



Antioch-Priest Lake Community Plan Appendix A: Rural Hill-Moss Road Detailed Design Plan

**Design Plan adopted January 28, 2010; Amended October 25, 2012
Readopted as part of the NashvilleNext General Plan on June 22, 2015**

Mission Statements

The Planning Commission guides growth and development as Nashville and Davidson County evolve into a more socially, economically and environmentally sustainable community, with a commitment to preservation of important assets, efficient use of public infrastructure, distinctive and diverse neighborhood character, free and open civic life, and choices in housing and transportation.

The Planning Department helps Nashville and Davidson County evolve into a more sustainable community, guided by a commitment to efficient use of infrastructure, distinctive and diverse community character, open and vibrant civic life, and choices in housing and transportation focused on improving the quality of life.

Acknowledgements

The staff of the Metropolitan Planning Department thanks the many stakeholders who participated in the development of the Rural Hill - Moss Road Detailed Design Plan. Their thoughtful engagement on the vision for this area has been invaluable.

Special thanks are due to the following persons and entities:

St. Ignatius Church for graciously hosting the majority of the community meetings.

Apollo Middle School for hosting the Visioning Workshop.

Councilmen Duane Dominy and Robert Duvall for their leadership and participation.

And to the numerous community stakeholders who spent their time working on this plan for a bright future for the Rural Hill - Moss Road Community.

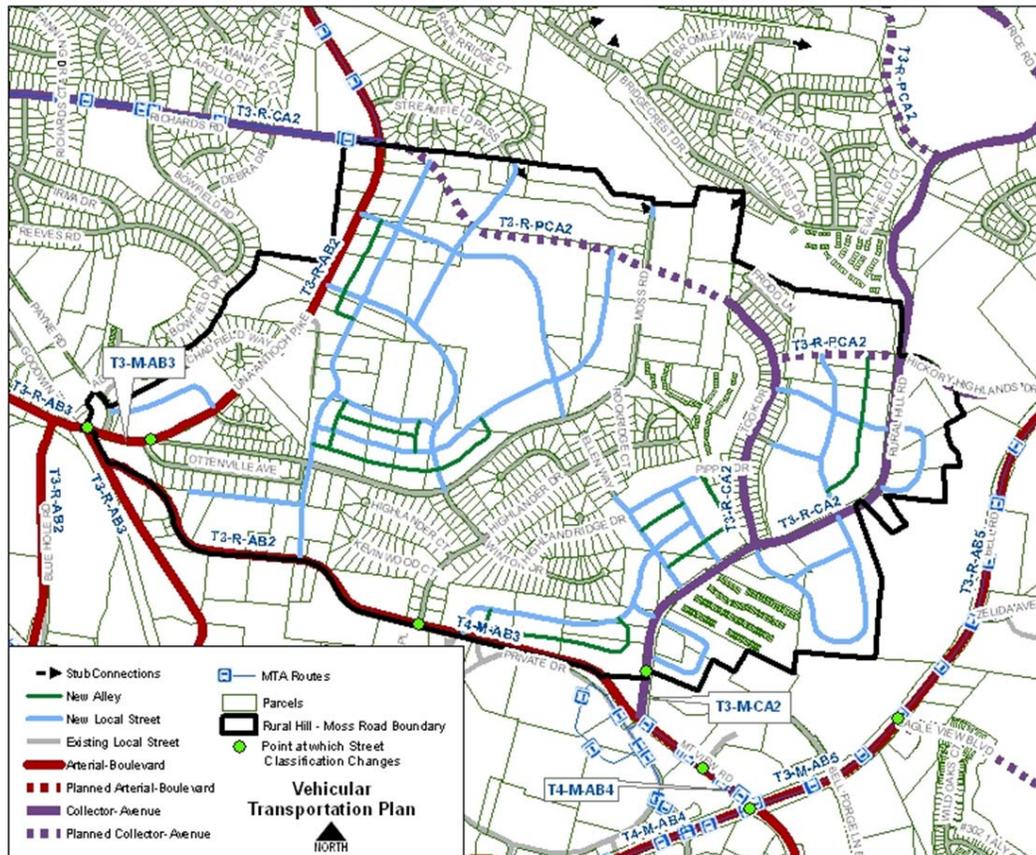
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The Rural Hill – Moss Road Detailed Design Plan was originally adopted by the Metropolitan Planning Commission on January 28, 2010. With the adoption of the *Antioch-Priest Lake Community Plan: 2012 Update* on October 25, 2012, the following amendments were incorporated into the Rural Hill – Moss Road Detailed Design Plan:

1. Replace *Figure 16: Vehicular Transportation Plan Map* on page 60 with the following updated figure:



2. Replace the section entitled **2007-2013 Capital Improvements Budget (CIB)** beginning on page 61 to the following:

2012-2018 Capital Improvements Budget (CIB)

The Mayor and Metro Council are responsible for approving funding for Capital Improvement projects and Metro Public Works is the lead agency for building street, sidewalk, and bikeway projects. Two projects for the area are recommended to proceed as described in the CIB. A series of projects are recommended to be amended so projects can be consolidated where duplication exists and to update project descriptions. Two projects are recommended to be deleted based upon the recommendations from the originally adopted Rural Hill – Moss Road Detailed Design Plan.

Table 4: Capital Improvements Budget (CIB) Projects in Rural Hill – Moss Road

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Rural Hill Road and Mt. View Road Intersection Improvements	08PW0018	Improve the Rural Hill Road at Mt. View Road intersection	FY 13 - \$500,000	
Rural Hill Road Resurfacing	13PW0033	Resurface Rural Hill Road from Mt. View Rd Intersection to entrance of Free Will Baptist Church Headquarters	FY 13 - \$50,000	

Table 5: Capital Improvements Budget (CIB) Recommended Amendments

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Antioch Pike Widening	92PW003	Phase 1 - Widen On Present Alignment From Richards Road To Reeves Road; Phase 2 - Construct On New Alignment From Reeves Road To Blue Hole Road Includes New Bridge Over Mill Creek And Roadway.	FY 13 - \$350,000 FY 14 - \$350,000 FY 15 - \$700,000 FY 16 - \$2,000,000 FY 17 - \$4,692,000 FY 18 - \$800,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road. -Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways. -Similar project as 10PW0027 and 13PW008; consider combining projects for clarity.				
Antioch Pike Widening	10PW0027	Widen Antioch Pike from Richards Road to Hickory Hollow Pkwy	FY 17 - \$12,900,000 FY 18 - \$5,600,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road. -Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways. -Similar project as 92PW003 and 13PW008; consider combining projects for clarity.				

Table 5: Capital Improvements Budget (CIB) Recommended Amendments Continued

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Antioch Pike Strategic Improvements	13PW0008	Widen from Haywood Lane to Blue Hole Road. Project includes signal upgrades, sidewalks and ROW acquisition. ROW, Eng, Design, Construction.	FY 13 - \$200,000 FY 14 - \$6,400,000	
<p>Recommended change to CIB Project:</p> <p>-Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road.</p> <p>-Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways.</p> <p>-Similar project as 92PW003 and 10PW0027; consider combining projects for clarity.</p>				
Hickory Hollow Parkway Extension	91PW002A	From Blue Hole Rd To Hickory Hollow Parkway Includes Phase I Engineering and ROW - Construct On New Alignment Bridge And Approaches Over Mill Creek And CSX RR, And Phase 2 - Roadway Up Grade Along Hickory Hollow Parkway	FY 13 - \$1,000,000 FY 14 - \$1,000,000 FY 15 - \$5,000,000 FY 16 - \$5,000,000 FY 17 - \$5,000,000 FY 18 - \$3,000,000	
<p>Recommended change to CIB Project:</p> <p>-Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections.</p> <p>-Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan.</p>				
Rural Hill Road Widening	87PW004C	Widen Rural Hill Road from Murfreesboro Pike to Mt. View Road	FY 13 - \$2,000,000 FY 14 - \$2,000 FY 15 - \$2,000,000 FY 16 - \$2,000,000 FY 17 - \$2,000,000 FY 18 - \$3,500,000	
<p>Recommended change to CIB Project:</p> <p>-Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections.</p> <p>-Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan.</p> <p>-Similar project as 10PW0030; consider combining projects for clarity.</p>				
Rural Hill Road Widening	10PW0030	Widen Rural Hill Road from Bridgecrest Drive to Mt. View Road	FY 13 - \$5,000,000	
<p>Recommended change to CIB Project:</p> <p>-Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections.</p> <p>-Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan.</p> <p>-Similar project as 87PW004C; consider combining projects for clarity.</p>				

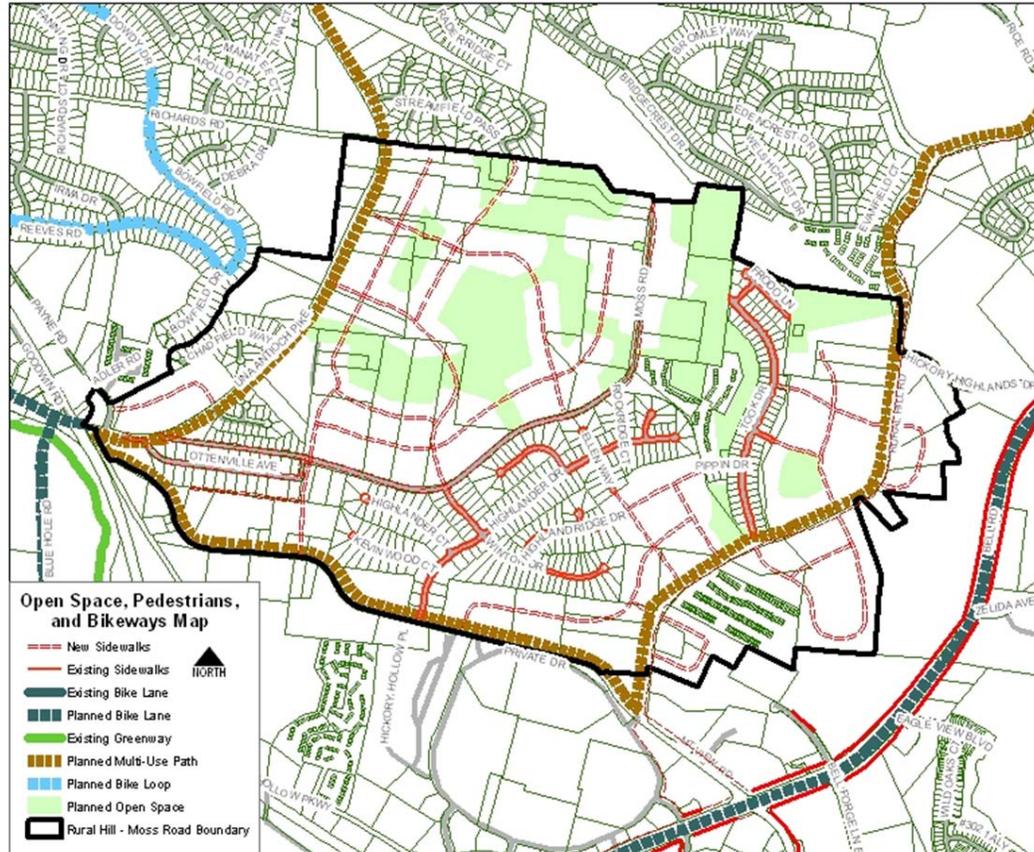
Table 6: Recommended Projects to be Removed from the Capital Improvements Budget (CIB)

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Una-Antioch Pike Widening	85PW016A	Widen Una-Antioch Pike from Murfreesboro Pike to 800 feet east of Hickory Hollow Parkway	FY 13 - \$100,000 FY 14 - \$100,000 FY 15 - \$250,000 FY 16 - \$250,000 FY 17 - \$1,000,000 FY 18 - \$1,000,000	
Recommended change to CIB Project: -Remove from CIB. -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Similar project as 10PW0028; recommend removing this project as well.				
Una-Antioch Pike Widening	10PW0028	Widen Una-Antioch Pike from Hickory Hollow Pkwy to Murfreesboro Road	FY 13 - \$20,500,000	
Recommended change to CIB Project: -Remove from CIB. -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Similar project as 85PW016A; recommend removing this project as well.				

3. Change the section entitled **CIB Recommendation** on page 62 to **Additional CIB Guidance**.
4. Delete the first paragraph on page 64 starting with "Table 4 in the Appendix..."
5. Replace the second paragraph that begins with "Route 38X, Antioch Express..." under the section entitled **Transit Recommendations** on page 64 with the following:

Route 38X – Antioch Express currently skirts the Rural Hill-Moss Road community via Richards Road and Una Antioch Pike. Just south of the community, Route 15 – Murfreesboro Road services Hickory Hollow Mall with buses along Bell Road and Murfreesboro Pike to Downtown Nashville. Route 27X – Hickory Hollow Express also services the mall and utilizes Interstate 24. A Park and Ride location exists at the mall. MTA plans to enhance transit service in the future with a mini-hub to co-locate with the new Park, Community Center and Library that Metro is constructing. This will also be the terminus of the new MTA Bus Rapid Transit Light Route, via Murfreesboro Road and Bell Road, from Downtown Nashville to Antioch/Hickory Hollow. The new route will include more permanent transit stations with travel time displays and more frequent service along Bell Road and Murfreesboro Pike into Downtown Nashville. Opportunities exist to better connect the Rural Hill-Moss Road community to this improved transit service.

6. Replace Figure 20: Open Space, Pedestrians and Bikeways Plan Map on page 66 with the following updated figure:



7. Replace the second paragraph that begins with "In the future..." under **Recommended Pedestrian Facilities for the Rural Hill-Moss Road Community** on page 68 to page 69 with the following:

In the future, new development is required to provide pedestrian facilities on both sides of new and most existing streets in the study area. In most cases, these will be standard sidewalks with curb, gutter, and planting strip. However, along the north side of Rural Hill Road, the east side of Mt. View Road, and the north side of Una Antioch Pike, pedestrian facilities will take the form of a multi-use path with a swale instead of gutter for stormwater drainage. These planned sidewalk improvements are depicted in Figure 20.

Additional pedestrian facilities include crosswalks. Crosswalks should be added to the intersection of Hickory Highland Drive and Bell Road. When sidewalks are completed crosswalks should also be added to the intersection of Rural Hill Road and Mt. View Road to improve access to the civic services and retail at Hickory Hollow Mall. Crosswalks are also needed at the

Ottensville Road, Una Antioch, and Hickory Hollow Parkway intersections. Crosswalks within neighborhood centers, commercial corridors, parks and near schools should receive regular maintenance to maintain pedestrian safety.

8. Add to the end of the section entitled **Bicycle Facilities** on page 69 the following:

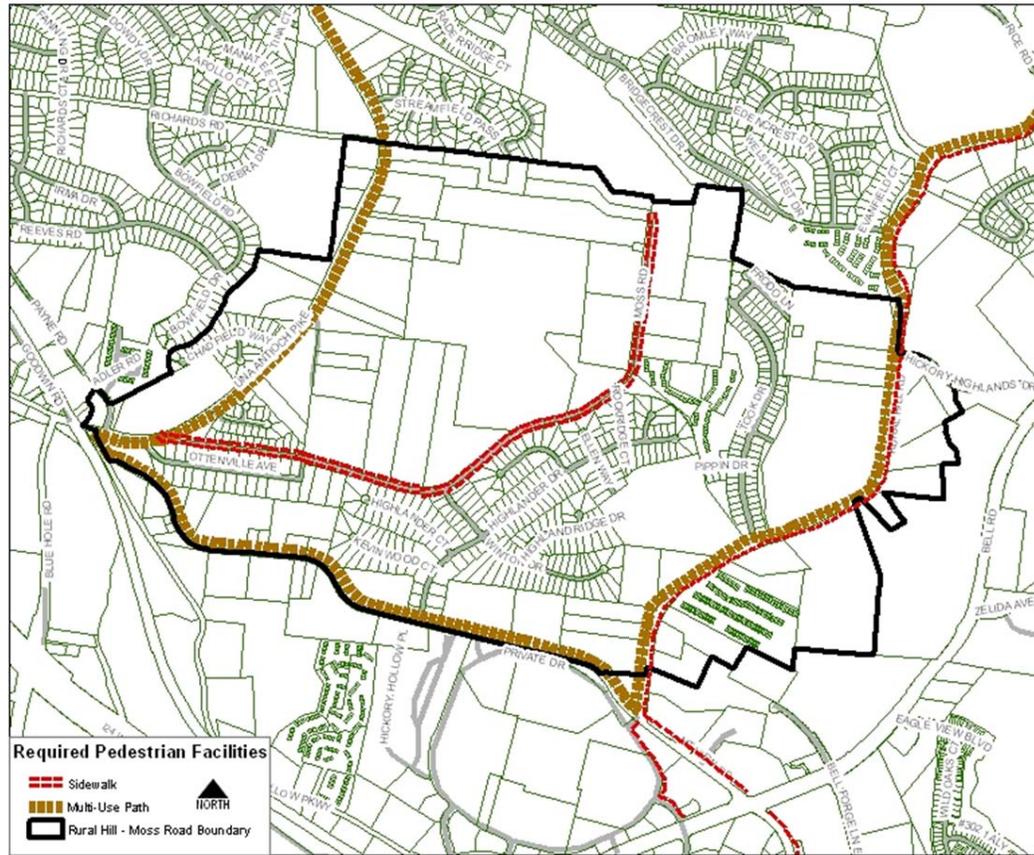
Different bikeway facilities are attractive to certain types of cyclists. Cycling options not on the community's busiest streets have been noted and designated as *bike loops*. These loops create routes through a series of bike lane and bike routes. They are intended to attract most riders onto low traffic streets for recreational purposes. In some cases, the bike loops connect to a bike lane or bike route on a busier street, connecting recreational riders to routes that provide access to commercial and civic designations.

9. Replace the paragraph under the section **Existing Bikeways in the Rural Hill-Moss Road Community – The Strategic Plan for Sidewalks and Bikeways** on page 69 with the following:

There are no existing on-road bikeways in or near the community. The Mill Creek Greenway however, is adjacent to the community. It is located at the Antioch Park and Community Center, just west of the community. There is a section of the Mill Creek Greenway that is under development. It will extend and connect to another segment of the greenway near Metro's Ezell Park.

10. Delete the second bullet under the section **Planned Bikeways in the Rural Hill-Moss Road Community – The Strategic Plan for Sidewalks and Bikeways** on page 69.

11. Replace Figure 26: Required Pedestrian Facilities on page 78 with the following updated figure:



METROPOLITAN PLANNING COMMISSION OF NASHVILLE AND DAVIDSON COUNTY, TENNESSEE

Resolution No. RS2010-14

“BE IT RESOLVED by The Metropolitan Planning Commission that 2008CP-002G-13 is **APPROVED WITH CONDITION, AND THE DELETION OF THE WORD “SLIGHTLY” FROM PAGE 80, SECOND COLUMN, SECOND PARAGRAPH, LAST SENTENCE. (9-0)**

Conditions of Approval:

1. *Add the following item to the Special Policies for each of the Community Character Policies applied in the Rural Hill-Moss Road Detailed Design Plan:*

Historically Significant Sites or Features: There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.”

WHEREAS the Metropolitan Planning Commission directed the Planning Department staff to conduct open community meetings to provide the community the opportunity to work with the staff on the creation of a Detailed Design Plan for the Rural Hill-Moss Road Community, an area that lies within the larger Antioch-Priest Lake Community, and

WHEREAS from October 2008 through December 2009, the Metropolitan Planning Department staff worked extensively with residents, Council members, and property owners, conducting seven community meetings, and prepared a Detailed Design Plan for the Rural Hill-Moss Road Community, hereafter known as the *Rural Hill-Moss Road Detailed Design Plan*; and

WHEREAS the *Rural Hill-Moss Road Detailed Design Plan* provides more detailed guidance on planning for growth and preservation in the Rural Hill-Moss Road Community than that provided solely by the *Antioch-Priest Lake Community Plan: 2003 Update*; and

WHEREAS a public hearing was held by the Metropolitan Planning Commission on January 28, 2010 to obtain additional input regarding the proposed *Rural Hill-Moss Road Detailed Design Plan*; and

WHEREAS, the Metropolitan Planning Commission is empowered under state statute and the charter of the Metropolitan Government of Nashville and Davidson County to adopt functional plans as part of the general plan for the county;

NOW THEREFORE, BE IT RESOLVED, that the Metropolitan Planning Commission hereby ADOPTS the *Rural Hill-Moss Road Detailed Design Plan* as an amendment to the *Antioch-Priest Lake Community Plan: 2003 Update*, a component of the General Plan, in accordance with sections 11.504(e), (j), and 18.02 of the charter of the Metropolitan Government of Nashville and Davidson County as the basis for the Commission’s development decisions in that area of the county.

James McLean
James McLean, Chairman

Adoption Date: January 28th, 2010

Attest:
Rick Bernhardt
Rick Bernhardt, Secretary and Executive Director

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Plan Summary

The Rural Hill-Moss Road Community is a developing community that has a unique opportunity to develop into a walkable, sustainable community that preserves characteristics of its rural heritage. The community's location in close proximity to a regional mall, interstate access to Downtown Nashville, and other points of interest in Davidson, Rutherford, and Wilson Counties make it an attractive location for future development. Rather than developing in a disjointed, conventional suburban pattern, however, the Rural Hill-Moss Road Community could develop in a coordinated fashion that provides some housing choice, and unique amenities such as an extended open space network that provides recreational opportunities and preserves the rolling hills that give the community its unique semi-rural character.

The Rural Hill-Moss Road Community is currently split between new, suburban residential development and longstanding rural development. It is directly adjacent to high intensity mixed use development (Hickory Hollow Mall) and community facilities such as schools, public open space, and the Southeast Branch Library. Despite the proximity of services, it is difficult for community members to travel to these services because of a disconnected street network and a lack of sidewalks and crosswalks. Much of the newer suburban development has taken place in such a way that it does not connect to, or identify with, the existing community fabric. The pattern of development surrounding these remaining rural areas makes preservation in their current undeveloped state unlikely. Meanwhile, the community is uninterested in continuing the disconnected development pattern currently encroaching into the area. Therefore, the Rural Hill-Moss Road Detailed Design Plan seeks to create a new development pattern and a new community identity.

The Rural Hill-Moss Road Detailed Design Plan reflects the community's desire to create a sense of place and neighborhood identity for the area by capitalizing on the rural heritage and providing a different type of neighborhood than is found elsewhere in Antioch. The Rural Hill-Moss Road Plan weaves together the dissimilar elements of the community in a way that creates an integrated whole through the use of appropriate and seamless transitions between existing

and new development patterns, the preservation of significant natural features, the provision of vehicular and pedestrian connectivity, and necessary infrastructure to support new development.

Throughout the community meetings, attendees consistently expressed a desire to preserve the natural beauty of the community. The Rural Hill-Moss Road Detailed Design Plan proposes significant open space preservation for recreation, stormwater management and the protection of natural features. The proposed open space network preserves environmental features such as steep slopes, stream corridors, and viewsheds and ties the community together and to adjacent points of interest such as commercial centers, parks, community facilities, and adjoining neighborhoods. The open space network acts as a focal point and organizing feature of the community to help further the goal of building a sense of identity and creating a unique community in Antioch.

The plan anticipates that the community will remain largely residential in character, with future development contributing to the sustainability, cohesiveness and unique character of the community. Residents expressed a desire for a range of housing types and values. Providing varied housing choices allows individuals to remain living in the Rural Hill-Moss Road community even as their housing needs and circumstances change. Housing choices predominantly take the form of single-family houses and cottages. Strategically located, higher intensity development such as townhomes serves as a transition between the existing development of Bell Road and the primarily single-family development within the Rural Hill-Moss Road community. All housing should be designed keeping in mind the environmental constraints and natural beauty of the remaining undeveloped lands.

The Rural Hill-Moss Road Plan calls for the preservation and enhancement of the historic neighborhood center located at Una Antioch Pike, Moss Road, and Mt. View Road to re-establish the center of the community. This center should provide a mixture of housing, small-scale commercial, and community services. New development should capture the character of downtown Antioch.



A multi-use path envisioned along Rural Hill Road, Una Antioch Pike and Mt. View Road provides pedestrian connections and a place for landscaping and natural stormwater management.



Trails within the proposed open space area provide recreational opportunities for the community.



Open space enhancements can create a sense of place and identity for the community.



Multi-family units should be limited in scale and number and be compatible with single-family character.

Pedestrian enhancements should be included with new development to ensure walkability and pedestrian safety within and surrounding downtown Antioch.

Overall, this plan emphasizes creating a development pattern with connections provided via roads, sidewalks, bikeways and open space. All new development is expected to provide for vehicular and pedestrian connectivity within their development proposals. There is also a need to modify the existing corridors to support new development and connect to services and shopping. The existing corridors of Rural Hill Road, Moss Road, Una Antioch Pike and Mt. View Road are envisioned to become complete streets that safely serve pedestrian, bicycle and vehicular travel. Access management techniques should be incorporated into

future development proposals to minimize the number of curb cuts and coordinate access and circulation among adjacent developments to optimize traffic flow and reduce pedestrian and automobile conflicts.

