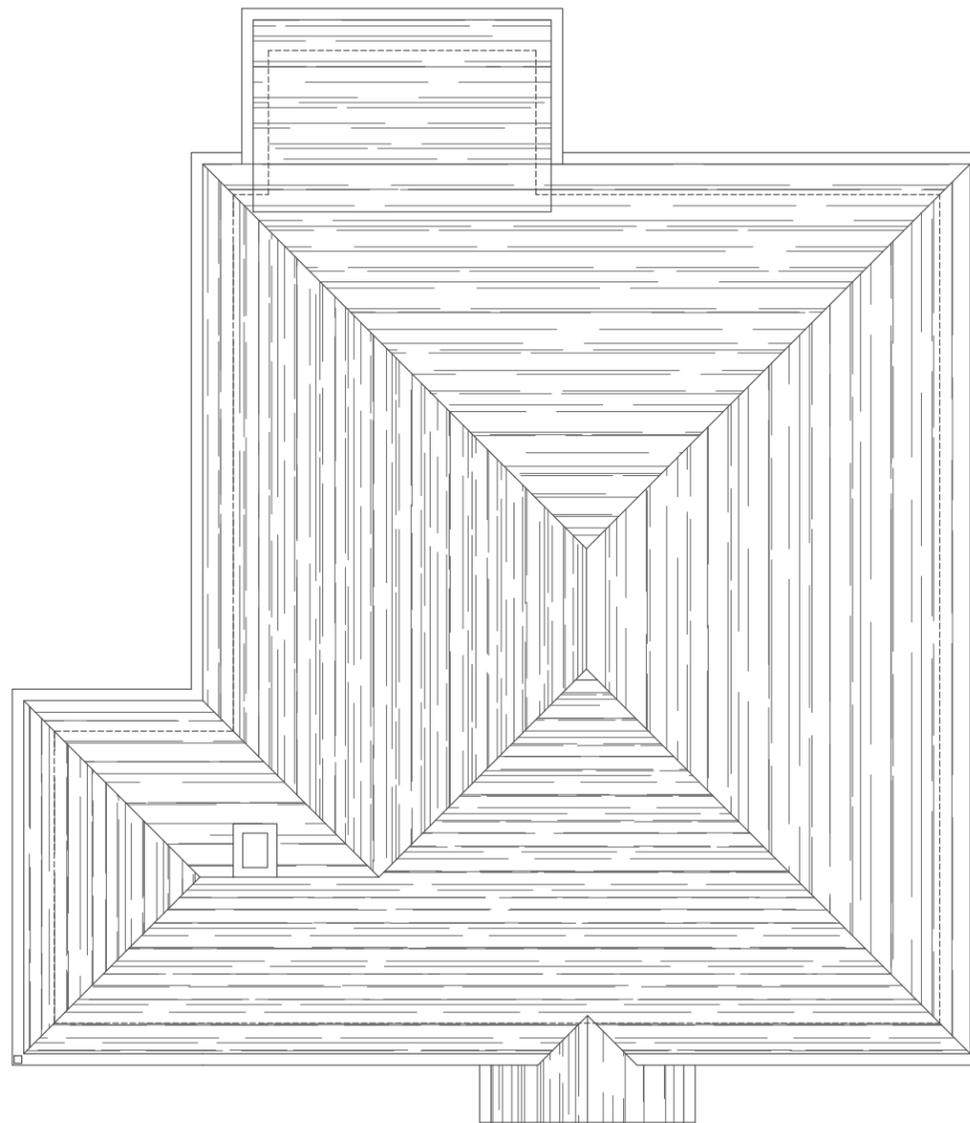
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EXISTING BASEMENT PLAN

EX0

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① Existing Roof Plan

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EXISTING ROOF PLAN

EX2



① Existing Front Elevation

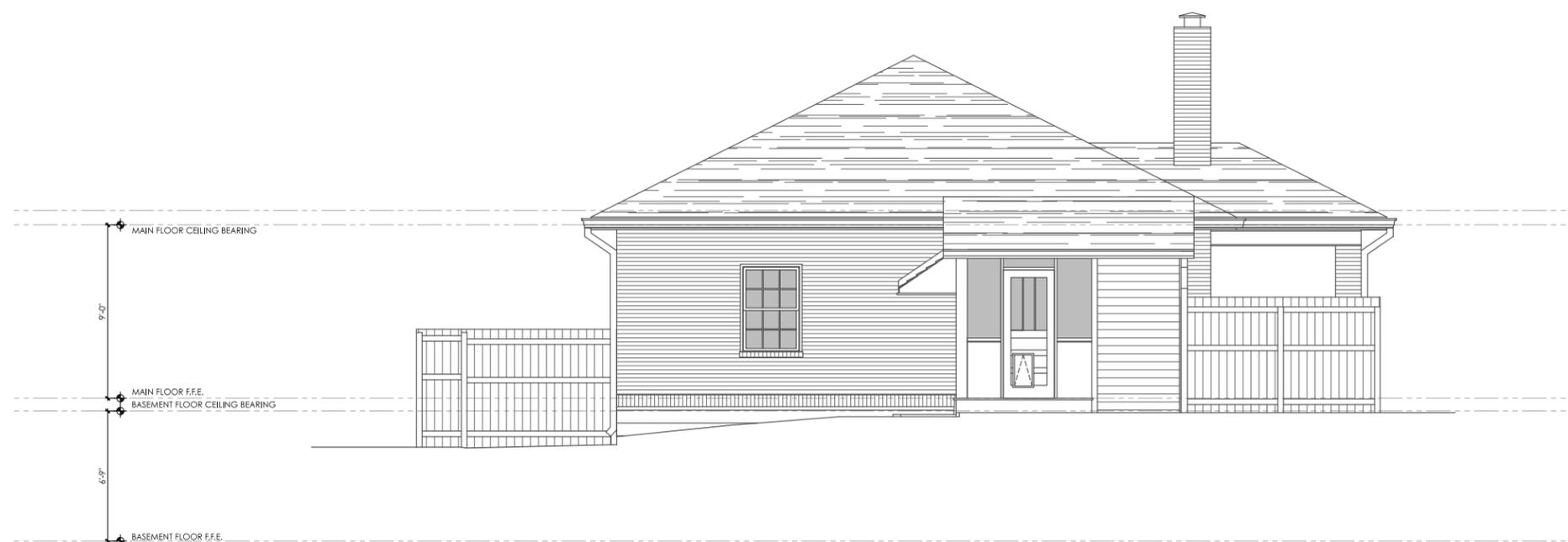


② Existing Side Elevation

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EXISTING FRONT + SIDE
ELEVATIONS

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① Existing Rear Elevation



② Existing Side Elevation

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EXISTING REAR + SIDE
ELEVATIONS