This plan strives to create a cohesive and sustainable new identity for the Rural Hill-Moss Road community that builds on its history and the asset of its existing semi-rural character. The recommendations within this plan should be followed to ensure the community's vision for preserving their natural beauty and becoming a sustainable community by incorporating a mix of housing types and values, creating recreational opportunities, and providing supportive infrastructure for stormwater management and multiple modes of travel.

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Chapter 1: Background

Introduction and Intent of Plan

The Rural Hill-Moss Road Detailed Design Plan (DDP) is a part of the *Antioch-Priest Lake Community Plan: 2003 Update*, which was adopted by Metro Planning Commission in July, 2003 and has subsequently been amended. The DDP provides planning guidance at a more detailed level than a community plan. The DDP addresses land use, transportation, and community character at the neighborhood level. The DDP is adopted as an amendment to the Antioch-Priest Lake Community Plan.

A DDP illustrates a particular neighborhood's vision for future growth, development, and preservation. DDPs guide, on a parcel-by-parcel basis, the appropriate land use, development character, and urban design intent of future development based upon the neighborhood's goals and sound planning principles. Like community plans, DDPs are developed through a participatory process that involves Planning Department staff working with the residents, property owners, business owners, developers, institutional representatives and elected and appointed officials.

Generally, the next step after the adoption of a DDP is to make the plan regulatory through the adoption of zoning that implements the plan – usually an Urban Design Overlay or Specific Zoning District. Since the adoption of the Antioch-Priest Lake Community Plan, staff has worked with the community to create regulatory tools for four neighborhoods in the area: Rural Hill-Rice-Bell Roads, Hickory Woods, Hamilton Hills and Bakertown. Because the Rural Hill-Moss Road area is currently generally zoned in a low-intensity agricultural zoning district, rezoning for *any* redevelopment will be required. Therefore, rather than rezoning the area, implementation will occur as individual property owners decide to redevelop and request a rezoning, which should be in conformance with this plan.



Example of the natural beauty that existing along Moss Road.

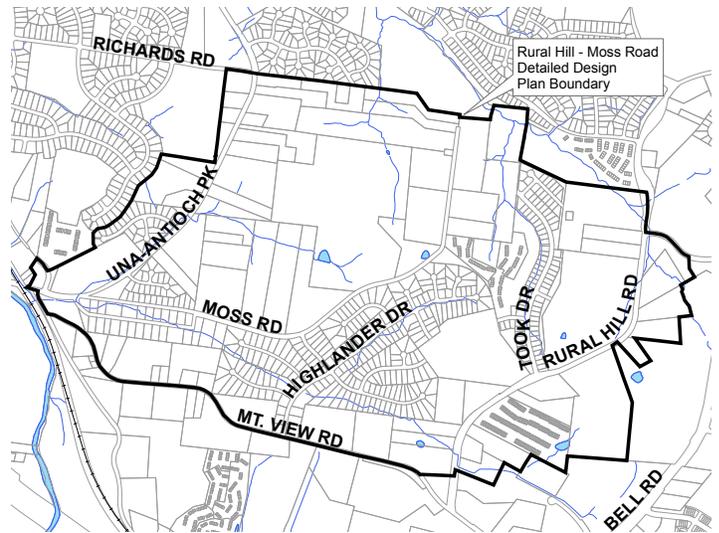


Figure 1: Detailed Design Plan Boundaries

Site Description

For the purposes of this DDP, the Rural Hill-Moss Road neighborhood is bounded by properties fronting Rural Hill Road to the east; Mt. View Road and Hickory Hollow Mall to the south; properties fronting Una Antioch Pike to the west; and the Cambridge Forest and Hunter's Run subdivisions to the north. The study area contains new, suburban development and longstanding rural development. It is directly adjacent to high intensity mixed use development and community facilities such as schools, public open space, and the Southeast Branch Library. The community's close proximity to the Hickory Hollow Mall area, interstate access to Downtown Nashville, and other points of interest in Davidson, Rutherford, and Wilson Counties and its unique semi-rural quality make it a convenient community in which to live.

History of the Rural Hill-Moss Road Community

Creating the Detailed Design Plan (DDP) for the Rural Hill-Moss Road Community requires understanding its historical background and evolution.

Antioch

The Rural Hill-Moss Road neighborhood contains part of the original town of Antioch. In past years, the term “Antioch” has been used incorrectly to describe a large swath of Davidson County stretching from Hermitage to Brentwood. The actual town of Antioch encompassed an area of one or two miles in each direction from the railroad station in the center of the village near Blue Hole Road and Antioch Pike. Beyond that point lay the various communities that surrounded Antioch, including Una, Mt. View, Cane Ridge, Tusculum and Bakertown, which now all fall under the present-day reference of “Antioch.” Exactly when the community took the name of Antioch is not known, but the name came from Antioch First Baptist Church, established in 1810.

Background

The village of Antioch was molded by several influences. The Mill Creek floodplain and waters were attractive for early farms. Most of the lands deeded in the early settlement of the area are described as being located, “on the waters of Mill Creek.” Charles Hays is considered to have been the first major settler in the area, and initially owned most of the land that later comprised the town of Antioch. He was also the founder of the Baptist Church at Antioch (later known as the Antioch First Baptist Church). Unfortunately, however, the flood waters of Mill Creek would later damage churches and buildings. Very few of the early structures remain.

Construction of Mill Creek Valley Pike (now known as Antioch Pike) was completed in 1846. The first passenger train from Nashville ran to a point ten miles south of town to Antioch in 1851. The construction of the railroad tracks later made it convenient for people to travel to jobs in Nashville. The first train station stood southwest of the present Una Antioch Pike between the old Baptist church building and the railroad – very likely the spot where Hickory Hollow Parkway runs today.

Following a common pattern of settlement, the village of Antioch expanded along Mill Creek. In



Antioch First Baptist Church.

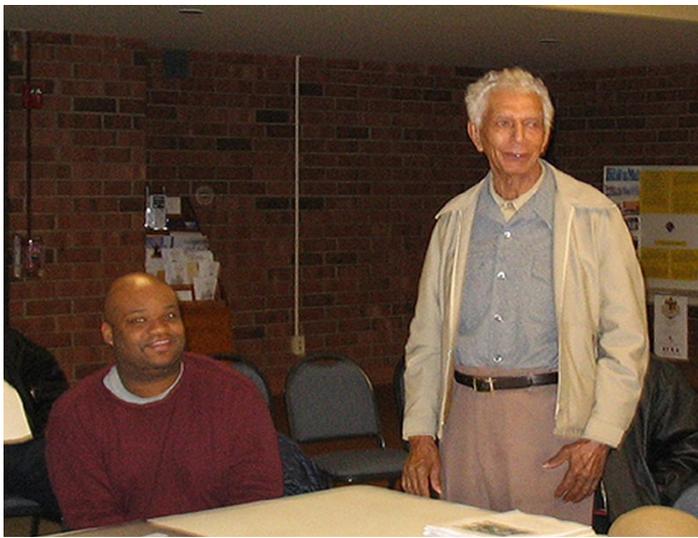


St. John Missionary Baptist Church.

the early 1900s, Davidson County began making road improvements to create what are now known as Nolensville Road and Haywood Lane, to connect neighboring communities. The highway department made improvements to what was Cane Ridge Road in 1916. The northern end of Cane Ridge Road was built and joined Mt. View Road. Mt. View Road wound around the former Antioch Baptist Church and continued on across the railroad to become Antioch Pike. The small stretch of land that is now Hickory Hollow Parkway (between Una Antioch Pike to the end of Mt. View Road) did not exist at that time. Sometime later, this land was bought by the Baptist Church and transferred to the highway department to construct the road.



Kidd Family Cemetery on Moss Road.



Charles M. Blackman, Sr. (right) providing a brief history of the area at a community meeting.

The area that is today Moss Road was originally the land of Charles Hayes and Joe Morgan. It was an unpaved dirt road ending near the Kidd and Blackman homes until the late 1940s and early 1950s when the road was improved and renamed Moss Road in honor of magistrate Claude Orville Moss, Sr.

Just off Moss Road is St. John Missionary Baptist Church, whose pastor Rev. Joseph Mullins was instrumental in encouraging African-American citizens to own land and build homes. The Shields Antioch Lands (encompassing Moss Road and Ottenville Road) became one of the first African-American subdivisions in Davidson County. In April 1870, John H. Oney purchased 113 acres of Mill Creek land with Eugene

Ottenville. Mr. Oney, also the local postmaster, named the roads as they were settled near the village, hence the name Ottenville Road. One lot in the original subdivision was set aside for a church and the St. John Missionary Baptist Church building was constructed on that lot through the work of Rev. Mullins and other church members. The congregation is still located Ottenville Road today, and several of the families who originally settled the area still have descendants living in the area.

Due to its close proximity to Nashville, Antioch did not develop an industrial base. The railroad and train depot were an important part of the town. The popularity of the automobile caused the demise of the rail service, and after World War II the train no longer stopped in Antioch and the depot was razed. The interstate highway of the fifties brought expansion to the southeastern corner of Davidson County; however it bypassed Antioch and placed exit ramps on the north and south sides of the town rather than engaging the town center.

Hickory Hollow Mall was developed in the 1970s, bringing an influx of surrounding commercial development, large apartment complexes, and disjointed subdivisions. The gathering of commercial establishments provided services for residents, yet displaced many farmlands and was developed in a way that did not complement or integrate the existing community. In many ways, however, the disjointed development pattern that has developed at the edges of the community has left much of the natural beauty of the area untouched, largely because the undeveloped portion of the Rural Hill-Moss Road community has natural features that have made development more difficult.

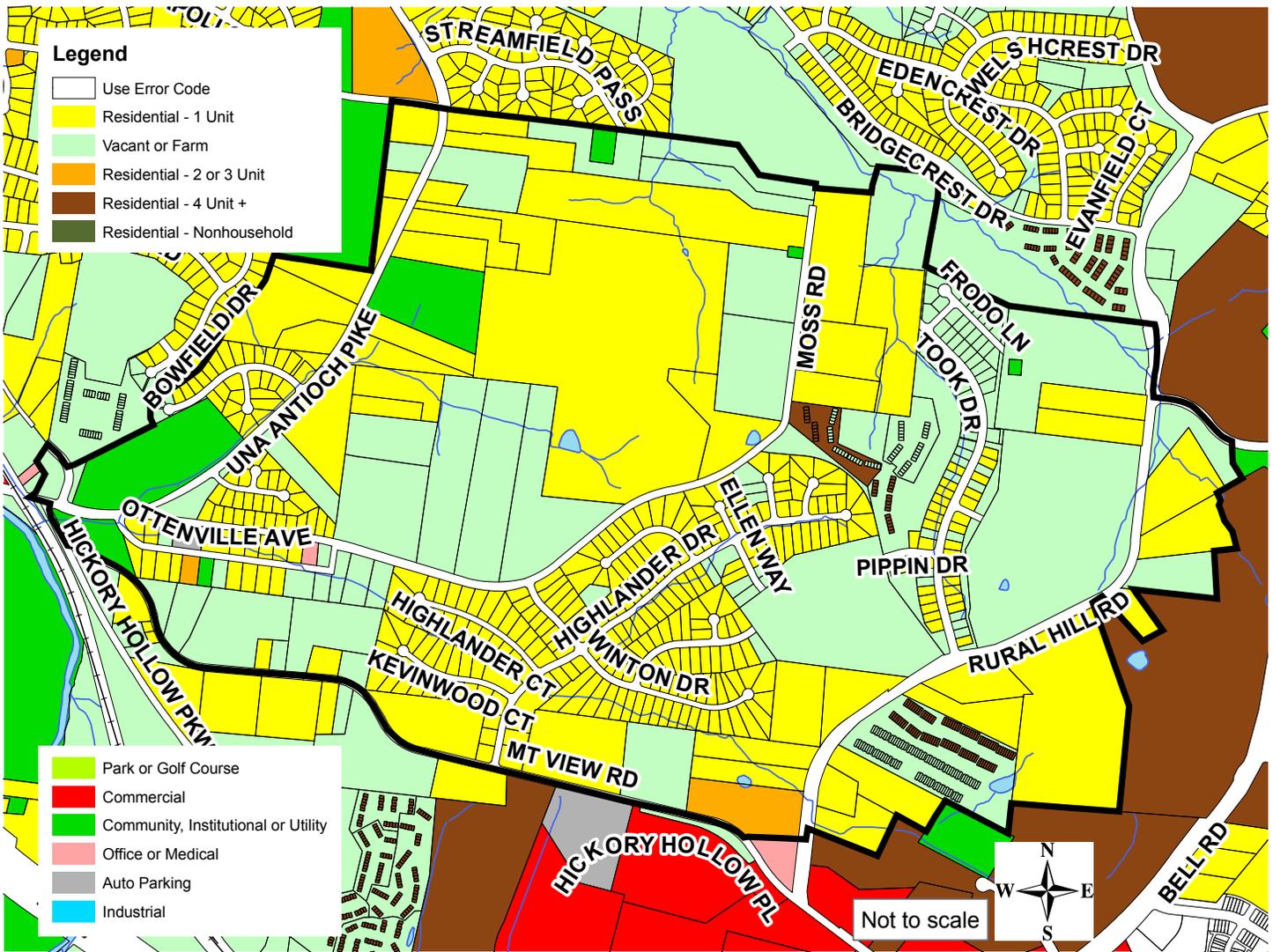


Figure 2: Existing Land Use Map

RESIDENTIAL USES	ACRES	% OF TOTAL PARCEL ACRES	TOTAL DWELLING UNITS	% OF TOTAL UNITS	UNITS PER ACRE
Single Family Detached Subtotal	327.24	54.43	383	74.51	1.17
Conventional Rural/Large-lot (3+ ac/du)	200.29	33.31	27	5.25	0.13
Conventional Urban/Suburban (< 3 ac/du)	126.95	21.11	356	69.26	2.80
Townhomes and Multifamily Subtotal	12.28	2.04	127	24.71	10.34
Conventional Duplexes, Triplexes & Zero Lot-line Units	5.99	1.00	3	0.58	0.50
Conventional 4+ Unit Structures	6.29	1.05	124	24.12	19.71
Household Residential on Nonresidentially Coded Parcels Subtotal	0.36	0.06	4	0.78	11.11
RESIDENTIAL GRAND TOTAL	339.88	56.53	514	100.00	1.51
NONRESIDENTIAL USES	ACRES	% OF TOTAL PARCEL ACRES	TOTAL FLOORSPACE (SQ. FT.)	% OF SUBTOTAL	FLOOR/ AREA RATIO
Office, Commercial & Industrial Subtotal	0.36	0.06	3,876	100.00	0.04
Office, Non-medical	0.36	0.06	3,876	100.00	0.04
Auto Parking (principal use) Subtotal	0.73	0.12	n/a	100.00	n/a
Civic & Public Benefit Uses Subtotal	23.10	3.84	n/a	100.00	n/a
Community Facilities	23.10	3.84	n/a	100.00	n/a
NONRESIDENTIAL USES TOTAL	24.19	4.02	3,876	100.00	n/a
VACANT AND FARMLAND	ACRES	% OF TOTAL PARCEL ACRES	TOTAL FLOORSPACE (SQ. FT.)	% OF SUBTOTAL	FLOOR/ AREA RATIO
Vacant/Farm Residential Codes	236.45	39.33	n/a	n/a	n/a
VACANT LAND TOTAL	236.45	39.33	n/a	n/a	n/a
Miscoded, uncoded or miscellaneous parcels	0	0.00	n/a	n/a	n/a
TOTAL PARCEL ACRES	601				

Table 1: Existing Land Use Table

Inventory and Analysis of Existing Land Use

As noted above, for planning purposes, the community referred to as Rural Hill-Moss Road includes the area between Una-Antioch Pike (including both sides of Una-Antioch Pike), the Cambridge Forest subdivision, Rural Hill Road (including both sides of Rural Hill Road), and Mt. View Road.

The Rural Hill-Moss Road Community is partially urbanized with residential development, however, much of it remains rural despite its location between the intense development of the Hickory Hollow Mall area to the south, the Bell Road corridor to the east, and the Murfreesboro Pike corridor to the north. The Rural Hill-Moss Road Community has long been planned to develop as a suburban residential community, although it has only begun to see such development over the past 15-20 years.

Altogether, the Rural Hill-Moss Road planning area covers about two tenths of a percent of Nashville/Davidson County. It contains approximately 636 acres comprised of the following:

- **Parceled Land:** About 94.5 percent of the area consists of privately owned parcels of land, including the common area portion of condominium complexes.
- **Right-of-Way and Water:** An estimated 5.5 percent is public street right-of-way or small bodies of water.

The community's existing generalized land use is shown on Figure 2 and in Table 1 and described below.

Residential Uses

- **Total Residential:** About 54 percent of the area (parceled acres) is currently being used for residential, totaling 327 acres. This residential development is predominantly single-family, reflecting the semi-rural character of much of the community. Nevertheless, 12 percent of the housing units in the community are forms of housing other than single-family. These two-through multi-family units are most commonly found in the eastern portion of the community where the neighborhoods are closer to the large

multi-family developments along nearby Bell Road.

- **Single-Family:** Of the total residential, single-family housing accounts for 74.5 percent and averages 1.17 units per acre.
- **Multi-Family:** Townhomes, duplexes and multi-family account for 24.7 percent of the total housing units and average 19.7 units per acre.

These figures are subject to change because some residential areas were still under construction at the time this information was compiled (April 2009).

Non-Residential Uses

There are very few non-residential uses in the Rural Hill-Moss Road Community, and most of them are houses of worship. The broader "non-residential use" category includes offices, civic and public benefit uses, and parking. Together these uses comprise around 4.0 percent of the area.

- **Civic and Public Benefit Uses:** These uses comprise the largest percentage, 3.8 percent, of the non-residential land uses in the Rural Hill-Moss Road Community. They are located mainly along Una Antioch Pike.
- **Office:** An office use between Ottenville Avenue and Moss Road accounts for approximately 0.06 percent of the Rural Hill-Moss Road Community. This building also contains four residential units.
- **Parking:** Surface parking as a principal use (also found between Ottenville Avenue and Moss Road) accounts for approximately 0.12 percent of the area.

Vacant Land

Vacant land is scattered throughout the Rural Hill-Moss Road Community. All of it is classified as either residential or agricultural. There are approximately 236.5 acres of vacant land in the area, representing about 39 percent of the total parceled acres.

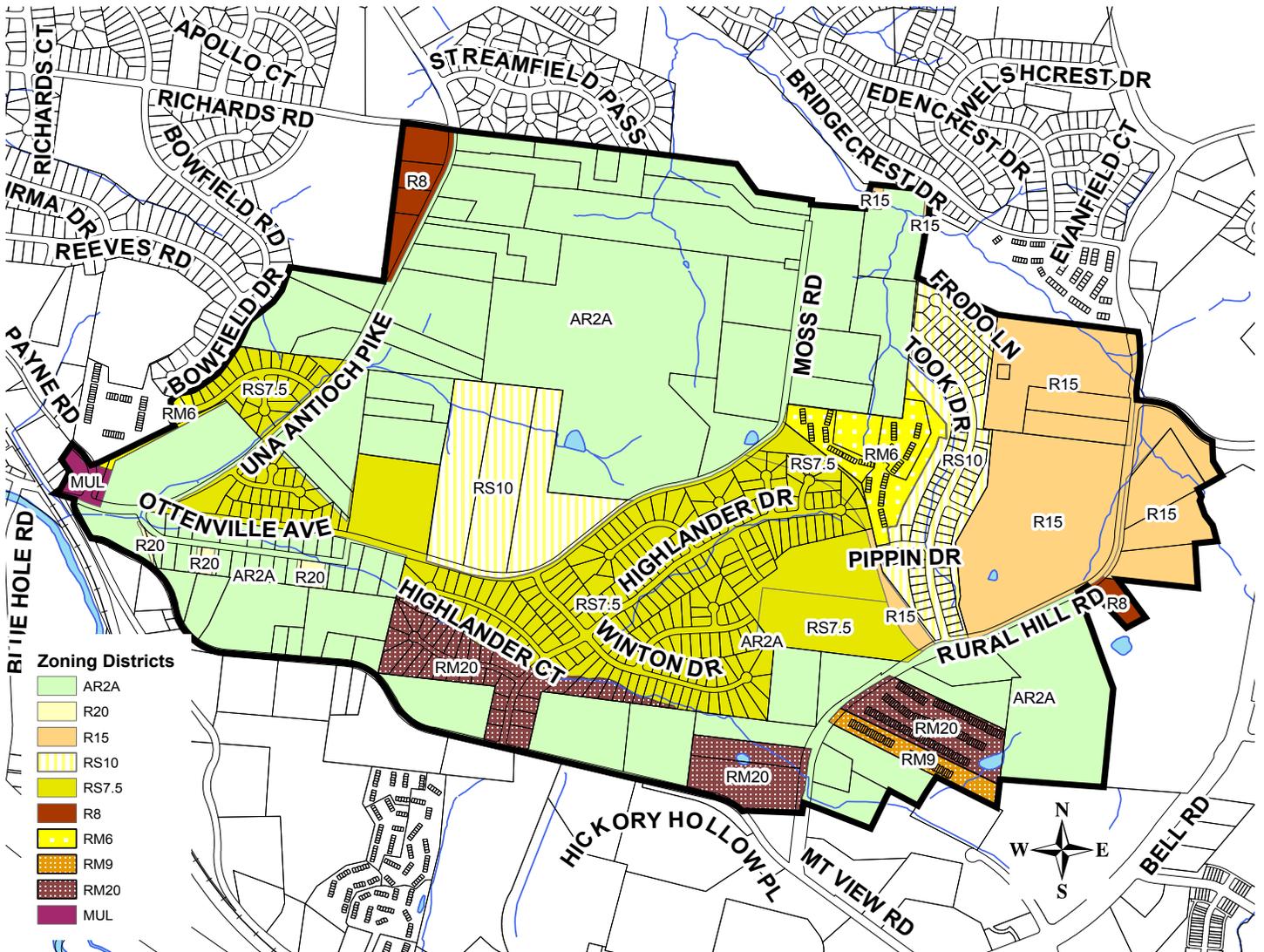


Figure 3: Existing Base Zoning Map

Category	Acres	% of Total
Agricultural	316.67	49.80
Res 1- and 2-Family Districts	263.12	41.38
Res Multi-Family Districts	53.84	8.47
Total Res Districts	316.96	49.85
Mixed Use Districts	2.21	0.35
Community		
Total	635.84	100.00

Table 2: Existing Base Zoning

Inventory and Analysis of Existing Zoning

Both the current base zoning districts and overlay district zoning are addressed in this section. Each type of zoning is also shown graphically and is summarized for the Rural Hill-Moss Road Community in Figures 3 and 4 and Tables 2 and 3.

An understanding of the existing zoning is crucial, because the DDP is *primarily implemented through zone changes*. The guidance of the DDP does not apply to properties that develop within their existing zoning (i.e., properties that seek building permits within their current zoning). Rather, when a property owner requests a zone change, subdivision request, variance, or other change in their development opportunities, *then* the proposal is judged for its conformance with the DDP. If the current zoning is very permissive toward development, there will be little need or incentive to rezone and follow the DDP. Therefore, during creation of the DDP, the Planning staff and the community discussed tradeoffs in development rights, to provide incentives to property owners to seek rezoning and be subject to the guidance of the DDP.

Base Zoning Districts

The zoning pattern of the Rural Hill-Moss Road Community reflects its partially developed and predominantly residential condition. The study area contains one agricultural/residential, eight residential, and one mixed use zoning district classifications. The agricultural zoning district applies to 317 acres in the study area, accounting for slightly less than 50 percent of its zoned land. The residential zoning districts apply to 317 acres in the study area, again slightly less than 50 percent of its zoned land. Of the residential districts, 263 acres are residential single- and two-family districts while 54 acres permit single-, two-, and multi-family housing. Meanwhile, 2.2 acres or less than half a percent of the zoned land in the study area is zoned for mixed use.

The residentially zoned properties are zoned with a variety of zoning districts that result in a diverse mixture of housing that is appropriate to the community's landform and its convenient and accessible location, and fulfill the expected housing needs of people at all stages of life. Currently, most of the residentially zoned land is developed at the densities envisioned under the

DDP except for most of the R15 land (this land is part of a larger circa-1979 Planned Unit Development that is mostly developed) and small percentages of the RS7.5, RS10, and RM20 zoned land. This land remains vacant or very sparsely developed, along with the agriculturally zoned land. The minimum lot sizes for single- and two-family housing permitted by these residential districts ranges from 7,500 to 20,000 square feet. Multi-family housing may be developed at densities between 6 and 20 units per acre, yielding a range of housing options from townhomes to low-rise stacked flats. The zoning districts are generally appropriately located within the study area, with most of the higher intensity housing permitted near the mall area and busier streets on the edge of the community.

The residential zoning districts currently present in the community, however, lack the design standards and incentives needed to achieve the development pattern envisioned for the Rural Hill-Moss Road Community. This is especially true of the multi-family districts and those lower-density residential districts (ex: R15) that have been combined with older Planned Unit Development Overlays (see next section, "Overlay Zoning Districts") that have dated or inadequate design standards. This DDP includes Community Character Policies in Chapter 3 that call for the use of design-based zoning to achieve the envisioned future development pattern. In addition, this DDP provides incentives through a Transfer of Development Rights (TDR) program described in Chapter 4 to achieve the desired mixture of diverse housing options and a comprehensive open space network.

Mixed-Use Limited or MUL is a zoning district encompassing a small area containing one parcel and a portion of another totaling only 2.2 acres at the intersection of Payne Road and Una Antioch Pike. The MUL zoning district allows a mixture of uses in one building. This MUL zoned land is at the end of a larger commercially zoned area extending to the west along Antioch Pike outside the study area. This is the commercial portion of the original downtown Antioch referred to in the Historic section. Currently, this MUL zoning is undeveloped and although it has a Planned Unit Development Overlay (PUD), the PUD is inadequate to meet the policy objectives of this DDP

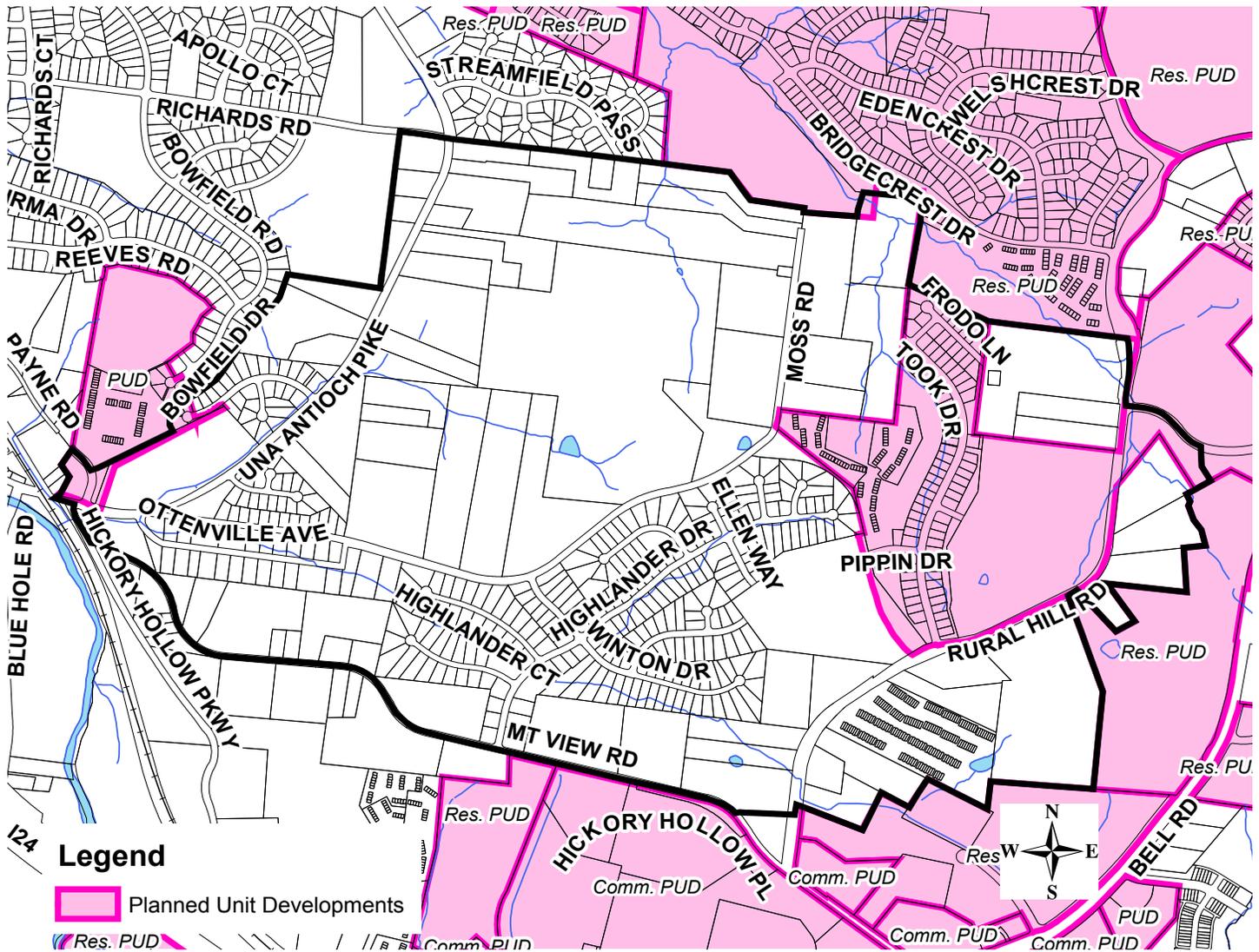


Figure 4: Existing Overlay Zoning Map

Category	Acres	% of Total
Res PUD	80.07	12.59
Mixed Res/ Nonres PUD	3.7	0.58
No Overlay	552.31	86.83
Total	636.08	100.00

Table 3: Existing Overlay Zoning

for the type of mixed-use development envisioned for the T3 Suburban Neighborhood Center planned here (see Chapter 3).

Overlay Zoning Districts

Overlay districts are zoning districts that add additional regulatory protections, but do not restrict the use of the land allowed under the base zoning. There is one type of overlay district that affects the study area, the Planned Unit Development Overlay District, or PUD. The acreage in each category of PUD is outlined in Table 3. The location of the PUDs is shown on Figure 4.

The PUD is an older form of design-based zoning that has been superseded by the Urban Design Overlay (UDO) and the Specific Plan (SP) base zoning district. Although the option still exists in the zoning code, new PUDs are rarely established and are not anticipated or recommended for the study area in the future. The use of the more flexible and updated UDO and SP tools is preferred. This DDP provides incentives in Chapters 3 and 4 for the replacement of the existing PUDs in the study area with these preferred zoning tools.

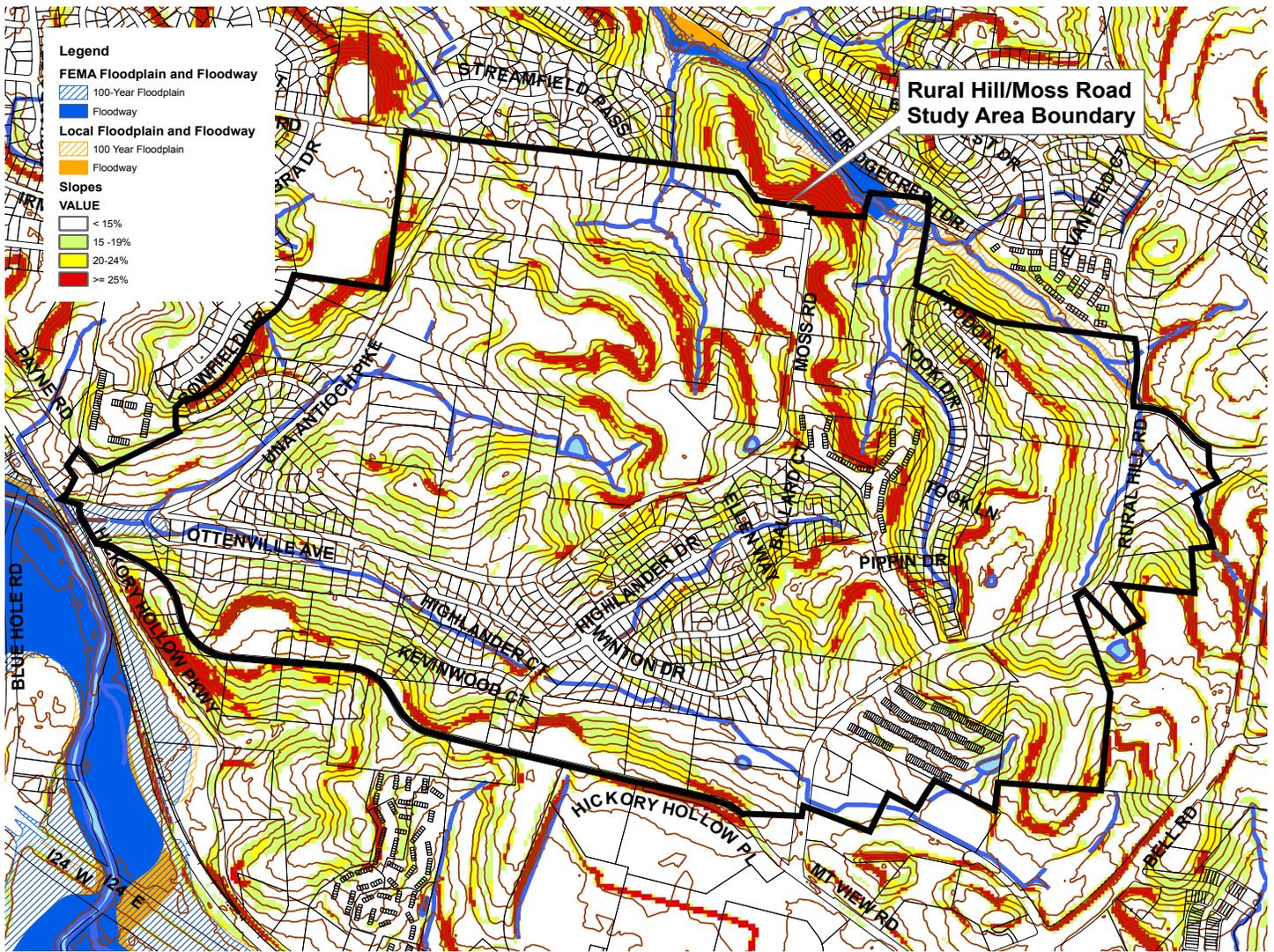


Figure 5: Natural Features Map



Stream channel adjacent to Moss Road near the intersection with Una Antioch Pike.



Rolling pasture land adjacent to Moss Road.

Inventory and Analysis of Existing Systems

Natural Features

The Rural Hill-Moss Road study area contains sensitive environmental features, primarily steep slopes, floodplains, and streams. These are shown on Figure 5. There are approximately 115 acres of steep slopes in the study area, or 18 percent of its land. The steep slopes are primarily undeveloped and are part of the reason the area remains semi-rural despite its accessible and convenient location. Steep slopes - any slope greater than 20 percent - are located throughout the study area, but are primarily concentrated in its central area. Steep slopes present a variety of development related issues relating to road and building construction and stormwater management. For slopes exceeding 20 percent, the best practice is to avoid altering the landform or the vegetation that acts to stabilize it and aid in its drainage function.

In Davidson County, steep slopes are protected from potential adverse effects of development through both policies and regulations. The Community Character Policies of this DDP provide protection for slopes greater than 20 percent, and supporting regulatory protection is currently provided for much of the residentially zoned land for those slopes of 25 percent and greater. Future zoning changes under the policies of this plan have the potential to provide even greater protection, and this DDP incorporates the majority of the steep slopes into the planned, comprehensive open space network, employing Transfer of Development Rights (TDR) as a tool for incentivizing its protection.

There is a small area of floodplain in the study area that is located around the intersections of Una Antioch Pike, Hickory Hollow Parkway, Ottenville Avenue, and Moss Road. There is also an even smaller area of floodplain at the confluence of three unnamed streams in the undeveloped northeast portion of the study area. Together, these areas contain approximately five acres or 0.75 percent of the land in the study area. This floodplain area is part of the larger Mill Creek floodplain that continues to the south and west and has sometimes hampered access to the study area through the occasional submergence of adjacent roads such as Antioch Pike and Blue Hole Road. Although Mt. View Road is part of this complex intersection, steep slopes separate it from the floodplain area. These slopes and floodplains present challenges for realigning the intersection to function properly to serve existing and envisioned development and traffic.

Floodplains are also protected through both policies and regulations. Floodplains consist of two areas:

- the *floodway*, which means the channel of a stream that has current, direction and velocity to it during a flood, and in which debris may be carried; and, the larger *floodplain*, which means land area, including the floodway of any river, stream or watercourse, susceptible to being inundated by water as identified by the 100-year flood

The term “100-year” flood refers to there being a one in one hundred chance that the area shown within the 100-year flood plain will flood to the extent shown in any given year. Both the floodway and 100-year flood plain are shown on Figure 5. Development within the floodway is strictly prohibited by regulation. Development within the floodplain portion that is outside of the floodway is controlled and restricted through regulation.

The locations of the floodplain and some of the steep slopes in this DDP area present challenges for balancing competing goals and objectives of the DDP and will require creative and innovative approaches to achieve the needed balance. As discussed further in Chapter 4, the major realignment of the Una Antioch-Hickory Hollow-Mt. View-Moss-Ottenville intersections is critical to the future growth and preservation of this study area and adjacent areas. It is important to the future development of the area to have safe and efficient mobility, but it is also important to protect its environmentally sensitive features. The land occupied by this street network is environmentally sensitive and will need to be disturbed to accommodate the needed solution to the tangled complex of interrelated intersections. Careful management of any disturbance will be required to minimize the environmental damage that may be caused by the intersection realignments.

The many streams in the study area are also sensitive and likewise require protection from the potential adverse impacts of development. There is, at a minimum, a required regulatory buffer of 30 feet on either side of all “blue-line” streams (those which convey water throughout most of the year), which is to remain undisturbed. Streams that have not been previously disturbed by development, and to the extent possible, those that have been, are incorporated into the planned, comprehensive open space network for the study area. Those undertaking a development project are advised to

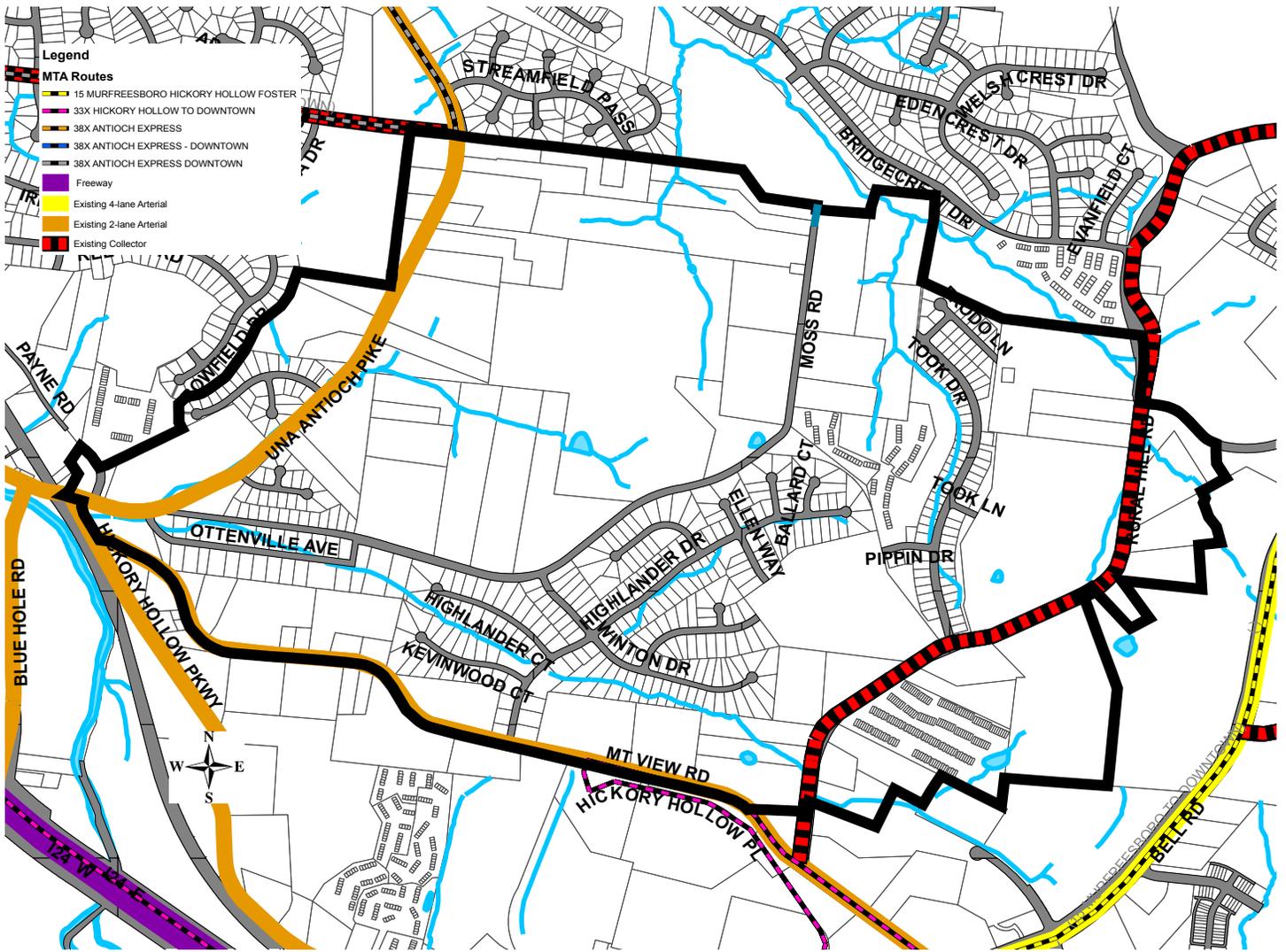


Figure 6: Street Network and Transit Map



Intersection of Ottenville Avenue, Moss Road and Una Antioch Pike.



Moss Road is a narrow road without sidewalks.

consult with Metro Stormwater for requirements based on the specifics of the project location.

Street Network

The Rural Hill-Moss Road study area has a street system that is partly developed and insufficient for either the existing or envisioned development pattern. The street network is shown on Figure 6. Most of the streets are poorly connected, failing to provide multiple routes by which to move throughout the community, and require improvements for the safety of all modes of transportation.

There are two arterial streets (the most major level of street that is not a freeway) on the edges of the study area. These are Una Antioch Pike, which is on the west side of the study area, and Mt. View Road, which is on the south side of the study area. A very small portion of Hickory Hollow Parkway, also an arterial street, touches the southwestern edge of the study area and forms part of the problematic Una Antioch-Hickory Hollow-Mt. View-Moss-Ottenville intersection complex. Rural Hill Road, which is on the eastern side of the study area, is classified as a collector, the level of street that falls between arterial streets and local streets. All of these are two-lane roads with occasional left and right turn lanes provided at the entrances to individual developments. Took Drive, also with two travel lanes, but without striping or turn lanes, forms part of a planned east-west collector that is intended to connect to Una Antioch Pike. Of these important streets, only Took Drive and a small section of Rural Hill Road have sidewalks.

The Una Antioch-Hickory Hollow-Mt. View-Moss-Ottenville intersection complex is among the chief impediments to the successful future development and preservation of the Rural Hill-Moss Road Community and adjacent areas whose traffic must pass through the community. The operational difficulties of this series of intersections are compounded by its close proximity to an active at-grade railroad crossing at the west boundary of the study area and the presence of steep slopes and floodplain around it. The ability to move people safely and efficiently to and from the central portion of the study area, while providing for alternate forms of transportation, is dependent on significantly reconfiguring the streets, properties, and landforms in this area. Due to anticipated high costs and environmental limitations, it will also be important to develop a street network *within* the Rural Hill-Moss

Road Community that can relieve some of the burden currently placed on this as-yet unsignalized intersection complex. Chapters 3 and 4 of this DDP address potential land use and transportation solutions.

Transit System

The Rural Hill-Moss Road Community is served by mass transit bus routes at the edges of the study area, shown on Figure 6. Metropolitan Transportation Authority (MTA) routes are located along Mt. View Road, Bell Road, Richards Road, and Una Antioch Pike with multiple bus stops along these corridors. Bus routes change periodically and the reader is recommended to visit the MTA web site www.nashvillemta.org for the most up to date information.

Bicycle and Pedestrian Systems

The Rural Hill-Moss Road Community has a very limited pedestrian network (see Figure 7). There are sidewalks along one side of the streets in the Hickory Highland Place subdivision (containing Highlander Drive, etc.), and on both sides of the streets in the Berkeley Hall subdivision (containing Took Drive, etc.). There are scattered sidewalks found elsewhere in the study area in the few places where there has been development that has taken place since the mid-1990s.

There are currently no bicycle facilities in the community. Recommendations for enhanced bicycle and pedestrian facilities are provided in Chapter 4.



Took Drive is constructed to collector street standards, with 37 feet between curbs.

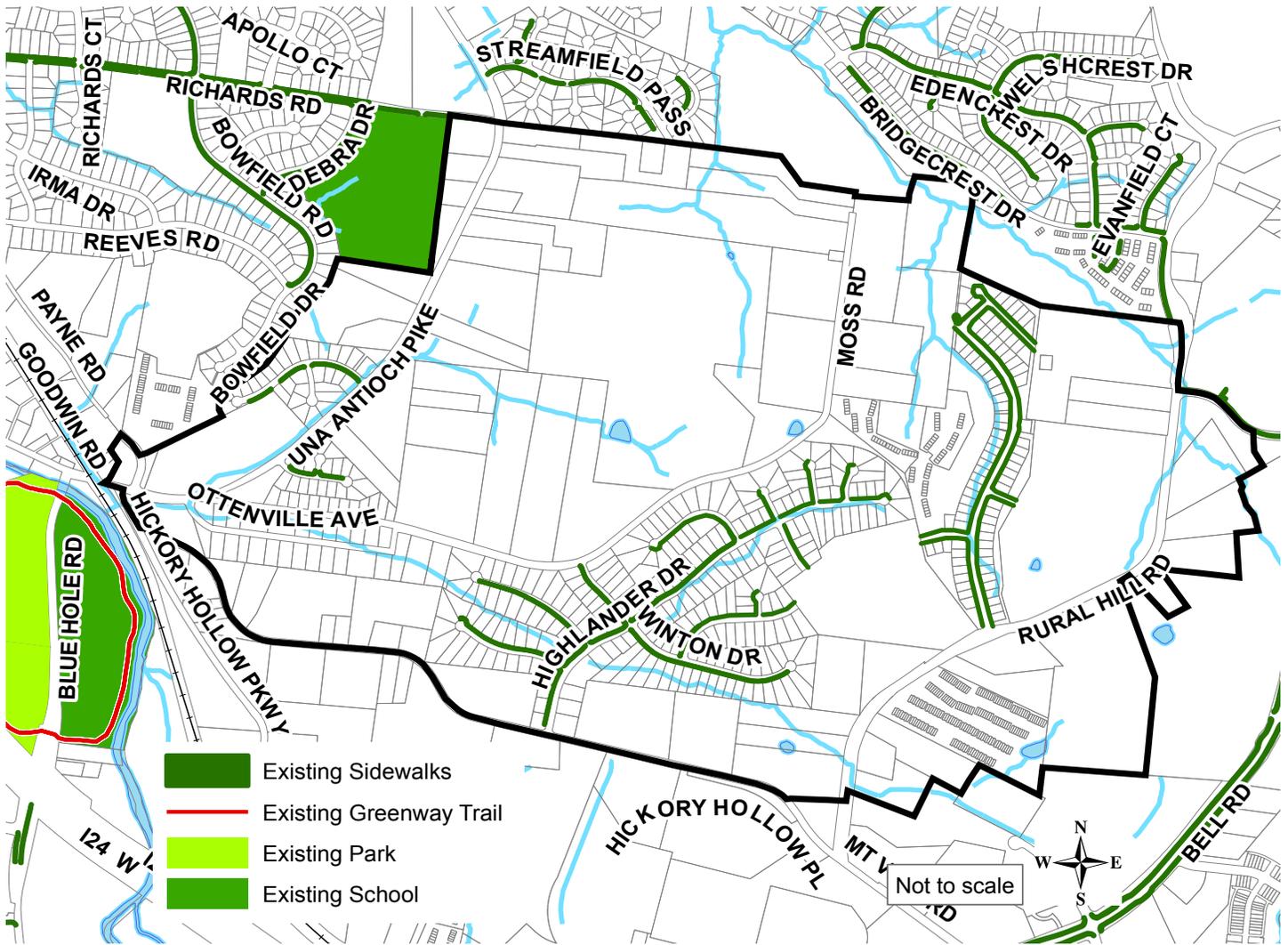


Figure 7: Bike, Pedestrian, and Open Space Map



The lack of sidewalks of Rural Hill Road creates hazards for both pedestrians and drivers.

How to Use This Detailed Design Plan

After researching the history and existing built environment within the Rural Hill-Moss Road Community, community stakeholders envisioned the possibilities for future growth and development at a Visioning community meeting. A Concept Plan was created from the initial visioning session, which balanced community stakeholders' visions for residential and commercial development while preserving the unique character of the Rural Hill-Moss Road Community, including significant open space preservation. The stages of translating this conceptual plan into Community Character Policies to guide the form of development are described below and shown graphically in Figure 8, guiding the reader in how to use and understand this plan.

The purpose of this DDP is to outline a comprehensive and integrated planning strategy to achieve the community's vision for future growth, development, and preservation. A community requires the interaction of many elements including land use, the built form and character, public services and facilities, infrastructure, transportation options, and presence of civic activity. Great places are established where those elements are balanced and supportive of each other. In other words, the land use pattern is supported by the scale, character and massing of the buildings; the placement and orientation of buildings in relation to streets, sidewalks and open spaces creates sense of place; the transportation systems support and enhance the land uses; and public services and facilities, as well as civic activities and open spaces meet community needs.

DDPs are used in the same way as the Community Plan. The community, private developers, the Planning Department, the Planning Commission, and Metro Council use the plan as a starting point to discuss public and private investment in the area, including proposed zone changes, subdivisions and public investment (including roads). Once adopted, the DDP serves as the primary guide for the neighborhood's development. In the section below, any topic that is **bolded** is a section of the DDP that the reader can refer to for more information.

In creating the DDP, initial conversations with the community establish the direction of the plan,

described through a **Vision Statement, Concept Plan, and Development and Preservation Goals and Objectives** for the Rural Hill-Moss Road Community. To create a plan that considers all the elements of the neighborhood, the goals and objectives address not only development of different types of residential, mixed use, and civic / open space development, but also elements that make up the framework for this development – circulation for vehicles, transit, pedestrians, and bicyclists as well as landscaping and signs.

The final products of the DDP are the **Community Character Policies and Plan**, which must be used together. The Community Character Policy Plan summarizes which land uses and character are allowed in which parts of the community. The Community Character Policies describe the appropriate building type and form in each policy area through analysis of actual and proposed buildings and lot patterns. Taken together, these tools provide detailed guidance on zoning and urban design (the relation of the building to the street, other buildings, and open space) to achieve the vision of the community.

Once officially adopted by Metro Planning Commission, rezoning requests within the DDP should be accompanied by a site plan – either by using a plan-based zoning district, preferably the Specific Plan (SP) District – to ensure that the community vision is met. Rezoning requests will be measured by Metro Planning staff, Planning Commission and Council on their conformance with the Community Character Policies.

The policies, goals and objectives in this plan will be the basis for Metro Planning Commission staff recommendations relative to rezoning requests, subdivision requests, variances and special exceptions. The policies, goals and objectives are intended to implement the guiding principles of this plan; they provide specific steps to create development that transforms the guiding principles into practices. All development is also encouraged, however, to offer additional or alternative innovative ways to achieve the guiding principles.

Community Character Manual, Community Character Policies and Special Policies

The Community Character Manual (CCM) is a functional plan component of Nashville's *Concept 2010: A General Plan for Nashville and Davidson County*. Adopted by the Planning Commission in 2008, the CCM has three main functions:

- to explain and institute the Community Character Policies that will be applied in each Community Plan;
- to provide direction for the creation of implementation tools such as zoning; and
- to help shape the form and character of open space, neighborhoods, centers, corridors and districts within communities.

The CCM is the dictionary of Community Character Policies that are applied to land in the Rural Hill-Moss Road Community. All Community Character Policies for the Rural Hill-Moss Road Community can be found in the T3 Suburban Transect Category. The following is step-by-step instruction of how to read and use the Rural Hill-Moss Road DDP.

First find the Community Character Policy and read about it in the CCM.

Users of the Rural Hill-Moss Road DDP should first determine what Community Character Policy is applied to the property that interests them. Look at the Community Character Policy Map in this DDP or online at www.nashville.gov/mpc and find the Community Character Policy Area (see "Interpreting Maps" below). The user can find the Community Character Policy in the CCM, which provides guidance on a variety of design principles, appropriate zoning districts, and building types.

Next, read the Rural Hill-Moss Road Detailed Design Plan to determine if there are any Special Policies for the area.

Within some Community Character Policy areas there are unique features that were identified during the detailed design planning process where guidance is needed that is in addition to the guidance provided in CCM. This additional guidance is called *special policy*. The special policies may provide additional specificity to the broad language in CCM or they may describe conditions that deviate slightly from the CCM policy. In all cases, users should first refer to the CCM to understand the

policy's intent, its application, general characteristics, and design principles, but then look to the Rural Hill-Moss Road DDP and the special policies to understand any unique conditions that may exist. The Special Policies are not identified as a separate graphic on the map, but are found within the text of a Community Character Policy Area (see Chapter 3). Thus, when a user looks up a Community Character Policy Area on a map, its number will correspond with the special policies in the text.

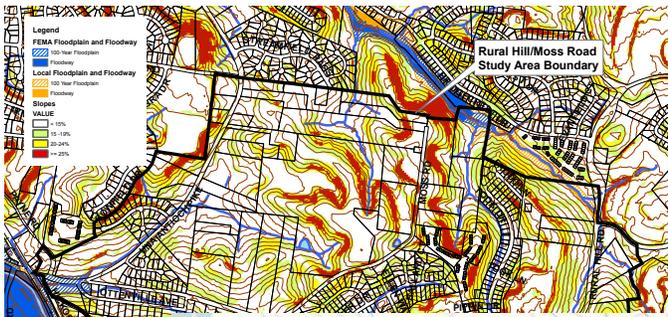
Finally, read the "General Principles" in the CCM for additional guidance on specific development and preservation topics.

In addition to the Community Character Policy and Special Policies unique to the area, users are encouraged to review the "General Principles" at the beginning of the CCM, where topics such as historically significant properties, transitions between Community Character Policies and stormwater are addressed.

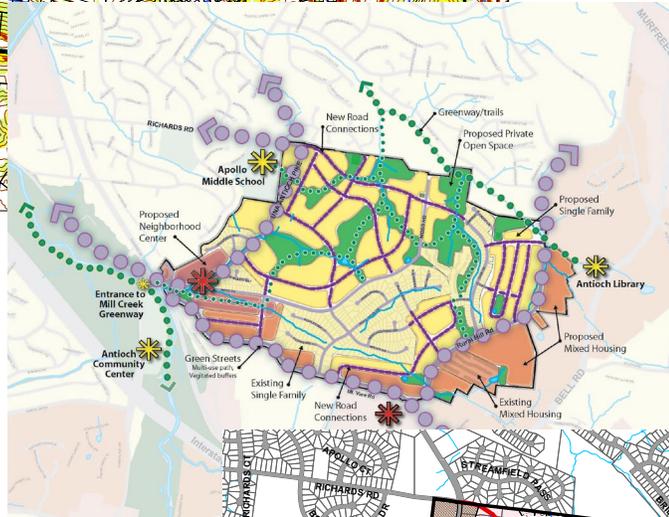
Essentially, the CCM provides general principles and is where the Community Character Policies contained in the Rural Hill-Moss Road DDP are defined, but with generalities for flexibility in the use of the policies. The Rural Hill-Moss Road DDP is where the Community Character Policies are applied and where generalities are made more specific through the use of Special Policies. The Community Character Policies and Special Policies should be used in the creation of development proposals and neighborhood, center, corridor and community planning efforts.

The CCM provides information that enables residents, business owners, property owners, institutional representatives, developers, and elected officials to take a proactive role in the community development by establishing standards against which development proposals can be judged.

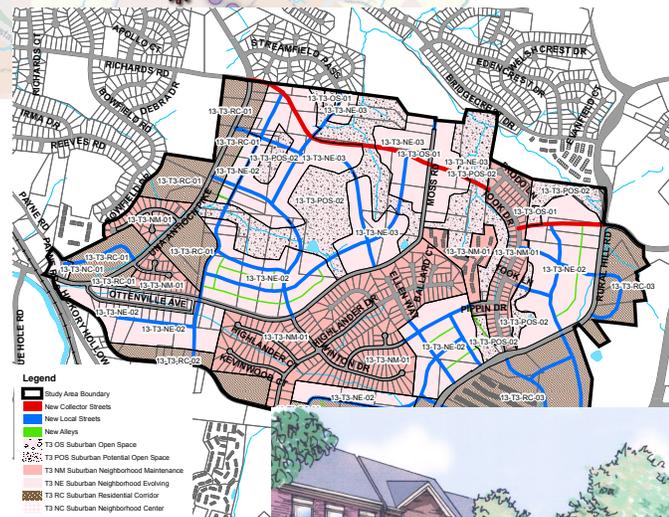
From existing conditions and community vision...



...to the broad direction set by the Concept Plan...



...to the specific direction set by the Community Character Policy Plan...



...and illustrated by future character illustrations.



Figure 8: Building the Plan

Interpreting Maps in the Rural Hill-Moss Road Detailed Design Plan

The Rural Hill-Moss Road DDP includes several maps. The Concept Plan illustrates many of the ideas that the community shared during the visioning session about its future growth, development and preservation. Additionally, the Rural Hill-Moss Road DDP includes four primary maps that guide future growth, development and preservation – the Community Character Policy map, and the Vehicular Transportation Plan map, the Pedestrian and Bicycle Network Plan map, and the Open Space and Stormwater Management Plan map.

Community Character Policy Map

The Concept Plan is translated into a *Community Character Policy Map*, found in Chapter 2, where the vision for the community is enacted through Community Character Policies. When using the Rural Hill-Moss Road Character Map to determine the guidance for a particular property, there are several items on the map to be aware of – the Community Character Policies and their symbols, and the Community Character Policy Areas including their numbers and Special Policies. The following describes how to read and understand the Community Character Policy Map.

Community Character Policy Symbolology

Community Character Policies are applied to all land in the Rural Hill-Moss Road Community. Each Community Character Policy has a unique symbol that is a combination of a color and a pattern. The color indicates the Transect Category and the pattern indicates the Community Character Policy. For example, all T3 Suburban Transect areas are denoted by a base color that is a shade of pink. The various Community Character Policies have different patterns layered on the base color to create its unique symbol. See Figure 9.

Community Character Policy Areas Symbolology and Numbering

Because different areas can have the same Community Character Policy (for example, there may be multiple residential areas), but still have different character, the community character policies are further identified by *areas* and are called Community Character Policy (CCP) Areas. The CCP Areas have policy boundaries that are identified by the development patterns (lot sizes, spacing of homes, in some instances density and intensity, and

zoning), and unique features in the area. To further identify these areas, a *CCP Area Number* is assigned to them and is mapped on the Rural Hill-Moss Road Community Character Policy Plan Map as shown in Figure 10.

The CCP Area Number in Figure 10 is displayed as: **13-T3-NM-01**. The **13** identifies the Subarea or community planning area. The Antioch – Priest Lake Community is Subarea 13. The **T3** identifies the Transect Category. In this case the Transect Category is T3 Suburban. The **NM** identifies the Community Character Policy. In this case the Community Character Policy is Neighborhood Maintenance. Finally, the **01** identifies the Community Character Policy area. In this case, it is Area 1.

Note that there may be areas with the same Community Character Policies applied in different neighborhoods or areas that are non-contiguous, but because the character is the same, their CCP Area Numbers are the same. See Figure 11.

Systems Strategies Maps

The Systems Strategies Maps include: The Vehicular Transportation Plan map, the Pedestrian and Bicycle Network Plan map, and the Open Space Plan and Stormwater Management Plan map.

The primary purpose of the Vehicular Transportation Plan map is to display proposed street connections, and proposed changes to the Major and Collector Street Plan. The Vehicular Transportation map also may display capital improvement budget items, Long Range Transportation Projects and projects in the Transportation Improvement Program. Similarly, the Pedestrian and Bicycle Network map displays where sidewalks and bikeways currently exist and recommended locations for future sidewalks and bikeways. The Open Space Plan and Stormwater Management Plan map displays existing parks and greenways, and shows where new parks and greenways are proposed and their scale. Note that the proposed open space for Rural Hill-Moss Road serves not only a recreational function, but also a stormwater management function.

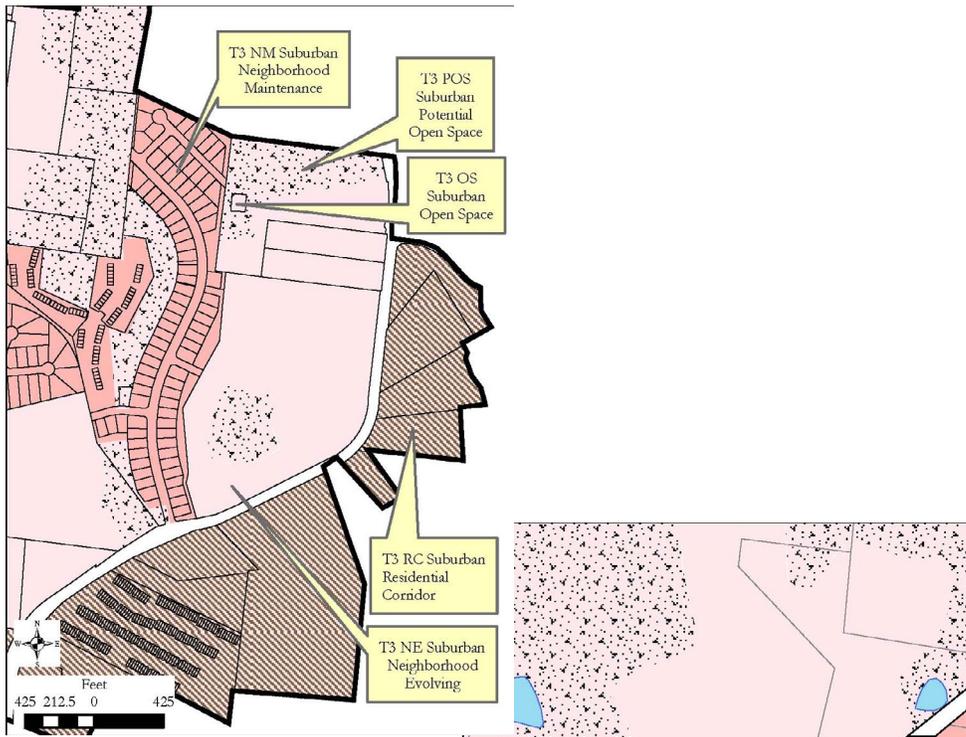


Figure 9: Example of Symbology for Community Character Manual policy categories

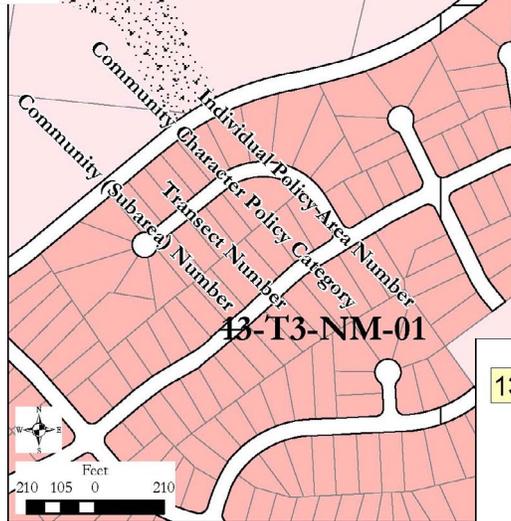


Figure 10: Numbering System for Community Character Manual policy categories

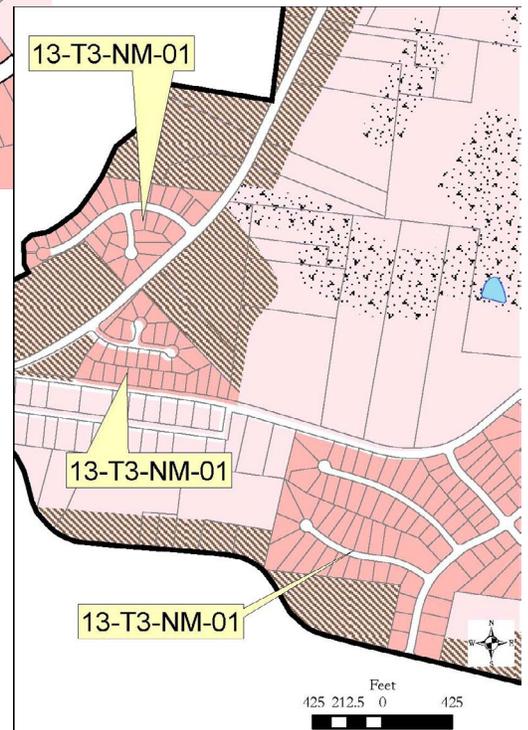


Figure 11: Example of Multiple Areas with Same Area Number

Community Character Policies and System Strategy Maps Influence Future Development

The Rural Hill-Moss Road Detailed Design Plan (DDP) is a policy document. Unlike zoning, it is not regulatory. The Plan does not alter the existing zoning on properties, nor does it initiate the taking of property. Rather, the Rural Hill-Moss Road DDP is used to guide Metro Planning staff recommendations to Metro Planning Commission and Metro Council on future zoning decisions, which is when the future land uses and densities/intensities of the property are determined. Zone change proposals are encouraged to follow the guidance of the Rural Hill-Moss Road DDP. The DDP also guides decisions on Metro's Capital Improvements Budget and Program and also on decisions regarding surplus properties. The DDP guides subdivision decisions to a lesser degree.

The Community Plan and any Detailed Design Plans can set the stage for individual property owners or groups of owners to change their zoning to fully realize the future plan. The Rural Hill-Moss Road DDP may be the first step toward rezoning an area to a Specific Plan District (SP) that implements the DDP.

The SP is a base zoning district where the rules of development are created to be more specific to the proposed development or to achieve a specific vision. It is used to achieve more specific, higher design standards than traditional base zoning would allow.

The SP is a logical next step from the DDP if the community wishes to fully implement its vision. While the Detailed Design Plan applies the more specific policy guidance, which guides decisions on future zone change and subdivision requests, the SP *actually changes zoning and has a regulatory effect*. After a SP is adopted at Council, any final development construction plans submitted for approval of development within the SP must be reviewed to ensure that they follow the standards stipulated in the SP. A SP rezoning could be pursued by the community with the Council members as the applicants, or SP rezoning to implement the plan could occur on a property-by-property basis, when individual property owners decide to develop.

Community Input

The Community Input for this plan consisted of a series of seven meetings held from October, 2008 to December, 2009. After the first Kick-off meeting, which was largely educational in nature, a Visioning Workshop was held on October 20th at Apollo Middle School to discuss the community members' issues and ideas regarding their neighborhood's land use, streets and circulation system, open space and recreation, building types and placement. Participants recorded their ideas on aerial photographs of the study area. The staff used this information to develop a Concept Plan and Vision Statement with Development and Preservation Goals and Objectives. Some common themes heard at the Visioning Workshop included preserving the natural beauty and environmental features of the community, a dislike for the disjointed character of recent development, a desire to connected the community to nearby destinations through streets, sidewalks and recreational corridors, and utilizing design standards to achieve a mix of high quality housing choices in the future.

The concept plan and vision statement, goals and objectives were further refined at a later community meeting on December 4th, 2008. The feedback received at that meeting was used to inform the Community Character Policy Plan and System Strategies and Implementation. These pieces were presented at later meetings and refined based on input received. The revised draft plan was presented to the community on March 19th. During the remainder of 2009, Planning staff worked with other Metro Departments and Councilmembers to further refine the plan. A final community meeting was held on December 3, 2009. The Planning Commission also received two educational work sessions on the plan, once of which was a video about issues facing the area that was also made available to the public through the Planning Commission website. The plan was presented to the Planning Commission at a public hearing on January 28, 2010 and was adopted on that date.



Participants at the Community Meetings.

Vision Statement

Rural Hill-Moss Road is a developing community that has the potential to create a stronger identity and a unique development pattern. It is currently split between new, suburban residential development and longstanding rural development. It is directly adjacent to high intensity mixed use development and community facilities such as schools, public open space, and the Southeast Branch Library. The community's close proximity to a regional mall, interstate access to Downtown Nashville, and other points of interest in Davidson, Rutherford, and Wilson Counties make it an attractive and convenient community in which to live, work, and recreate.

Building a strong sense of community in the Rural Hill-Moss Road community could be based on the appreciation of its attractive natural areas, convenience to commercial services and major points of employment and interest in the Nashville region, and diverse neighborhoods. The Rural Hill-Moss Road community seeks to build on these assets to become a more livable and sustainable community where:

- *The area's natural beauty is preserved and enhanced through the development of an open space network that preserves environmental features such as steep slopes, stream corridors, and viewsheds and ties the community together and to adjacent points of interest such as commercial centers, parks, community facilities, and adjoining neighborhoods;*
- *Established neighborhoods throughout the Rural Hill-Moss Road community are preserved with enhancements tailored to each neighborhood to contribute to a more sustainable development pattern;*
- *New sustainable neighborhoods are created utilizing high standards of design to provide housing choice, connectivity, and convenient access to commercial, employment and civic and public benefit services;*
- *The historic neighborhood center located at Una Antioch Pike, Moss Road, and Mt. View Road is preserved and enhanced to provide a mixture of housing, small-scale commercial, and community services;*
- *The corridors of Una Antioch Pike, Rural Hill Road, Moss Road, and Mt. View Road are preserved and enhanced to provide a balanced mixture of housing and community services behind a deep setback and connected through a multi-modal transportation network that balances automobile, pedestrian, and bicycle travel and features a parkway character;*
- *Supporting community character policies and design-based regulatory tools are used to realize these goals and objectives, with special attention paid to employing high standards of design.*

The Rural Hill-Moss Road community seeks to maintain its community spirit by engaging all stakeholders in achieving the vision to create a more livable and sustainable community.

Chapter 2: Concept Plan, Community Vision and Goals

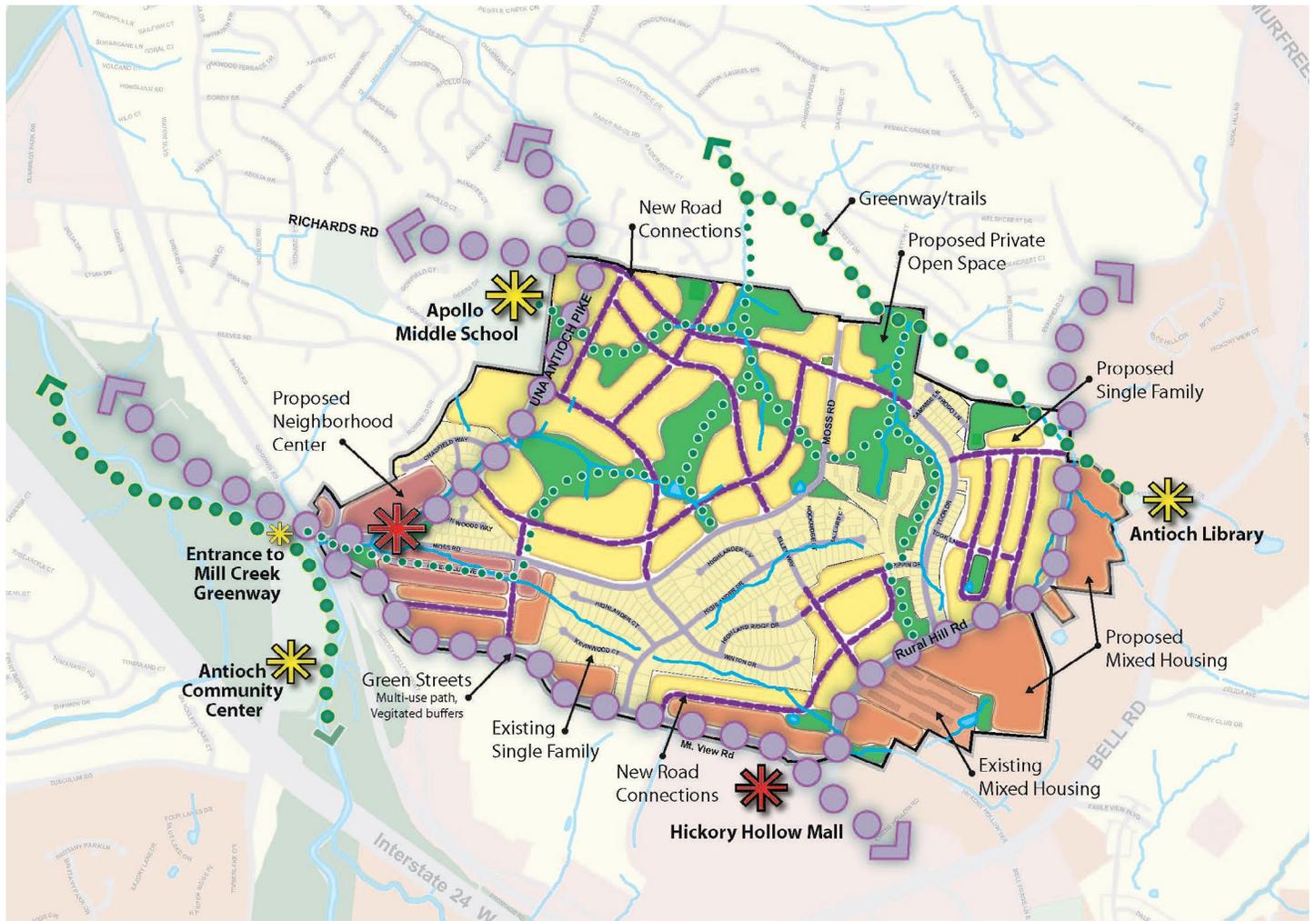


Figure 12: Concept Plan

Concept Plan

The Concept Plan (Figure 12) is a broad graphic representation of the community's vision for future development and preservation of the Rural Hill-Moss Road community. The Concept Plan outlines the appropriate location of particular land uses and provides recommendations for street and pedestrian connectivity, for streetscape enhancement, and for parking and access. Perhaps most importantly, the Concept Plan highlights the open space network that will be a unique feature for this community. The Concept Plan is summarized on the following page and refined into more detail through the Guiding Principles, Vision Statement, and Development Goals and Objectives.

Guiding Principles

Sustainable Development

The Rural Hill-Moss Road Detailed Design Plan reflects a commitment to “sustainable development.” Sustainable development is understood as having four core elements. The Rural Hill-Moss Road Plan meets the four core elements, each of which is discussed below:

1. A commitment to balancing growth with the social/cultural identity of the community and with protection of the environmental assets of the County;

The Rural Hill-Moss Road Plan encourages the preservation of existing, sustainable neighborhoods, while identifying locations for additional housing choice in evolving neighborhoods and in the center and the corridors. The Plan encourages the enhancement and intensification of these areas to add housing choice, support area commercial establishments, and support transit while preserving established neighborhoods. By encouraging intensification in strategic locations, some rural land and land with environmentally sensitive features such as the steep slopes, creeks and floodplains in the area can be preserved.

2. A commitment to creating development that is beneficial to the community today and in the future;

The Rural Hill-Moss Road Plan sets forth development standards that ensure that development will be beneficial to the community today and in the future. The Plan recognizes that Rural Hill-Moss Road, like other communities in Nashville/Davidson County, struggles with the aesthetic and functional qualities of its development. The Rural Hill-Moss Road Plan encourages a form and character of development that creates viable and functional neighborhoods and a key mixed-use portion of historic “downtown Antioch” that will enhance the community by creating a sense of place, providing more services and housing to the community and setting the stage for enhanced transit opportunities. Similarly, the Rural Hill-Moss Road Plan encourages the preservation of sustainable neighborhoods and the enhancement or creation of neighborhoods where additional housing types provide housing choice for today and for the future. Finally, by creating an extended, connected open space and stormwater management space in the community the Rural Hill-Moss Road Plan creates a unique asset

for the community that will distinguish it from other communities in Antioch in the future.

3. A commitment to engaging all stakeholders in planning for growth and development in Nashville/Davidson County; Stakeholders in the Rural Hill-Moss Road community include residents, property owners, institutional leaders, developers, and elected or appointed officials. Community Planning in Nashville/Davidson County relies on stakeholder engagement. The Metro Planning Department utilizes community meetings and other channels for public input in creating the community plans. Community members met over the course of six months to create the Rural Hill-Moss Road Plan.

4. A commitment to thinking regionally in planning for the growth and development of Nashville/Davidson County.

Rural Hill Moss Road’s unique location – immediately adjacent to the Hickory Hollow Mall and its surroundings, which are planned to become a regional-level mixed use center – provides both challenges and opportunities for the future health of the community. On the one hand, the mall is experiencing the negative changes common to malls around the country as the retail environment undergoes a major transformation. This has a direct effect on adjoining neighborhoods. On the other hand, this transformation has the potential to bring about a new era of local growth and prosperity that features a more urban, mixed use form of development that capitalizes on concentrated infrastructure and planned multi-modal transportation.

In creating the Rural Hill-Moss Road Plan community members recognized several assets that their community can build upon. These include their highly accessible location; established neighborhoods and room for new neighborhoods; the direct proximity to the Hickory Hollow Mall area; the convenience to public facilities and services such as the library, schools, park, and greenway; and the beautiful, rolling, semi-rural landscape. All of these assets make the Rural Hill-Moss Road community unique in the region. The Rural Hill-Moss Road Plan recognizes these assets and attempts to capitalize on them in positioning the community for growth and preservation in the future.

The Transect

The Transect is the central organizing tool of the Community Character Policies, which are the main product of the Rural Hill-Moss Road Plan. The Transect is a system for categorizing, understanding and guiding the various development patterns of a region, from the most rural to the most urban. The Transect calls for all elements of the natural and built environment to be consistent with the character of the Transect Category within which they are located.

The Nashville/Davidson County Transect consists of seven categories of natural and built environments:

- T1 Natural
- T2 Rural
- T3 Suburban
- T4 Urban
- T5 Center
- T6 Downtown
- D District

The Transect system is used to ensure diversity of development in Davidson County. The Transect system discourages homogeneous development. The Rural Hill-Moss Road community is evolving from its T2 Rural state in the past to a T3 Suburban community. T3 Suburban Transect areas have a mixture of uses, including residential, civic and public benefit, and mixed uses, that are generally separated from one another, with residential as the predominant use. Building patterns vary, but T3 Suburban Transect areas are generally characterized by moderate to deep setbacks and side yards, curvilinear streets, and informal landscaping. Residential building types include single- and two-family structures as well multi-family structures. The Rural Hill-Moss Road Plan proposes to keep some of the elements of the T2 Rural character of the area – primarily the preservation of significant open space – to create a unique T3 Suburban community.

The Community Elements

The Transect describes the built environment from the most natural and rural areas to the most urban

areas across a region. The “built environment” that the Transect describes is comprised of four basic Community Elements – Open Space, Neighborhoods, Centers and Corridors. Each of these Community Elements is found within most of the Transect Categories, but the scale, character, and intensity of the Community Element varies depending on the Transect Category in which it is located. For example, a Center in the T2 Rural Transect Category may consist of two storefronts at a rural intersection, a Center in the T4 Urban Transect Category may be an entire block or more of shops and offices built to the sidewalk.

The Rural Hill-Moss Road Community Elements

Open space is the least developed Community Element in each Transect Category. In many instances, it preserves the natural environment from growth and development. In other instances, open space may be developed in a low-impact manner to provide recreation opportunities for the community. Open space can take many different forms within a Transect Category, but requires planning in order to create a space that is truly beneficial to its users.

While the Rural Hill-Moss Road community has significant undeveloped land, very little of it is *preserved* open space. The preserved open space that does exist is in the form of three small private cemeteries. A key feature of the Rural Hill-Moss Road Plan is the planned, comprehensive, multi-purpose open space network shown on the Concept Plan and described in later chapters.

Neighborhoods are the backbone of the larger community, providing a diversity of housing for residents at every point in the life cycle. The Rural Hill-Moss Road Plan encourages a carefully integrated, well-designed mixture of housing. To strike balance between providing diversity in housing and maintaining the existing or envisioned character of the neighborhood, several factors are considered including building type and location, and the building’s mass, placement, and orientation.

The neighborhoods within the Rural Hill-Moss Road community range from neighborhoods that are primarily single-family housing to neighborhoods that contain

a mixture that ranges from single- and two-family to multi-family housing. The Community Character Policies place neighborhoods in one of two categories – “maintenance neighborhoods,” which indicates that the neighborhood is not anticipated to change significantly within the planning period of seven to ten years, and “evolving neighborhoods,” which indicates that the neighborhood or undeveloped residential land is likely to experience significant change over the planning period. In the Rural Hill-Moss Road community, maintenance neighborhoods exist in the Highlander and Berkeley Hall subdivisions between Moss Road and Rural Hill Road and the Chadwick Hall and Antioch Woods subdivisions off Una Antioch Pike. Evolving neighborhoods exist in most of the remainder of the study area.

Centers are gathering places situated within neighborhoods or at the edges of adjoining neighborhoods or communities. Centers offer access to retail and services, civic and public benefit uses such as schools, churches and post offices, employment and, in some cases, residential options as well. Centers vary in form, character and intensity across the Transect. In T2 Rural settings, centers are encouraged to be smaller main street areas and village centers where the intensity and scale are smaller to complement the rural character. In T3 Suburban and T4 Urban, and T5 Center areas, centers become larger concentrated areas of commercial, residential, employment, entertainment, and civic/public benefit uses that may serve the immediate neighborhood, community, or region.

There is one small center planned for the Rural Hill-Moss Road community that is at the edge of a center that continues to the west along Antioch Pike outside the study area. This undeveloped small scale neighborhood center is located at the end of Payne Road. The Rural Hill-Moss Road Plan encourages the center to develop into a walkable, mixed use area of activity.

Corridors link neighborhoods, communities and the region. The scale and character of the corridor can vary depending on its use and location in a particular Transect Category – T2 Rural, T3 Suburban, T4 Urban, T5 Centers or District. Corridors are intended to be designed and to function differently depending on the Transect Category through which they pass. As corridors pass through Transect Categories from a more natural

setting to an urban setting, they change in character and size. The land uses adjacent to corridors change as well. In T3 Suburban areas, corridors act as throughways, moving people to and from the outer areas of the region into more densely populated urban areas. Land uses adjoining these corridors range from residential to commercial uses contributing to the increase in usage of the corridor. Land uses in suburban areas are preferably centered on corridor intersections, but are also found in a linear fashion along the corridor with deeper setbacks, where possible, to accommodate landscaping.

There are three residential corridors in the Rural Hill-Moss Road community that connect open space, neighborhoods, and centers. These are Mt. View Road, Rural Hill Road, and Una Antioch Pike. These corridors are planned to contain a mix of housing, multi-modal transportation facilities, and generously landscaped buffers between the buildings and the street.

Development and Preservation Goals and Objectives

Open Space and Natural Areas

Goal:

Create and enhance open space that serves multiple functions, acts as a focal point and organizing feature of the community, and preserves natural features and areas within the Rural Hill-Moss Road community.

Objectives:

1. Develop an interconnected open space network throughout the neighborhood that takes advantage of topography, water features, and viewsheds.
2. Preserve sensitive natural areas and features such as steep slopes and stream corridors throughout the community.
3. Preserve important viewsheds.
4. Connect the community to the Mill Creek Greenway and to other civic uses such as schools and the library.
5. Seek funding to develop a community park with a regional community center in or near the community, preferably to include an expanded library and with consideration given to using the Hickory Hollow Mall site.
6. Protect and preserve scenic vistas through the application of conservation easements, Transfer of Development Rights, and subdivisions designed to group housing around functional open space areas that also preserve important natural features.



Sitephocus

Houses front on a common open space so it is a recreational asset for the entire community.



Sitephocus

Natural open space can provide recreational opportunities and preservation of scenic vistas.



Sitephocus

Neighborhoods

Goal:

Preserve and enhance the character of established neighborhoods, and create new neighborhoods that contribute to the sustainability, cohesiveness and unique character of the community.

Objectives:

1. Preserve the character of “maintenance” neighborhoods by identifying the character of these areas through the use of community character policies and associated design principles.
2. Enhance and create the character of “evolving” neighborhoods by identifying the envisioned character of these areas through the use of community character policies and associated design principles.
3. In all cases, encourage infill development that is compatible with the character of maintenance and evolving neighborhoods as defined by the community character policies and associated high standards of design.
4. Create housing choices that attract varied age and income groups. Housing choices will predominantly take the form of houses and cottages. Strategically located, higher intensity development such as townhomes and small stacked flats buildings serves as a focal point or a transition between the existing development of Bell Road and the primarily single-family development within the Rural Hill-Moss Road community.
5. Utilize new development as an opportunity to create a cohesive community by providing an interconnected vehicular and pedestrian network and housing options that are compatible in terms of design elements such as scale, orientation, and massing.
6. Recognize the environmental constraints and natural beauty of the remaining undeveloped lands. Design future development to protect and preserve land, maintain natural view sheds, and create interconnected greenway/open space systems. Employ building types and construction techniques that are the least impactful and most compatible with the terrain.



Sitefocus

Cottage court developments can provide a single-family character without the maintenance of a large yard and can blend well with the character of established neighborhoods.



Single-family attached homes are a way to provide a variety of housing types and values in neighborhoods.



Front porches help provide for an active neighborhood streetscape.

Centers

Goal:

Create a visually-cohesive and accessible mixed use center at the intersection of Una Antioch Pike, Moss Road, Mt. View Road, and the railroad tracks that promotes neighborhood and community-serving commercial and civic services and that features compatible housing variety within walking distance.

Objectives:

1. Create a neighborhood center in historic downtown Antioch by identifying significant features worth preserving, while encouraging new development that captures the character of downtown Antioch, and works to create a center that may include vertical mixed use development, and that supports a varied selection of civic, commercial, and residential uses.
2. Enhance street connectivity within and immediately surrounding mixed-use centers both in, and adjacent to, the study area.
3. Enhance walkability and pedestrian safety within and surrounding mixed use centers both in and adjacent to the study area through street design that will slow traffic, enhance landscaping, provision of sufficient separation from vehicle traffic for pedestrians and improved sidewalks and crosswalks.



Buildings in the neighborhood center may be two story and contain a mixture of uses.



Wide front sidewalks in the neighborhood center enhance walkability and provide spaces for outdoor dining or displays.

Corridors

Goal:

Enhance corridors that provide access to and from the Rural Hill-Moss Road community with a balanced mix of residential and open space land uses with mixed use development located at specified intersections, all of which display high standards of design.

Objectives:

1. Enhance corridors in the Rural Hill-Moss Road community to create complete streets – streets that are designed and operated to enable safe, attractive, and comfortable access and travel for all users.
2. Preserve residential corridors as an economically viable option to commercial along all corridors, by encouraging multiple housing options along them.
3. Minimize the number of curb cuts and coordinate access and circulation along corridors such as Rural Hill Road, Moss Road, Una Antioch Pike, and Mt. View Road as development occurs to reduce the number of automobile and pedestrian conflicts, improve traffic flow, and create corridors that function as a whole instead of as separate building sites.
4. Enhance corridors in the neighborhood by locating commercial activity at major intersections or nodes such as Moss Road and Una Antioch Pike and Mt. View Road and Rural Hill Road with a variety of housing mixed with open space.
5. Make streetscape improvements along the corridors including adding pedestrian-scale coordinated signage, sidewalks and crosswalks, landscaping, transit stops, and other streetscape elements that emphasize high standards of design.
6. Create a transition in development intensity between the high-intensity development along Bell Road and the eastern portion of Mt. View Road and the lower-intensity single-family interior of the Rural Hill-Moss Road community through new mixed housing development at an intermediate density.
7. Preserve sensitive environmental features along the corridors through measures such as site and building design.



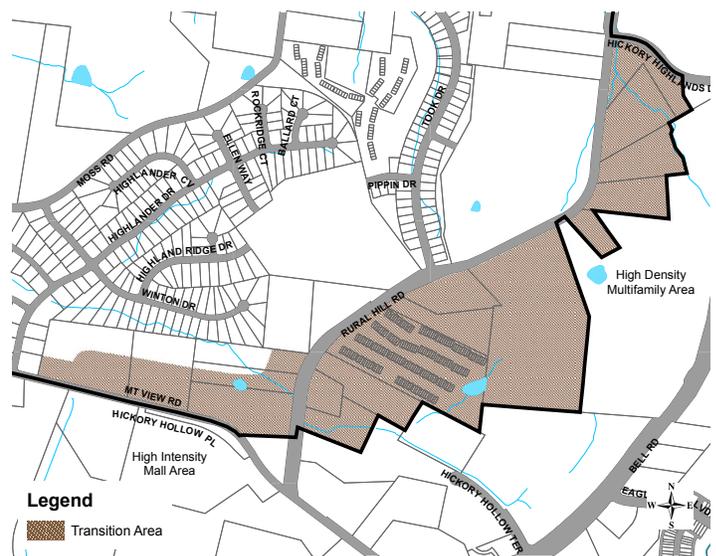
Sitephocus

More intense forms of residential development, such as town homes, are appropriate along Corridors.



Sitephocus

Heavily traveled corridors are still a viable location for residential uses when they are designed to include options for bike and pedestrian travel, as well as appropriate landscaping.



Transition area between higher intensity development outside of the Rural Hill-Moss Road Community and the lower intensity single family development within it. See Objective 6.

Community Appearance

Goal:

Improve the community appearance in terms of building form, landscaping, lighting, signage, and building materials and maintenance.

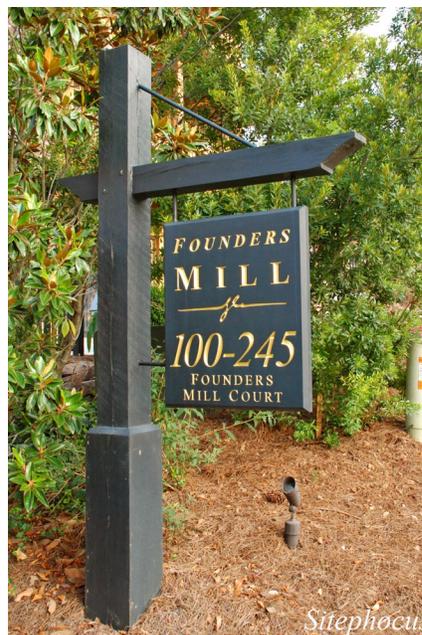
Objectives:

1. Create major gateways into the Rural Hill-Moss Road community through enhanced landscaping and signage treatments.
2. Enhance corridors and new streets through landscaping and street furniture where appropriate.
3. Relocate above ground utilities or shield utilities from public rights of way and adjacent property to have less visual impact.
4. Work with the Metro Codes Department to address codes violations in the Rural Hill-Moss Road community.
5. Promote the harmony and unique character of downtown Antioch through sign standards that are appropriate in scale and design.
6. Construct buildings with durable materials that require little maintenance in order to demonstrate sustained quality and a sense of permanence.



Sitephocus

Incorporating special landscape design features into developments creates a sense of place and adds value for the developer and future residents.



Sitephocus

Special signage can be used to help create a sense of place and community identity.

Chapter 3:

Community Character Policy Plan and Special Policies

The Rural Hill-Moss Road Community Character Policy Plan builds upon the vision and objectives expressed in the Concept Plan. The Community Character Policies take the Concept Plan to the next level of detail by addressing the form, character, preservation and development of areas.

T3 Suburban Open Space Policy

General Character of T3 Suburban Open Space in the Rural Hill-Moss Road Community

In general, T3 Suburban Open Space areas may include publicly-owned parks and open spaces associated with civic and public benefit uses, public or private cemeteries or burial grounds, and privately-held land trusts and conservation easements. The T3 Suburban Open Space areas in the Rural Hill-Moss Road community consist of three small private cemeteries.

How to Use This Guidance

The intent for T3 Suburban Open Space Policy is to preserve and enhance existing open space in suburban areas. T3 Suburban Open Space Policy includes public parks and may also include private land held in conservation by land trusts and private groups or individuals. Users of the *Rural Hill-Moss Road Detailed Design Plan* should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Open Space Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);
- The existing character of the particular T3 Suburban Open Space Area; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan including any Special Policies for the particular T3 Suburban Open Space Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Open Space policy and the General Principles in the CCM are controlling.

T3 Suburban Open Space Community Character Policy Areas

The Rural Hill-Moss Road Detailed Design Plan has three cemeteries where the T3 Suburban Open Space policy is applied. These are titled “T3 Suburban Open Space Policy Areas” in the plan.

Special Policies

The following provides additional guidance on unique conditions that exist in the T3 Suburban Open Space Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Design Principles Found in the T3 Suburban Open Space Policy
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-OS-01

Rural Hill-Moss Road’s T3 Suburban Open Space Area 01 is referenced as 13-T3-OS-01 on the accompanying map. It consists of three small private cemeteries. This policy area does not contain any unique features that warrant Special Policies, therefore, the guidance of the T3 Suburban Open Space policy applies.



Small family cemetery on Moss Road..

T3 Suburban Potential Open Space Policy

General Character of T3 Suburban Potential Open Space in the Rural Hill-Moss Road Community

The T3 Suburban Potential Open Space areas are land that is encouraged to be used as open space in the future as part of a comprehensive open space network that is intended to serve recreational, stormwater management, and amenity purposes. This land is generally vacant and contains undisturbed stream corridors, undisturbed steep slopes, lands adjacent to stream corridors and steep slopes, and lands that when adjacent lands are developed can serve as part of an open space network that provides a combination of stormwater management and active or passive recreational open space functions. The open space network is also intended to contribute to a distinctive identity that will add value to the community as it continues to develop.

Active land uses allowed in open space are generally those associated with civic and public benefit activities, and include play grounds, picnic areas, recreational sports fields, and multi-use paths. Passive open space land uses may include the undeveloped portions of the greenways, nature reserves, and cemeteries. In all cases, T3 Suburban Potential Open Space Areas may have moderate development to allow for active uses while leaving environmentally sensitive areas such as steep topography, dense vegetation, and view sheds undisturbed.

How to Use This Guidance

The intent for T3 Suburban Potential Open Space Policy is to identify areas that should be used for suburban open space in the future. Such open space can be created in the form of public parks or private land held in conservation by land trusts and private groups or individuals. Users of the *Rural Hill-Moss Road Detailed Design Plan* should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Potential Open Space Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);
- The proposed character of the particular T3 Suburban Potential Open Space Area;

- The alternate policy for the particular T3 Suburban Potential Open Space Area; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan including any Special Policies for the particular T3 Suburban Potential Open Space Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Potential Open Space policy and the General Principles in the CCM are controlling.

T3 Suburban Potential Open Space Community Character Policy Areas

The Rural Hill-Moss Road Detailed Design Plan has a planned comprehensive open space network where the T3 Suburban Potential Open Space policy is applied. This is titled “T3 Suburban Potential Open Space Policy Area” in the plan.

Special Policies

The following provides additional guidance on unique conditions that exist in the T3 Suburban Potential Open Space Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Design Principles Found in the T3 Suburban Neighborhood Potential Open Space Policy
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-POS-02

Rural Hill-Moss Road’s T3 Suburban Potential Open Space Area 02 is referenced as 13-T3-POS-02 on the accompanying map. In this area, the following Special Policies apply. Where the special policy is silent, the guidance of the T3 Suburban Potential Open Space Policy applies.

- Design Principle: Connectivity (Pedestrian/Bicycle)
 - Pedestrian and bicycle connectivity to surrounding neighborhoods is high because of this neighborhood’s proximity to adjacent community facilities and services, interstate access, and a major mixed-use center, and also because of an envisioned character that

is built around a comprehensive open space system. Required greenways are included within this open space system. Refer to the Open Space, Pedestrian, and Bikeways Plan Map in Chapter 4 for additional guidance.

- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan requires the construction of numerous new streets and alleys. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street connections in this policy area.
- Design Principle: Density and Intensity
 - Because some small areas of level land without environmental constraints are captured within this POS area, densities of Area 13-T3-NE-03 may apply to such unconstrained land within this POS area when the alternate policy of Conservation otherwise applies.
- Design Principle: Landscaping
 - In general, landscaping in this area is intended to consist of existing vegetation that is left in its natural state. Preservation of existing trees is highly recommended.
- General Principle: Historically Significant Sites or Features:
 - There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.
- General Principle: Stormwater
 - This open space network will also serve as part of the comprehensive stormwater management network for the portions of the Rural Hill-Moss Road Detailed Design Plan area that are adjacent to it.



Land such as this, with slopes and drainage features, is planned to be part of the comprehensive multi-purpose open space network for the Rural Hill-Moss Road community.



Local developments such as Carothers Crossing in Nashville provide examples of how a comprehensive open space network can provide recreational, stormwater management, and community amenity and identity features for neighborhoods.

Low Impact Development stormwater management techniques should be used in this area to the greatest extent possible (see <http://www.nashville.gov/stormwater/LIDResources.asp> for more detail).

- Stormwater management devices in this area should be treated as community amenities in addition to their other functions. Examples of this include the planned greenway system through this POS area, rain gardens, benches, and plantings and retention of native

- vegetation.
- At the date of publication of this Detailed Design Plan, Metro Water Services Department’s Stormwater division was researching and evaluating best practices for creating stormwater management devices and techniques that also serve as aesthetic amenities to the community. Future development in the Rural Hill-Moss Road DDP area is required to at a minimum incorporate the current best practices as specified by the Metro Water Services Department as they may be updated or amended in the future.
- Alternate Policy: Conservation

T3 Suburban Neighborhood Maintenance Policy

General Character of T3 Suburban Neighborhood Maintenance Policy Areas in the Rural Hill-Moss Road Community

T3 Suburban Neighborhood Maintenance policy has been applied to the Highlander and Berkeley Hall subdivisions between Moss Road and Rural Hill Road and the Chadwick Hall and Antioch Woods subdivisions off Una Antioch Pike. These are recent subdivisions that are anticipated to remain largely as they are throughout the planning period of seven to ten years. It is recommended through this policy that these areas be preserved, with some improvements to vehicular and non-vehicular connectivity.

How to Use This Guidance

The intent for T3 Suburban Neighborhood Maintenance Policy is to preserve the general character of the suburban neighborhood. Users of the *Rural Hill-Moss Road Detailed Design Plan* should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Neighborhood Maintenance Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);
- The existing character of the particular T3

- Suburban Neighborhood Maintenance Area; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan and the Antioch – Priest Lake Community Plan including any Special Policies for the particular T3 Suburban Neighborhood Maintenance Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Neighborhood Maintenance policy and the General Principles in the CCM are controlling.

T3 Suburban Neighborhood Maintenance Community Character Policy Areas

The Rural Hill-Moss Road Detailed Design Plan has one area consisting of four subdivisions where the T3 Suburban Neighborhood Maintenance policy is applied. This is titled “T3 Suburban Neighborhood Maintenance Policy Area” in the plan. This Area was identified by examining the general characteristics, development pattern (lot sizes, spacing of homes, density, and zoning), environmental features, and man-made features for the area.

Special Policies

The following provides additional guidance on unique conditions that may exist in a particular T3 Suburban Neighborhood Maintenance Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Infill Areas
- Design Principles Found in the T3 Suburban Neighborhood Maintenance Policy
- Building Types
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-NM-01

Antioch – Priest Lake’s T3 Suburban Neighborhood Maintenance Area 01 is referenced as 13-T3-NM-01 on the accompanying map. In this area, the following Special Policy applies. Where the special policy is silent, the guidance of the T3 Suburban Neighborhood Maintenance Policy applies.

- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan recommends an extension of Ellen Way to the south to ultimately intersect with Rural Hill Road after first intersecting with two other planned new streets. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street connections in this policy area.
- General Principle: Historically Significant Sites or Features:
 - There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.



Other established developments in the community include the Hickory Highlands subdivision between Mt. View and Moss Roads.



The Antioch Woods subdivision off Una Antioch Pike is another established subdivision.



The Berkley Hall Subdivision off Rural Hill Road is one of the most recent developments in the community.



Townhouses in the Berkley Hall Subdivision.

T3 Suburban Neighborhood Evolving Policy

General Character of T3 Suburban Neighborhood Evolving Policy Areas in the Rural Hill-Moss Road Community

T3 Suburban Neighborhood Evolving policy has been applied to a large proportion of the study area, most of which is undeveloped or very sparsely developed. These areas are intended to evolve as single-family neighborhoods with a mixture of lot sizes. The neighborhoods will generally correspond to the gross density of the neighboring T3 Suburban Neighborhood Maintenance areas, but will reflect the presence of the comprehensive open space network, which will cause some variation in the sizes of individual lots. The variation in lot sizes will also result in some of the lots being accessed through alley networks rather than through front driveways.

How to Use This Guidance

The intent for T3 Suburban Neighborhood Evolving Policy is to create suburban neighborhoods that are compatible with the general character of classic suburban neighborhoods as characterized by their building form, land use and associated public realm, with opportunities for housing choice and improved pedestrian, bicycle and vehicular connectivity. Users of the *Rural Hill-Moss Road Detailed Design Plan* should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Neighborhood Evolving Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);
- The proposed character of the T3 Suburban Neighborhood Evolving Areas;
- The envisioned character of other surrounding policy areas; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan including any Special Policies for the particular T3 Suburban Neighborhood Evolving Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Neighborhood Evolving policy and the General Principles in the CCM are controlling.



Single-family alley accessed housing types envisioned in the Neighborhood Evolving Policy.



Single-family with driveways accessing the street directly are envisioned in the portions of the Neighborhood Evolving Policy that are environmentally constrained.

T3 Suburban Neighborhood Evolving Community Character Policy Areas

The Rural Hill-Moss Road Detailed Design Plan has several neighborhoods where the T3 Suburban Neighborhood Evolving policy is applied. These are titled “T3 Suburban Neighborhood Evolving Policy Areas” in the plan. These Areas were identified by examining the general characteristics, development patterns (lot sizes, spacing of homes, and in some instances density and intensity, and zoning), environmental features, and man-made features for each area. While there are multiple areas where T3 Suburban Neighborhood Evolving Policy is applied, each area is different in some respect due to the factors mentioned above.

Special Policies

The following provides additional guidance on unique conditions that may exist in a particular T3 Suburban Neighborhood Evolving Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Design Principles Found in the T3 Suburban Neighborhood Evolving Policy
- Building Types
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-NE-02

Rural Hill-Moss Road's T3 Suburban Neighborhood Evolving Area 02 is referenced as 13-T3-NE-02 on the accompanying map. In this area, the following Special Policies apply. Where the special policy is silent, the guidance of the T3 Suburban Neighborhood Evolving Policy applies.

- Design Principle: Access
 - Alleys are the preferred form of access. The use of single-access driveways from the street should be avoided wherever possible.
- Design Principle: Building Form (Mass, Orientation, Form)
 - Buildings should be oriented to streets or open space, but should never have double-frontage on two streets.
- Design Principle: Connectivity (Pedestrian/Bicycle)
 - Pedestrian/Bicycle connectivity is higher than in a typical T3 Suburban Neighborhood Evolving Area because of this neighborhood's proximity to adjacent community facilities and services, interstate access, and a major mixed-use center, and also because of an envisioned character that is built around a comprehensive open space system. Refer to the Open Space, Pedestrian, and Bikeways Plan Map in Chapter 4 regarding greenways and open space.

- Sidewalks are required on both sides of all new public and private streets developed within this Special Policy Area as well as along both sides of any existing streets that border any new developments requiring development approval within this Special Policy Area.
- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan requires the construction of numerous new streets and alleys.. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street and alley connections in this policy area.
- Design Principle: Density/Intensity
 - Because of the locational characteristics noted above, the carrying capacity of the somewhat environmentally constrained land, and local infrastructure capacity, the gross density of the area should be similar to that permitted within the adjacent 13-T3-NM-01, which is zoned RS7.5. The SP Zoning Template in Chapter 5 represents the preferred choice for future rezonings in this Special Policy Area. Deviations from this Template will need to conform closely to the Vision, Goals and Objectives, and Community Character Policies of this Detailed Design Plan and must meet the general intent of the Rural Hill-Moss Road Detailed Design Plan.
- Design Principle: Landscaping
 - Preservation of existing trees is highly recommended.
- Design Principle: Parking
 - Driveway parking from the street is generally avoided. Instead, parking is provided via the planned alley system.
- Building Types
 - Building types are limited to houses and alley houses, with alley houses being substantially predominant and houses being the rare exception. Houses are justified only when physical site limitations prevent the construction of alleys, which is expected to be a rare occurrence.

- General Principle: Historically Significant Sites or Features:

- There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.

- General Principle: Stormwater

- As much of the stormwater management infrastructure for this area as is feasible should be provided within adjacent POS Area 13-T3-POS-02.
- Low Impact Development stormwater management techniques should be used in this area to the greatest extent possible (see <http://www.nashville.gov/stormwater/LIDResources.asp> for more detail).
- Stormwater management devices in this area should be treated as community amenities in addition to their other functions. Examples of this include rain gardens, benches, and plantings and retention of native vegetation.
- At the date of publication of this Detailed Design Plan, Metro Water Services Department's Stormwater division was researching and evaluating best practices for creating stormwater management devices and techniques that also serve as aesthetic amenities to the community. Future development in the Rural Hill-Moss Road DDP area is required to at a minimum incorporate the current best practices as specified by the Metro Water Services Department as they may be updated or amended in the future.

13-T3-NE-03

Rural Hill-Moss Road's T3 Suburban Neighborhood Evolving Area 03 is referenced as 13-T3-NE-03 on the accompanying map. In this area, the following Special Policies apply. Where the special policy is silent, the guidance of the T3 Suburban Neighborhood Evolving Policy applies.

- Design Principle: Access
 - Driveways from the street are the preferred form of access, except when cottage courts are being developed, in which case other forms of access such as shared rear parking accessed via drive aisles should be considered.
- Design Principle: Building Form (Mass, Orientation, Form)
 - Buildings should be oriented to streets or open space, but should never have double-frontage on two streets.
- Design Principle: Connectivity (Pedestrian/Bicycle)
 - Pedestrian/Bicycle connectivity is higher than in a typical T3 Suburban Neighborhood because of this neighborhood's proximity to adjacent community facilities and services, interstate access, and a major mixed-use center, and also because of an envisioned character that is built around a comprehensive open space system. Refer to the Open Space, Pedestrian, and Bikeways Plan Map in Chapter 4 regarding greenways and open space.
 - Sidewalks are required on both sides of all new public and private streets developed within this Special Policy Area as well as along both sides of any existing streets that border any new developments requiring development approval within this Special Policy Area.
- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan requires the construction of numerous new streets and alleys. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street connections in this policy area.

- Design Principle: Density/Intensity
 - Because of the arrangement of planned open space in this area, the carrying capacity of the somewhat environmentally constrained land, the intended access system for properties, and local infrastructure capacity, gross density should be slightly lower than that permitted within 13-T3-NM-01, which is zoned RS7.5. This area should correspond more closely with the lot sizes permitted under the R10 and RS10 districts. There may be locations where lot sizes are smaller when homes are grouped in arrangements such as cottage courts wherein homes are sited around common open spaces. The SP Zoning Template in Chapter 5 represents the preferred choice for future rezonings in this Special Policy Area. Deviations from this Template will need to conform closely to the Vision, Goals and Objectives, and Community Character Policies of this Detailed Design Plan and must meet the general intent of the Rural Hill-Moss Road Detailed Design Plan.
- Design Principle: Landscaping
 - Preservation of existing trees is highly recommended.
- Building Types
 - Building types are limited to houses and alley houses when part of a cottage court development.
- General Principle: Historically Significant Sites or Features:
 - There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.



Lenox Village is a local development that provides some good examples to emulate in the Rural Hill-Moss Road Community.

- General Principle: Stormwater
 - As much of the stormwater management infrastructure for this area as is feasible should be provided within adjacent POS Area 13-T3-POS-02.
 - Low Impact Development stormwater management techniques should be used in this area to the greatest extent possible (see <http://www.nashville.gov/stormwater/LIDResources.asp> for more detail).
 - Stormwater management devices in this area should be treated as community amenities in addition to their other functions. Examples of this include rain gardens, benches, and plantings and retention of native vegetation.
 - At the date of publication of this Detailed Design Plan, Metro Water Services Department's Stormwater division was researching and evaluating best practices for creating stormwater management devices and techniques that also serve as aesthetic amenities to the community. Future development in the Rural Hill-Moss Road DDP area is required to at a minimum incorporate the current best practices as specified by the Metro Water Services Department as they may be updated or amended in the future.

T3 Suburban Neighborhood Center Policy

General Character of the T3 Suburban Neighborhood Center in the Rural Hill-Moss Road Community

T3 Suburban Neighborhood Center policy has been applied to the undeveloped small scale area of mixed use zoning located at the end of Payne Road on Una Antioch Pike. This area is part of a larger neighborhood center that extends westward outside of the study area along Antioch Pike and forms the heart of historic “downtown Antioch.” The T3 Suburban Neighborhood Center policy encourages pedestrian friendly centers with a mix of commercial, office, mixed use, civic and public benefit land uses. Any residential is present only in mixed use buildings. These centers are generally located at intersections of prominent suburban streets and they serve suburban neighborhoods within a five minute drive.

How to Use This Guidance

The intent for T3 Suburban Neighborhood Center Policy is to enhance and create suburban neighborhood centers that are compatible with the general character of suburban neighborhoods as characterized by the service area, development pattern, building form, land use, and associated public realm. Where not present, enhancements should be made to infrastructure and transportation networks to improve pedestrian, bicycle and vehicular connectivity. Users of this plan should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Neighborhood Center Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);
- The proposed character of the T3 Suburban Neighborhood Center Areas;
- The envisioned character of other surrounding policy areas; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan including any Special Policies for the particular T3 Suburban Neighborhood Center Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Neighborhood Center policy and the General Principles in the CCM are controlling.



Buildings in the neighborhood center may be two story and contain a mixture of uses.

T3 Suburban Neighborhood Center Community Character Policy Area

The Rural Hill-Moss Road Detailed Design Plan has one area where the T3 Suburban Neighborhood Center policy is applied. This is titled “T3 Suburban Neighborhood Center Policy Area” in the plan. This center was identified by examining the general characteristics, development patterns (lot sizes, street setbacks, spacing of buildings, and in some instances density and intensity, and zoning), environmental features, and man-made features for the area.

Special Policies

The following provides additional guidance on unique conditions that may exist in a particular T3 Suburban Neighborhood Center Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Design Principles Found in the T3 Suburban Neighborhood Center Policy
- Building Types
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-NC-01

Rural Hill-Moss Road’s T3 Suburban Neighborhood Center 01 is referenced as 13-T3-NC-01 on the accompanying map. In this area, the following Special Policies apply. Where the special policy is silent, the guidance of the T3 Suburban Neighborhood Center Policy applies.



Although a small area, the planned neighborhood center can provide goods, services, and a gathering place for area residents and visitors.

- Design Principle: Access
 - Alleys are the preferred form of access. The use of single access driveways from the street should be avoided wherever possible.
- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan requires the construction of numerous new streets and alleys and street realignments. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street and alley connections and street realignments in this policy area.
- Design Principle: Density / Intensity
 - The SP Zoning Template in Chapter 5 represents the preferred choice for future rezonings in this Special Policy Area. Deviations from this Template will need to conform closely to the Vision, Goals and Objectives, and Community Character Policies of this Detailed Design Plan and must meet the general intent of the Rural Hill-Moss Road Detailed Design Plan.
- Design Principle: Parking
 - With the exception of Una Antioch Pike, where on-street parking would be problematic due to traffic patterns in the area, parking is provided on-street or on-site on surface lots. When provided on-site, no parking is allowed between the building and the street unless the building

is “L” shaped and the portion of the building that comes up to the edge of the right-of-way occupies the majority of the lot frontage.

- Design Principle: Signage
 - Monument signs are acceptable in this area.
- General Principle: Historically Significant Sites or Features:
 - There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.
- General Principle: Stormwater
 - Low Impact Development stormwater management techniques should be used in this area to the greatest extent possible (see <http://www.nashville.gov/stormwater/LIDResources.asp> for more detail).
 - Stormwater management devices in this area should be treated as community amenities in addition to their other functions. Examples of this include rain gardens, benches, and plantings and retention of native vegetation.
 - At the date of publication of this Detailed Design Plan, Metro Water Services Department’s Stormwater division was researching and evaluating best practices for creating stormwater management devices and techniques that also serve as aesthetic amenities to the community. Future development in the Rural Hill-Moss Road DDP area is required to at a minimum incorporate the current best practices as specified by the Metro Water Services Department as they may be updated or amended in the future.



Figure 14: Future Character Illustration for Residential Corridor Policy Buffer: Townhouses and other moderately intensive residential building forms are envisioned along the Residential Corridor Policy areas of Una Antioch Pike, Rural Hill Road and Mt. View Roads. While oriented to the street, these buildings are set back behind a sizeable buffer from the street.

T3 Suburban Residential Corridor Policy

General Character of T3 Suburban Residential Corridors in the Rural Hill-Moss Road Community

T3 Suburban Residential Corridor policy has been applied along Una Antioch Pike, the north side of Mt. View Road, and along the east side of Rural Hill Road. T3 Suburban Residential Corridors are prominent due to their role in connecting communities and other prominent streets, their size and scale, and/or their accessibility by a variety of transportation modes. Given the prominence, connectivity, and scale of these corridors, they are ideal for providing housing choice. Moreover, the planned land use pattern provides a transition from more intense land uses outside the study area to less dense development within the study area. In addition to providing opportunities for a diversity

of housing, T3 Suburban Residential Corridors should also feature multiple modes of transportation, creating “Complete Streets” – streets designed and operated to enable safe, attractive and comfortable access and travel for vehicles, transit, pedestrians and cyclists.



Figure 15: Future Character Illustration for Residential Corridor Policy Housing Mix: The interior drives within larger multi-family developments should either be designed to be public streets or to conform to public street standards, and should include sidewalks and other pedestrian friendly streetscape elements.

How to Use This Guidance

The intent for T3 Suburban Residential Corridor Policy is to preserve, enhance and create suburban residential corridors that support predominately residential land uses; are compatible with the general character of suburban neighborhoods as characterized by development pattern, building form, land use, and associated public realm; and that move vehicular traffic efficiently while accommodating sidewalks, bikeways, and mass transit. Users of the *Rural Hill-Moss Road Detailed Design Plan* should meet the policy intent by creating and evaluating development and preservation plans in light of the following:

- The T3 Suburban Residential Corridor Policy including the policy intent, general characteristics, design principles and all other guidance provided in the policy;
- The General Principles found in the Community Character Manual (CCM);

- The proposed character of the particular T3 Suburban Residential Corridor Area; and
- Additional guidance provided by the Rural Hill-Moss Road Detailed Design Plan including any Special Policies for the particular T3 Suburban Residential Corridor Area.

Note that if the Special Policy for an area does not provide additional guidance, then the guidance in the T3 Suburban Residential Corridor policy and the General Principles in the CCM are controlling.

T3 Suburban Residential Corridor Community Character Policy Areas

The Rural Hill-Moss Road Detailed Design Plan has several neighborhoods where the T3 Suburban Residential Corridor policy is applied. These are titled “T3 Suburban Residential Corridor Policy Areas” in the plan. These Areas were identified by examining the general characteristics, development patterns (lot sizes, spacing of homes, and in some instances density and intensity, and zoning), environmental features, and man-made features for each area. While there are multiple areas where T3 Suburban Residential Corridor Policy is applied, each area is different in some respect due to the factors mentioned above.



Examples of housing types that might be expected to develop in the future within Area 13-T3-RC-01.

Special Policies

The following provides additional guidance on unique conditions that may exist in a particular T3 Suburban Residential Corridor Policy Area. The Special Policies may cover one or more of the following issues. This list is not exhaustive:

- Design Principles Found in the T3 Suburban Residential Corridor Policy
- Building Types
- Transitions
- Non-Conforming Land Uses
- Historically Significant Sites or Features
- Stormwater

13-T3-RC-01

Rural Hill-Moss Road’s T3 Suburban Residential Corridor Area 01 is referenced as 13-T3-RC-01 on the accompanying map. This Special Policy Area is located along both sides of Una Antioch Pike, along the north side of Mt. View Road, and along the east side of Rural Hill Road. In this area, the following Special Policies apply. Where the special policy is silent, the guidance of the T3 Suburban Residential Corridor Policy applies.

- Design Principle: Access
 - Alleys are the preferred form of access. The use of single-site access driveways from the street should be avoided wherever possible, but is expected to be more common in locations where property depths are shallow or there are steep slopes. Frontage roads are not supported in this area.
- Design Principle: Block Length
 - Where environmental conditions such as topography permit, blocks are between 300 and 600 feet long to foster appropriate mixing of building types in a neighborhood environment as well as to help maintain reasonable traffic speeds and ensure pedestrian safety and comfort.
- Design Principle: Building Form (Mass, Orientation, Form)
 - The first tier of buildings closest to the three corridors should orient to those corridors behind a substantial vegetated open space buffer (mature trees are strongly preferred to be included) while any tiers of buildings behind the first tier of buildings will orient to an internal, interconnected street network or to open space. It is understood that there are some constrained properties where this buffer may not be feasible. There may also be exceptions where, depending on how properties are combined for development and the distance between intersections, buildings arranged around loop roads in a “close” or “court” pattern may be developed. In such instances, at least some of those buildings should orient to Una Antioch Pike, Mt. View Road, or Rural Hill Road due to their prominence as corridors.

- The mass of individual buildings should not exceed approximately 15,000 square feet. Ideally, there should be at least 2 buildings on shorter blocks and at least 3 buildings on the longest blocks. In addition, building facades should be articulated to break up long frontages. Where topography does not permit the ideal building massing and spacing, the use of functional open space that includes pedestrian connections to break up building mass may be considered. The purpose of these standards is to foster appropriate mixing of building types within each block and to create a pedestrian friendly streetscape.
- Construction techniques should be used that minimize the alteration of the land form, particularly in areas of steep slopes.
- Consideration may be given to up to five stories in this area where adjacent existing and permitted future buildings outside the study area are of equal or higher elevation.
- “Tuck-under” parking (industry term for partial underground parking enabled by topographic conditions) levels do not count as stories.

■ Design Principle: Connectivity (Pedestrian/Bicycle)

- Sidewalks are required on both sides of all new public and private streets developed within this Special Policy Area.
- For developments requiring development approval that are bordered by Una Antioch Pike, a multi-use greenway path is required along the east side of Una Antioch Pike.
- For developments requiring development approval that are bordered by Mt. View Road, a multi-use greenway path is required along the north side of Mt. View Road.
- For developments requiring development



Sitephocus

A mix of building designs along with carefully designed open space adds variety and interest to higher intensity developments.



Sitephocus

Building facades of larger scale buildings should be articulated to break up long frontages and establish a rhythm and proportion to create a pedestrian friendly block structure.



Sitephocus

Private drives within individual developments should be designed to look and function like public streets, with the inclusion of sidewalks and street trees.

approval that are bordered by Rural Hill Road, a sidewalk is required along the east side of Rural Hill Road.

- Design Principle: Connectivity (Vehicular)
 - The Rural Hill-Moss Road Detailed Design Plan requires the construction of numerous new streets and alleys. Street construction techniques should be used that minimize the alteration of the landform, particularly in areas of steep slopes. Refer to the Vehicular Transportation Plan in Chapter 4 for additional guidance on street and alley connections in this policy area.
- Design Principle: Density / Intensity
 - Densities within this area will vary with the carrying capacity of the land, which contains areas of steep slopes that will

limit densities. Densities may be higher than otherwise achieved within the area in cases where exceptional development design is achieved. The SP Zoning Template in Chapter 5 represents the preferred choice for future rezonings in this Special Policy Area. Deviations from this Template will need to conform closely to the Vision, Goals and Objectives, and Community Character Policies of this Detailed Design Plan and must meet the general intent of the Rural Hill-Moss Road Detailed Design Plan.

- Design Principle: Landscaping
 - Preservation of existing trees is highly recommended.
- Design Principle: Parking
 - Driveway parking from streets is avoided and is instead provided via the planned alley system. Parking lots in front of buildings are inappropriate and on-street parallel parking is the preferred method of providing parking in front of buildings.
- General Principle: Historically Significant Sites or Features:
 - There are small family cemeteries that have been documented within the study area, some of which share parcels with other uses. Because of the historic settlement patterns within the Rural Hill-Moss Road Community, it is likely that there may be additional such cemeteries located within the community that have not yet been documented. Therefore, additional study to discover possible cemeteries and archeological surveys if warranted are recommended prior to development of properties within the Rural Hill-Moss Road community.



Local examples of residential multifamily buildings where the mass does not exceed 15,000 square feet. Generally 2-3 of these buildings fit on each block.

- General Principle: Stormwater
 - Low Impact Development stormwater management techniques should be used in this area to the greatest extent possible (see <http://www.nashville.gov/stormwater/LIDResources.asp> for more detail).
 - Stormwater management devices in this area should be treated as community amenities in addition to their other functions. Examples of this include rain gardens, benches, and plantings and retention of native vegetation.
 - At the date of publication of this Detailed Design Plan, Metro Water Services Department's Stormwater division was researching and evaluating best practices for creating stormwater management devices and techniques that also serve as aesthetic amenities to the community. Future development in the Rural Hill-Moss Road DDP area is required to at a minimum incorporate the current best practices as specified by the Metro Water Services Department as they may be updated or amended in the future.



Examples of site and building design that is sensitive to steep slopes and retains existing vegetation.

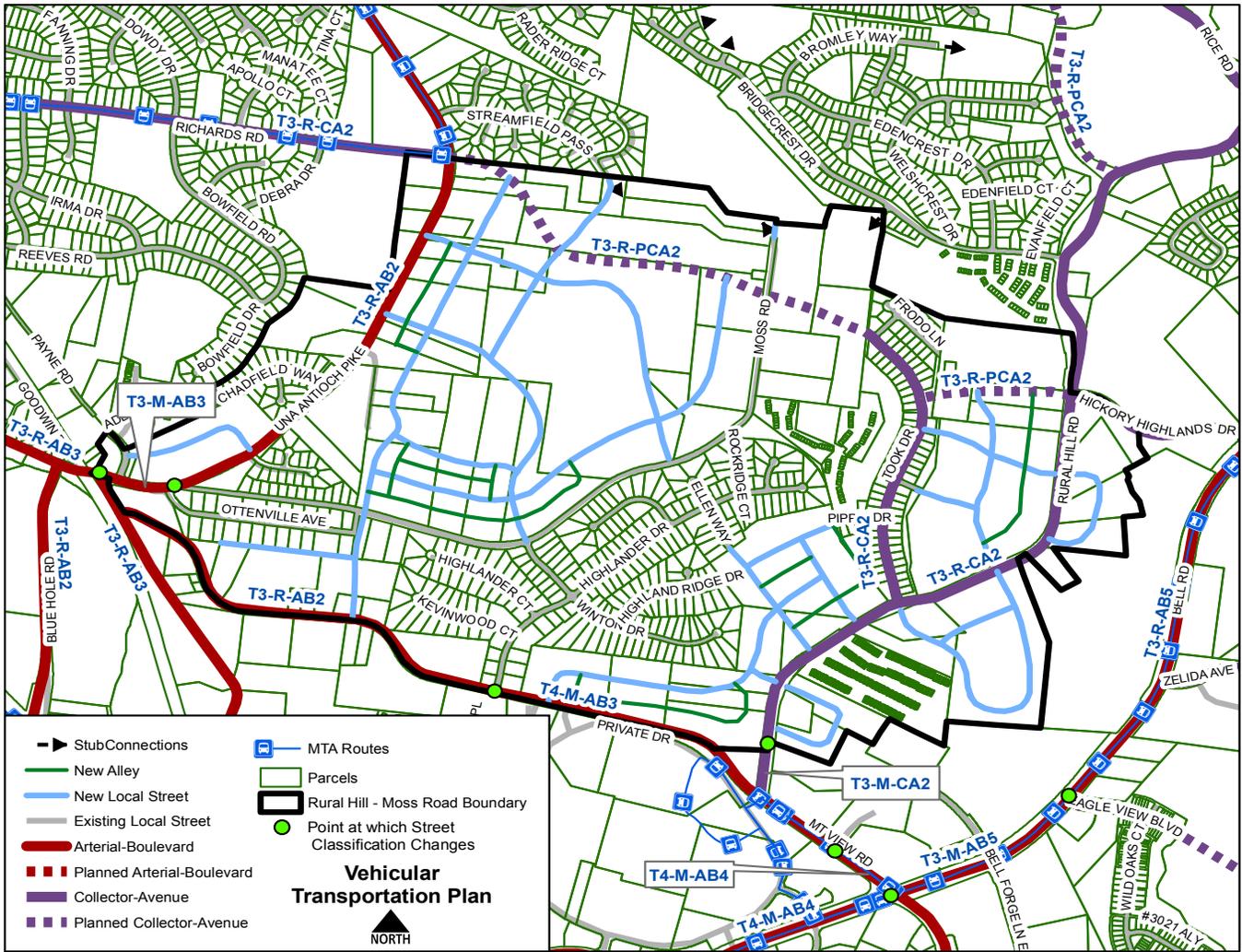


Figure 16: Vehicular Transportation Plan Map



View of Una Antioch Pike near the intersection of Hickory Hollow Parkway and the railroad crossing.

Chapter 4: Systems Strategies

Introduction

This chapter addresses several systems that must be thoughtfully planned to ensure the future success of the Rural Hill-Moss Road community. These include vehicular transportation, pedestrian and bicycle transportation, the comprehensive open space network, and schools. The planning for these systems is closely tied to the vision, goals, and objectives contained in Chapter 2 and the Community Character Policies of Chapter 3.

Vehicular Transportation Plan

Introduction

The Rural Hill-Moss Road area's existing transportation system lacks overall connectivity and functionality, indicating a need for a more comprehensive system as the area continues to develop. I-24 serves controlled-access traffic while Una Antioch Pike, Mt. View Road, Rural Hill Road, Hickory Hollow Parkway, and Murfreesboro Pike provide major surface street transportation. Richards Road provides significant east-west movement to I-24, Haywood Lane and points west. Local streets, primarily built in a disconnected web between the 1970s and today, tend to funnel traffic onto a limited number of the area's overburdened major streets.

Given these conditions, any future development in the area needs support from an adequate number of interconnected local, collector and arterial streets and an adequate public transit system. Figure 16 shows the planned vehicular transportation system for the study area, the recommendations for which are discussed below.

Major and Collector Streets

The adopted Major Street Plan (MSP) and Collector Street Plan (CSP) are the official Metro Nashville/Davidson County Government plans for these types of streets. Implementation of the MSP and CSP occurs through the programming and funding of projects at both the regional and local levels.

Locally funded projects, including those with both Metro and non-Metro funds, are programmed and

funded in Metro's 6-year Capital Improvements Program and Budget (CIB).

Projects under the following funding programs include:

2012-2018 Capital Improvements Budget (CIB)

The Mayor and Metro Council are responsible for approving funding for Capital Improvement projects and Metro Public Works is the lead agency for building street, sidewalk, and bikeway projects. Two projects for the area are recommended to proceed as described in the CIB (Table 4). A series of projects are recommended to be amended so projects can be consolidated where duplication exists and to update project descriptions. Two projects are recommended to be deleted based upon the recommendations from the originally adopted Rural Hill – Moss Road Detailed Design Plan (Table 5).

Additional CIB Guidance: The older CIB projects listed above will need review in order to assess their actual costs and account for any changed conditions that may affect them. A potential alternative to Project 91PW002A above for the Una Antioch Pike / Mt. View Road / Moss Road / Ottenville Avenue / Hickory Hollow Parkway intersection complex could include (see Figure 17):

- Aligning Hickory Hollow Parkway at a 90-degree intersection with Una Antioch Pike and closing leftover right-of-way/alignment.
- Realigning Moss Road for a closer to 90-degree intersection with Una Antioch Pike, which still provides a 300 foot block face between Moss Road and Antioch Woods Way.
- The potential realignment of Mt. View Road at a north-south 90-degree intersection with Una Antioch Pike, which would also have the benefit of allowing a connection of Ottenville Avenue and a planned east-west street just south of Ottenville Avenue, was studied. The expense and environmental constraints involved in this potential project were determined to be excessive and it will be difficult for this project to be successfully undertaken. If this potential project were ever to be deemed feasible, it would provide additional relief to the problems at the intersection complex. The resulting block face

between Una Antioch Pike and Ottenville Avenue would be 230 feet. After closing the leftover right-of-way/alignment from Mt. View Road (portion south of the church and daycare) and small portion of Ottenville Avenue, the resulting block face for the church and daycare is 600 feet.

Table 4: Capital Improvements Budget (CIB) Projects in Rural Hill – Moss Road

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Rural Hill Road and Mt. View Road Intersection Improvements	08PW0018	Improve the Rural Hill Road at Mt. View Road intersection	FY 13 - \$500,000	
Rural Hill Road Resurfacing	13PW0033	Resurface Rural Hill Road from Mt. View Rd Intersection to entrance of Free Will Baptist Church Headquarters	FY 13 - \$50,000	

Table 5: Capital Improvements Budget (CIB) Recommended Amendments

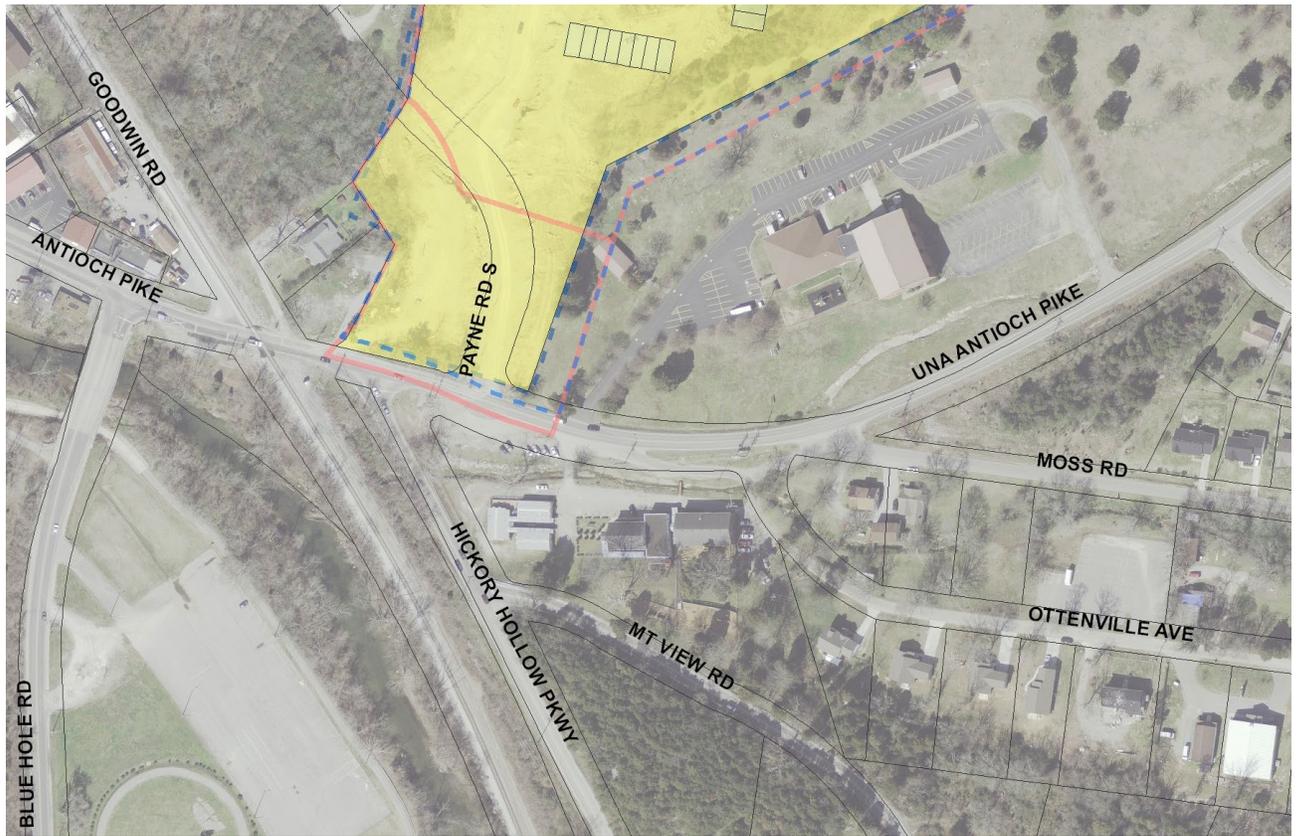
Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Antioch Pike Widening	92PW003	Phase I - Widen On Present Alignment From Richards Road To Reeves Road; Phase 2 - Construct On New Alignment From Reeves Road To Blue Hole Road Includes New Bridge Over Mill Creek And Roadway.	FY 13 - \$350,000 FY 14 - \$350,000 FY 15 - \$700,000 FY 16 - \$2,000,000 FY 17 - \$4,692,000 FY 18 - \$800,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road. -Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways. -Similar project as 10PW0027 and 13PW008; consider combining projects for clarity.				
Antioch Pike Widening	10PW0027	Widen Antioch Pike from Richards Road to Hickory Hollow Pkwy	FY 17 - \$12,900,000 FY 18 - \$5,600,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road. -Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways. -Similar project as 92PW003 and 13PW008; consider combining projects for clarity.				
Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Antioch Pike Strategic Improvements	13PW0008	Widen from Haywood Lane to Blue Hole Road. Project includes signal upgrades, sidewalks and ROW acquisition. ROW, Eng, Design, Construction.	FY 13 - \$200,000 FY 14 - \$6,400,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near the intersections of Richards Road and Blue Hole Road. -Update description to reflect addition of bike lane per the Strategic Plan for Sidewalks and Bikeways. -Similar project as 92PW003 and 10PW0027; consider combining projects for clarity.				
Hickory Hollow Parkway Extension	91PW002A	From Blue Hole Rd To Hickory Hollow Parkway Includes Phase I Engineering and ROW - Construct On New Alignment Bridge And Approaches Over Mill Creek And CSX RR, And Phase 2 - Roadway Up Grade Along Hickory Hollow Parkway	FY 13 - \$1,000,000 FY 14 - \$1,000,000 FY 15 - \$5,000,000 FY 16 - \$5,000,000 FY 17 - \$5,000,000 FY 18 - \$3,000,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan.				
Rural Hill Road Widening	87PW004C	Widen Rural Hill Road from Murfreesboro Pike to Mt. View Road	FY 13 - \$2,000,000 FY 14 - \$2,000 FY 15 - \$2,000,000 FY 16 - \$2,000,000 FY 17 - \$2,000,000 FY 18 - \$3,500,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan. -Similar project as 10PW0030; consider combining projects for clarity.				

Table 5 continued

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Rural Hill Road Widening	10PW0030	Widen Rural Hill Road from Bridgecrest Drive to Mt. View Road	FY 13 - \$5,000,000	
Recommended change to CIB Project: -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Update project descriptions to only include the addition of sidewalks as outlined as a high priority in the Community Plan. -Similar project as 87PW004C; consider combining projects for clarity.				

Table 6: Recommended Projects to be Removed from the Capital Improvements Budget (CIB)

Project	CIB ID	Description	Proposed G.O. Bonds	Federal Funds
Una-Antioch Pike Widening	85PW016A	Widen Una-Antioch Pike from Murfreesboro Pike to 800 feet east of Hickory Hollow Parkway	FY 13 - \$100,000 FY 14 - \$100,000 FY 15 - \$250,000 FY 16 - \$250,000 FY 17 - \$1,000,000 FY 18 - \$1,000,000	
Recommended change to CIB Project: -Remove from CIB. -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Similar project as 10PW0028; recommend removing this project as well.				
Una-Antioch Pike Widening	10PW0028	Widen Una-Antioch Pike from Hickory Hollow Pkwy to Murfreesboro Road	FY 13 - \$20,500,000	
Recommended change to CIB Project: -Remove from CIB. -Widening of the entire corridor may not be feasible given financial constraints. The MCSP recommends widening only near intersections. -Similar project as 85PW016A; recommend removing this project as well.				



Una Antioch-Hickory Hollow-Mt. View-Moss-Ottenville intersection complex - Existing Conditions

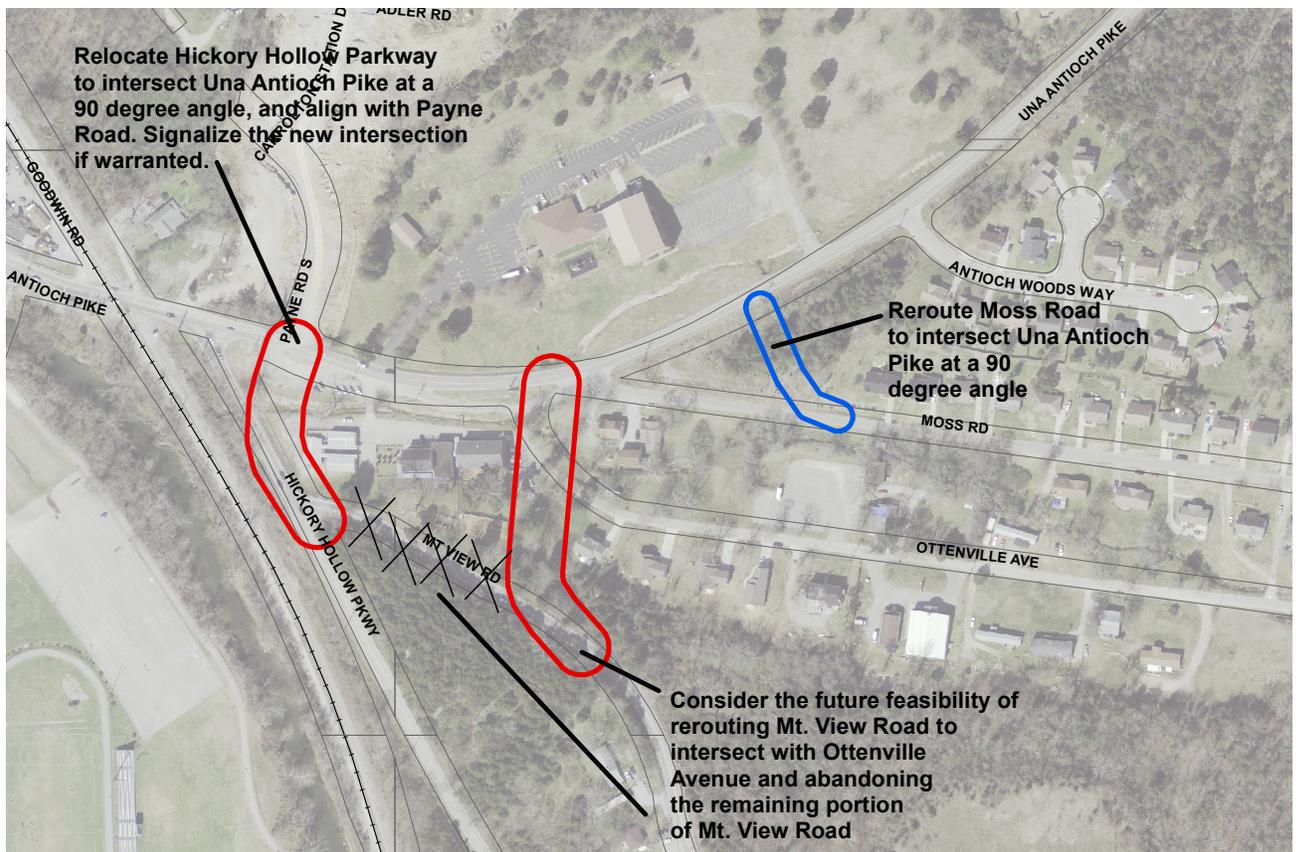


Figure 17: Una Antioch-Hickory Hollow-Mt. View-Moss-Ottenville intersection complex - Sample Solution

Major Street Plan (MSP) & Collector Street Plan (CSP) Recommendations

Consideration of the cost-benefit impact of many MSP / CSP projects makes the wisdom of pursuing these projects questionable. A five-lane surface street generally has a traffic capacity of 35,000-40,000 Average Daily Traffic (ADT). Given recent ADT trends for MSP / CSP projects in the area, the following projects do not appear to warrant large-scale widening.

Any future widening or reconstruction of collector and arterial streets in the area should examine the feasibility of using variations of Metro Public Works' ST-255 as a standard street cross-section in an attempt to increase the area's "green infrastructure" (see Figures 18 and 19 for examples).

The following streets may be candidates for the ST-255 as a standard street cross-section:

- Rural Hill Road
- Mt View Road
- Una Antioch Pike
- Moss Road
- New east-west collector extension of Richards Road to Moss Road
 - At this street's intersection with Moss Road, consider studying a traffic circle as an alternative to a four-way or three-way stop, providing a speed-break at the street's midpoint in the plan area.
 - Due to the importance of this street to future connectivity within and beyond the study area, some flexibility should be given to the amount of grading warranted in constructing this street. The logical route for this planned street from a connectivity and development standpoint causes it to pass through a few areas of steep slopes. To compensate for this, some variance in the right-of-way's cross sections in such areas should be considered in order to achieve the most sensitive design. Techniques could include narrowed lanes, parking prohibitions, and separated and/or meandering lanes and sidewalks.

Table 4 in the Appendix summarizes staff recommendations for streets in terms of:

- Functional Classification (i.e. local, collector, arterial)

- Number of travel lanes
- Use and placement of center turn lanes
- Preferred drainage system (i.e. urban curb and gutter or natural swales)

Local Streets Recommendations

Figure 16 shows a conceptual network of proposed new local streets designed to provide additional street connectivity throughout the Rural Hill-Moss Road community. The conceptual street network is also designed to provide a strong and cohesive block structure to support the Community Character Policies applied in Chapter 3 of this Detailed Design Plan. The reasoning and benefits behind street connectivity include more efficient service delivery, increased route options, decreased vehicle miles traveled (VMT), improved emergency access, and efficient subdivision of land. Many evolving and maintenance areas of the Rural Hill-Moss Road Detailed Design Plan have opportunities to improve street connectivity, which should be provided through the zoning and subdivision processes.

The planned local street network that is shown on Figure 16 is not intended to be precisely followed but is instead intended to be used as a guide to providing increased connectivity and a cohesive block structure. In general, the numbers of east-west and north-south connections shown on Figure 16 are intended to be provided, although actual alignments may vary due to factors such as topographic conditions and property consolidation patterns. The numbers of planned intersections shown on Figure 16 along Una Antioch Pike and Moss Road are especially important because of the lack of connectivity in this large area and the potential for undesirably long blocks along these two existing streets.

In providing the planned local street network, consideration should be given to the following:

- Minimizing alteration of natural slopes,
- Avoiding stream buffers,
- Avoiding other sensitive environmental features such as rare plant species and sinkholes
- Creating a cohesive block structure that allows for an efficient lot layout that corresponds to the appropriate Community Character Policies and the Specific Plan Template contained in Chapter 5 of this Detailed Design Plan,
- Providing substantially improved east-west and north-south vehicular connectivity in the study

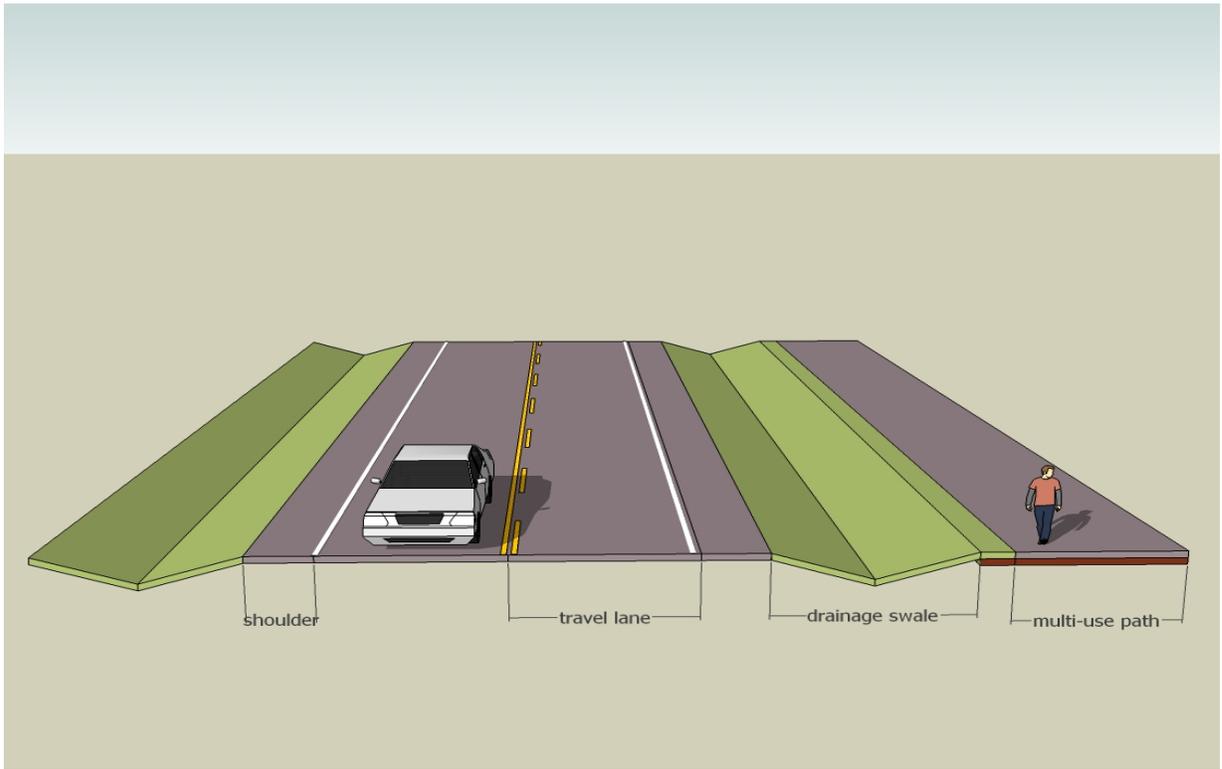


Figure 18: ST-255, Non-Curb and Gutter Section, Light Imprint Development

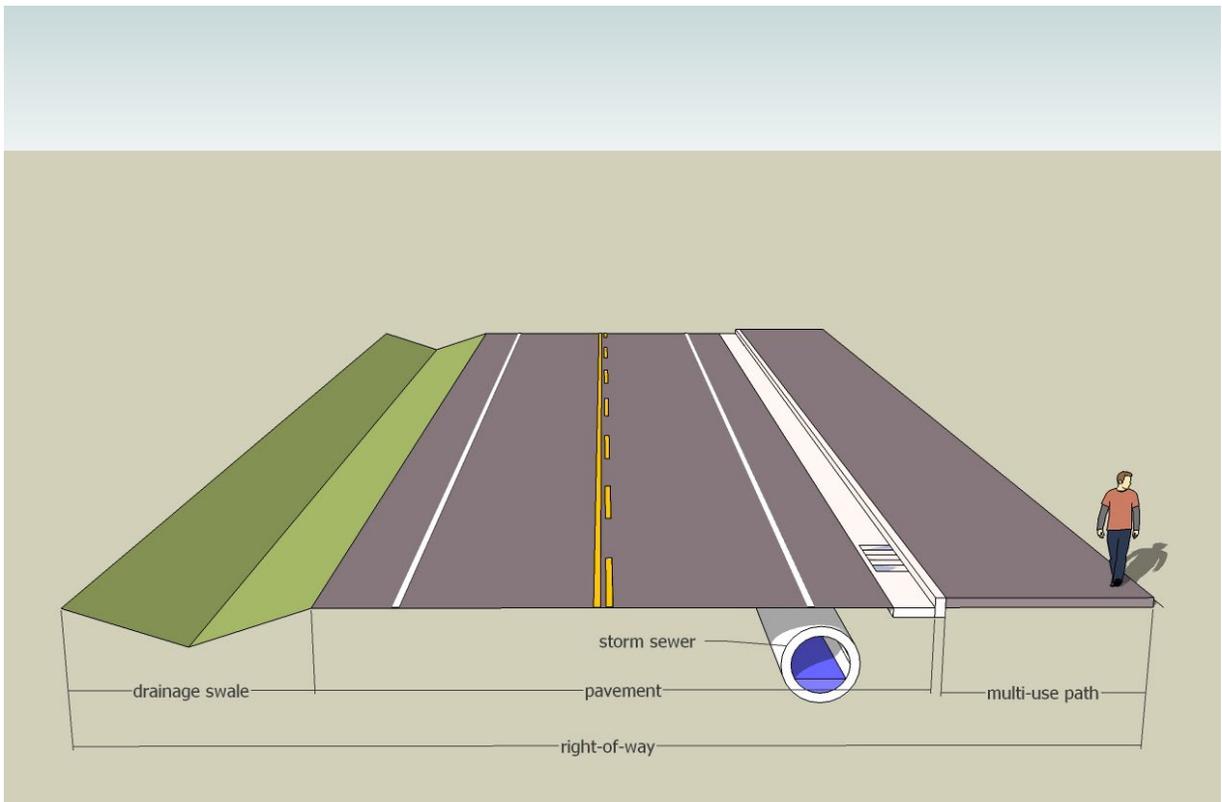


Figure 19: ST-255, Non-Curb and Gutter Section, Hybrid

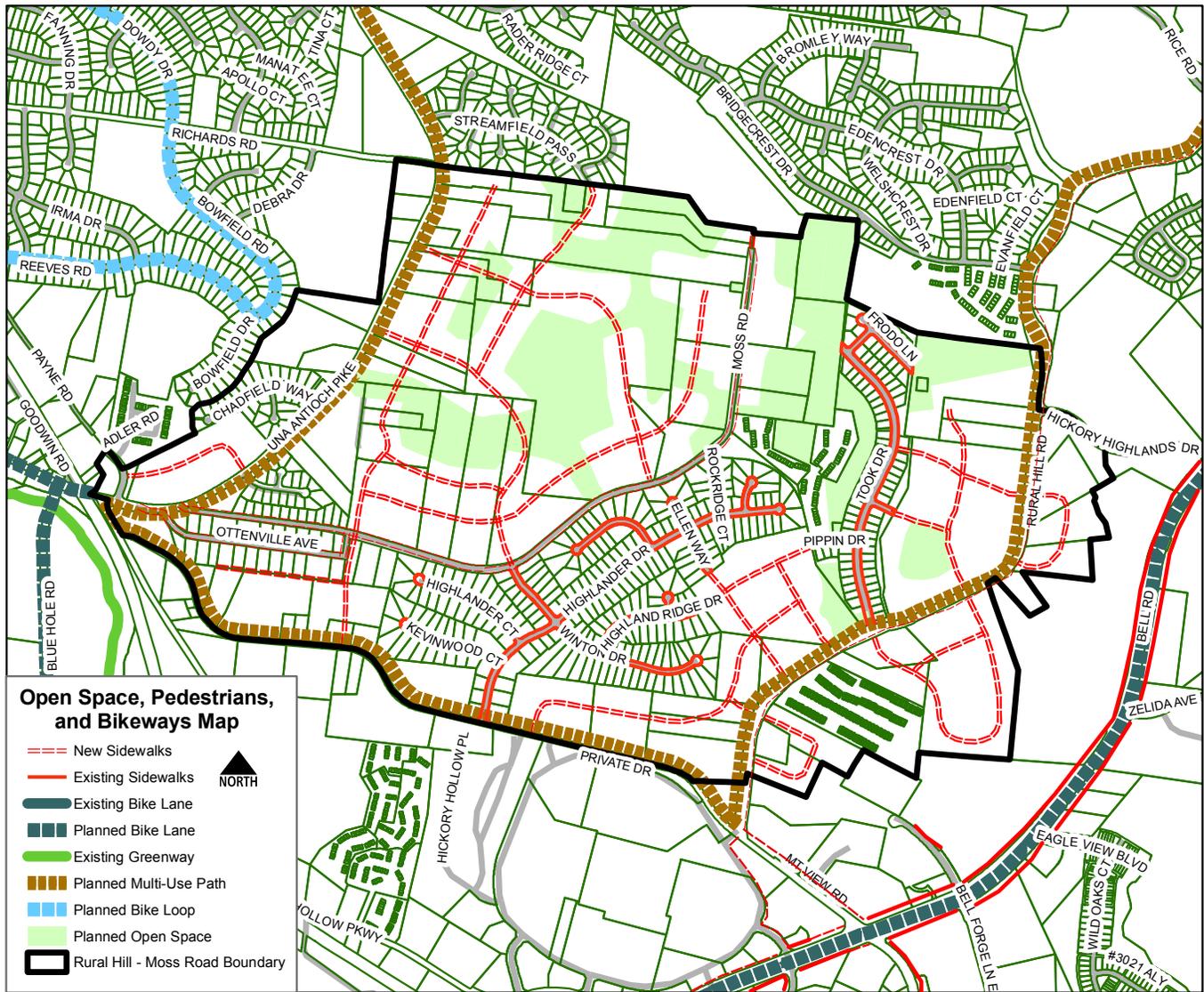


Figure 20: Open Space, Pedestrians and Bikeways Plan Map



Greenway trails provide a safe place for walkers and bikers to enjoy the outdoors.



Pedestrian facilities provided by future development along roadways will provide options for alternate modes of travel as well as recreation.

area particularly to a degree that is sufficient to relieve the proportion of traffic that must use the Una Antioch Pike / Mt. View Road / Moss Road / Ottenville Avenue / Hickory Hollow Parkway intersection complex,

- Aligning new intersections with existing intersections, and
- Using alleys where possible.

Transit Recommendations

The Metropolitan Transit Authority (MTA) currently operates bus lines running in a “pulse network,” meaning lines generally run in and out of downtown Nashville along the radial pikes (Charlotte, Nolensville, Gallatin, Lebanon, Elm Hill, Murfreesboro Pike) rather than crossing each other on a widespread grid.

Route 38X – Antioch Express currently skirts the Rural Hill-Moss Road community via Richards Road and Una Antioch Pike. Just south of the community, Route 15 – Murfreesboro Road services Hickory Hollow Mall with buses along Bell Road and Murfreesboro Pike to Downtown Nashville. Route 27X – Hickory Hollow Express also services the mall and utilizes Interstate 24. A Park and Ride location exists at the mall. MTA plans to enhance transit service in the future with a mini-hub to co-locate with the new Park, Community Center and Library that Metro is constructing. This will also be the terminus of the new MTA Bus Rapid Transit Light Route, via Murfreesboro Road and Bell Road, from Downtown Nashville to Antioch/Hickory Hollow. The new route will include more permanent transit stations with travel time displays and more frequent service along Bell Road and Murfreesboro Pike into Downtown Nashville. Opportunities exist to better connect the Rural Hill-Moss Road community to this improved transit service.

Given these existing conditions, recommendations for future transit include:

- MTA and/or private developers creating “complete transit stops” (shelter, seating, timetable, lighting, trash can, clearly-marked sign with route number, etc.) through the development review process (zoning, subdivision) as collector and arterial streets redevelop with more intense land uses over time.
- MTA creating bus stop spacing guidelines;

recommended guidelines for consolidating existing bus stops and improving service would be placing stops every 780 feet in dense areas (Neighborhood Center) and placing stops every 1,000 feet in medium to low density areas (remainder of study area).

Bicycle and Pedestrian Transportation Plan

Introduction

A complete transportation network provides options for pedestrians and cyclists in addition to vehicles and transit. Providing viable transportation options makes a community more welcoming to more residents, employees and visitors and encourages healthy living. In addition, communities with complete transportation systems help create a more sustainable environment, and can improve the economic viability of a neighborhood. This section, therefore, includes recommendations on the following non-vehicular transportation networks: bikeways, sidewalks, multi-use paths, greenways, crosswalks, and pedestrian signs/signals. Figure 20 shows the recommended **Open Space, Pedestrian, and Bikeways Plan** for the community.

The *Strategic Plan for Sidewalks and Bikeways* (adopted by the Planning Commission in March, 2003 and updated in 2008), establishes priority sidewalk areas and outlines future sidewalk projects planned for the Antioch-Rural Hill neighborhood. The *Plan* can be viewed online at mpw.nashville.gov/IMS/stratplan/. The *Strategic Plan* also includes the *Bikeways Vision Plan* for the community. The *Vision Plan* identifies major and minor roadways that are ideal for bike lanes and bike routes. The overall purpose of the *Strategic Plan* is to enable Metro Nashville to effectively plan and implement sidewalks and bikeways that improve safety, enhance mobility, and promote a higher quality of life for Nashvillians.

The sections below and Figure 20 outline existing on-road pedestrian and bikeway facilities in the Rural Hill-Moss Road community and planned components of the *Strategic Plan for Sidewalks and Bikeways*. Also included are recommendations from community meetings during the creation of the Detailed Design Plan and Planning Department staff analysis regarding pedestrian networks and bicycle facilities in the community.

Pedestrian Facilities

Creating a walkable community involves more than installing a sidewalk or crosswalk. A truly viable pedestrian system involves both the big picture and small details — from how the sidewalk network is planned and developed to the actual materials used during construction of sidewalks and crosswalks. Pedestrian facilities should be accessible to all potential users, including those with disabilities. Also important when creating a walkable community is the consideration of important amenities like pedestrian signals, street furnishings/buffers, and raised medians or pedestrian refuges. Below are descriptions and definitions of important pedestrian facilities:

A *sidewalk* is a walkway that provides people with space to travel within the public right-of-way that is separated from roadway vehicles. The *Strategic Plan* design guidelines recommend a minimum width of five feet for a sidewalk, which allows two people to pass comfortably or to walk side-by-side. Wider sidewalks should be installed near commercial areas, in urban areas, or anywhere with a high concentration of pedestrian traffic.

The *furnishing zone* is the area parallel to the roadway and sidewalk that provides a buffer between pedestrians and vehicles. It may contain landscaping, public street furniture, transit stops, public signage, and/or utilities. The *Strategic Plan* recommends that furnishing zones be a minimum of four feet to six feet, depending on the roadway classification.

The *curb extension/bulb out* is the extension of the sidewalk curb into the roadway that serves the purpose of reducing crossing time for a pedestrian crossing the street, minimizing the pedestrian's exposure to vehicular traffic, and increasing convenience and safety of people crossing a roadway. Curb extensions/bulb outs are most effective on streets that include on-street vehicle parking.

A *multi-use path* is a path that is physically separated from motor vehicle traffic by an open space or barrier, used by bicyclists, pedestrians, joggers, skaters and other non-motorized travelers. The separation from the roadway should be at least six feet and in rural areas the separation should be incorporated through a swale or ditch.

Pedestrian connections are public walkways or pathways not adjacent to a street. They may connect between two

public streets, or between a public street and a public facility such as a school, library, park, or community center. The standard pedestrian connection includes a minimum five-foot sidewalk and landscaped buffers on each side (which may also provide access for maintenance). Pedestrian connections may include other items (such as lighting for safety and security).

Pedestrian signals provide specific guidance to pedestrians as to when they have the right-of-way in the crosswalk; they are set to provide enough time for pedestrians to cross a roadway. All signalized intersections should include pedestrian signals and crosswalk markings at each leg of the intersection and are extremely important in areas with high pedestrian volumes, such as areas near schools or mixed use centers or corridors.

Existing Pedestrian Facilities in the Rural Hill-Moss Road Neighborhood

Sidewalks are relatively sparse in the Rural Hill-Moss Road community due to the prevalence of T3 Suburban development in the community and previous Metro government regulations that did not require sidewalks. Only eight streets within the study area have sidewalks including: Highlander Court, Highlander Drive, Kevinwood Court, Frodo Lane, Took Drive, Ellen Way, Ballard Court and Winton Drive. Sidewalks outside the study area are mostly located in fragments along major roadways including Richard Road, Bell Road, and Hickory Highlands Drive. Sidewalks are also being built along streets in new developments and areas near Metro public schools throughout the community area.

Crosswalks are located at some major roadway intersections in the community including Bell Road at Murfreesboro, but new crosswalks and upgrades are still needed throughout the study area.

Planned Pedestrian Facilities in the Rural Hill-Moss Road Community - The Strategic Plan for Sidewalks and Bikeways

In Nashville/Davidson County, future publicly-constructed sidewalks projects are planned based on the Pedestrian Generator Index or "PGI" (detailed explanation of PGI can be found in the 2008 update of the *Strategic Plan* noted above). If the segment of a roadway scores PGI-rating ranges between low-medium and high (or 20 to 60), it is planned to have a sidewalk built within a ten year period (2008 - 2018), pending the availability of funding. No street segments

within the study area have a 20 and above score rating, so public funding is not expected to occur until a longer time into the future. The PGI's criteria's for sidewalks are generally based on the Transect Categories, proximity to schools, location of public parks and greenways, roadway classifications, and existing transit routes. Other higher scoring factors for new sidewalks include proximity to hospitals, community centers, and public housing. For more detailed information on the sidewalk projects noted above and information about sidewalk maintenance and repairs per the *Strategic Plan*, visit the Public Works engineering department website at: http://www.nashville.gov/pw/divisions/div_engineering.asp.

Recommended Pedestrian Facilities for the Rural Hill-Moss Road Community

A majority of the arterial and collector roadways in the Rural Hill-Moss Road community have segments of future planned sidewalks along them, but a few other major corridors need to be added to the priority list including sections of Una Antioch Pike, Rural Hill Road and Mt View Road. These streets are encouraged by the Rural Hill-Moss Road Detailed Design Plan to redevelop with a significant open space buffer and a multiuse path along the street. These sidewalks will need to receive funding through the development process in order to happen in the 7 to 10 year time horizon of this plan. The Rural Hill-Moss Road DDP contains an Infrastructure Deficiency Area (IDA) policy, which is detailed in Chapter 5, to help fund these needs.

In the future, new development is required to provide pedestrian facilities on both sides of new and most existing streets in the study area. In most cases, these will be standard sidewalks with curb, gutter, and planting strip. However, along the north side of Rural Hill Road, the east side of Mt. View Road, and the north side of Una Antioch Pike, pedestrian facilities will take the form of a multi-use path with a swale instead of gutter for stormwater drainage. These planned sidewalk improvements are depicted in Figure 20.

Additional pedestrian facilities include crosswalks. Crosswalks should be added to the intersection of Hickory Highland Drive and Bell Road. When sidewalks are completed crosswalks should also be added to the intersection of Rural Hill Road and Mt. View Road to improve access to the civic services and retail at Hickory Hollow Mall. Crosswalks are also needed at

the Ottensville Road, Una Antioch, and Hickory Hollow Parkway intersections. Crosswalks within neighborhood centers, commercial corridors, parks and near schools should receive regular maintenance to maintain pedestrian safety.

Bicycle Facilities

The *Bikeways Vision Plan* of the *Strategic Plan for Sidewalks and Bikeways* recommends a county-wide bicycle network. Rather than considering current constraints and ruling out potential bikeways, the Vision Plan recognizes that roadways will be improved and, at that time, options for including bikeways should be considered. The Strategic Plan recommends that if a roadway is called out in the Vision Plan to have bikeways, any future roadway improvement projects on those roads should include bikeways as an important component of the overall project plan and budget.

Bicycling has long been a form of transportation, yet modern-day cyclists face problems related to suburban sprawl, motor vehicle speed and traffic volume. Bicycling on local streets can be an enjoyable form of recreation, but is also a viable transportation option for many. The various kinds of facilities needed to maintain bicycling as a feasible transportation mode have been frequently overlooked in creating our transportation systems. This situation has been changing in recent years, and now people want more ways to get around their communities and elsewhere via bicycle. People want to be able to make bicycling trips in a safe and enjoyable manner. Below are descriptions and definitions of important bikeway facilities:

A *bikeway* is a generic term used to describe a roadway or path that in some form is specifically designated for bicycle travel.

Bike lanes are sections of a roadway that have been designated by striping, signing and pavement marking for the exclusive use of bicycles.

A *bike route* is a roadway designated with appropriate directional and informational route signage for bicycle travel. This type of roadway is mostly a "shared use" road with wide curb lanes or paved shoulders.

Shared use roadway and/or wide outside lane are sometimes synonymous with a bike route. It is a roadway which is open to both bicycle and motor vehicle travel. This may

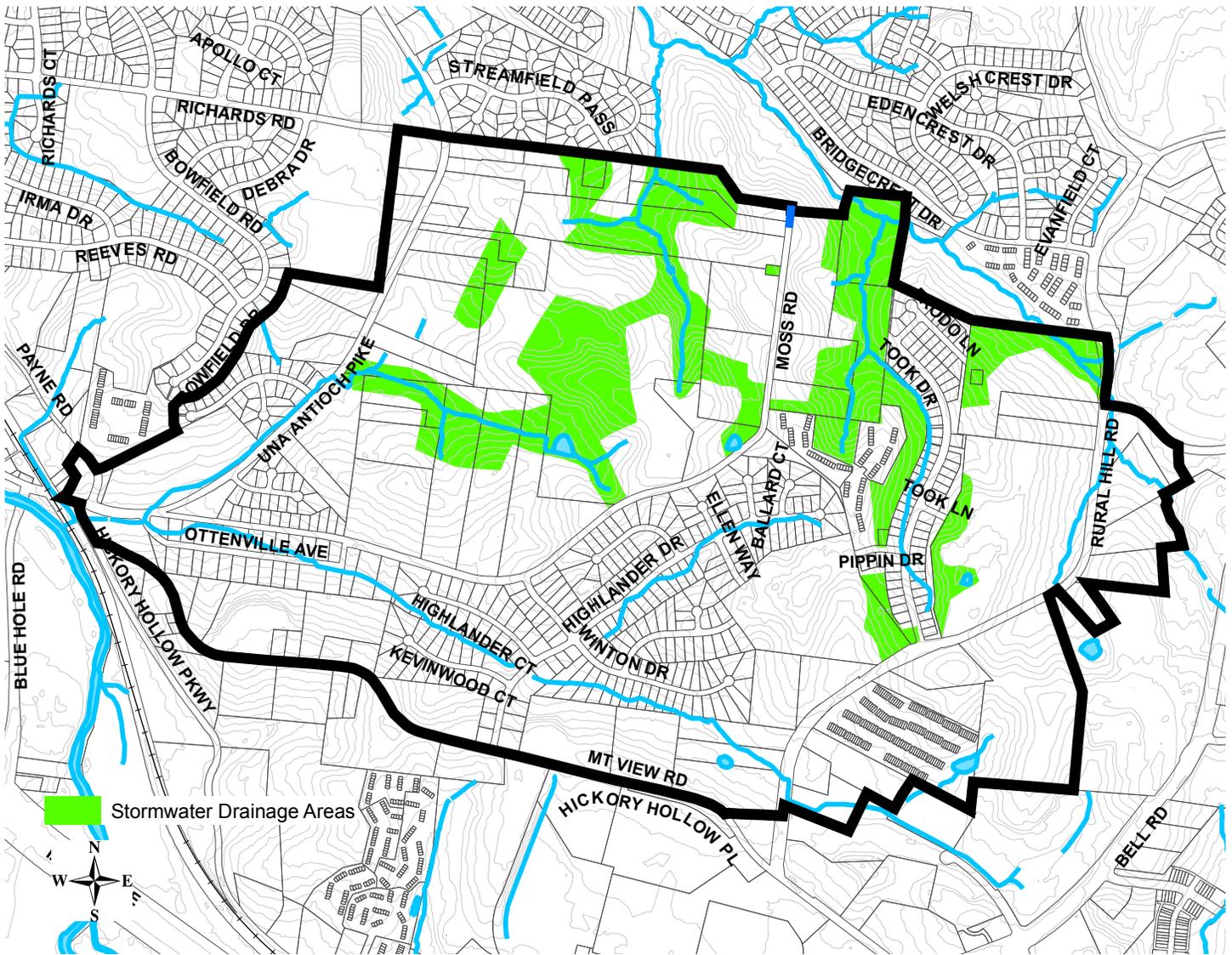


Figure 21: Planned Stormwater Drainage System



Fully vegetated buffers require minimal maintenance and help reduce stormwater mitigation requirements.



In this image, runoff from the street is guided to a vegetated swale through breaks in the curb.

be an existing roadway, a street with wide curb lanes, or road with paved shoulders.

Bike parking includes a designated area and secured equipment (rack) for safely parking bicycles. A lack of bicycle parking is still recognized as a significant deterrent to bicycle use. The availability of safe and convenient parking is important at land uses such as commercial shops, libraries and schools in a community. Rack designs should either be an “Inverted U” or a single loop post. Metro Nashville should adopt a bicycle parking ordinance with design requirements and standards that outline requirements for certain new developments.

Different bikeway facilities are attractive to certain types of cyclists. Cycling options not on the community’s busiest streets have been noted and designated as bike loops. These loops create routes through a series of bike lane and bike routes. They are intended to attract most riders onto low traffic streets for recreational purposes. In some cases, the bike loops connect to a bike lane or bike route on a busier street, connecting recreational riders to routes that provide access to commercial and civic designations.

Existing Bikeways in the Rural Hill-Moss Road Community - The Strategic Plan for Sidewalks and Bikeways

There are no existing on-road bikeways in or near the community. The Mill Creek Greenway however, is adjacent to the community. It is located at the Antioch Park and Community Center, just west of the community. There is a section of the Mill Creek Greenway that is under development. It will extend and connect to another segment of the greenway near Metro’s Ezell Park.

Planned Bikeways in the Rural Hill-Moss Road Community - The Strategic Plan for Sidewalks and Bikeways

The following roads in the community are on the *Vision Plan* for bikeways in the adopted *Strategic Plan*. This planned network of bikeways can also be seen on Figure 16.

- Planned Bike Lanes: Antioch Pike, Blue Hole Road, and Bell Road



Pervious pavement and curbless planting islands work together to minimize stormwater impacts.

Recommended Bikeways Facilities for the Rural Hill-Moss Road Community

The following roads were recommended for bikeways by stakeholders and/or the Planning Department staff during the creation of the detailed design plan:

- Multi-use path along Una Antioch Pike, Rural Hill and Mt.View Road (see section on greenways for descriptions).

Bike Parking

It is also recommended that all T3 Suburban Centers include an appropriate amount of bicycle parking. Since Metro Nashville has yet to adopt a standard or ordinance for bike parking, the general standard is that the number of parking spaces for bikes should reflect five percent of the number of spaces provided for cars. It is recommended that all schools, parks, and centers currently without bike parking should add it where appropriate.

Different bikeway facilities are attractive to certain types of cyclists. Cycling options not on the community’s busiest streets have been noted and designated as bike loops. These loops create routes through a series of bike lane and bike routes. They are intended to attract most riders onto low traffic streets for recreational purposes. In some cases, the bike loops connect to a bike lane or bike route on a busier street, connecting recreational riders to routes that provide access to commercial and civic designations.

Comprehensive Open Space Network

One of the key features of the Rural Hill-Moss Road Detailed Design Plan is the planned, comprehensive open space network. The community will need this open space network to serve recreational purposes as it reaches a more suburban level of development. In addition, this planned open space network is intended to serve as the main component of the stormwater management system for the entire community. Finally, the open space network is intended to serve as one of the developing community's chief amenities, providing not only enjoyment, but also value and contributing to the community's distinct identity and successful market positioning. This open space network is expected to be partly public (the greenways system) and partly private (the open lands that may be held by a variety of ownership entities ranging from homeowner associations to conservation easements held by nonprofits), but is intended to be widely accessed by residents of the Rural Hill-Moss Road Community.

It is important to understand that because this planned open space network will take place through the process of developing privately owned property through the rezoning and subdivision processes, its exact layout and size will be determined over time and may not exactly match the graphics shown in the plan. Nonetheless, these graphics provide a good indication of the expected configuration and extent of this planned network. The sections that follow describe its intended functions.

Recreation

The planned open space network in the Rural Hill-Moss Road Community is intended to serve recreational purposes. This open space network contains both a planned greenway system, which will become part of the Metro Parks Department's greenway system and will link to the nearby Mill Creek Greenway, and unprogrammed open space that can also provide opportunities for exercise, socialization among community residents, constructive activities for young people, and quiet reflection. These elements are shown on Figure 20. The greenways system will contain multi-use paths for pedestrian and bicycle use, and the remainder of the open space could be used for a variety of active and passive recreational activities depending on the suitability of the land and its tree cover, which should be

preserved to the greatest extent possible.

Stormwater Management Best Practices

The purpose of open space is not only to provide for natural beauty and recreational opportunities, but also to minimize the negative impact on environmental resources, including but not limited to: streams, ponds, floodplains, wetlands, steep slopes, scenic views, open fields, farmland, wildlife habitat, habitat of rare and endangered species, and historic sites and features. Figure 21 shows the Planned Stormwater Drainage System – the connected network of open space that will serve not only as a recreational site and amenity, but also as a stormwater management tool for the community.

The term “Stormwater” means precipitation that falls most often as rain, but can also take the form of snow, sleet or hail. Stormwater is best understood in terms of the water cycle. Under natural conditions, about 10 percent of precipitation runs over the land surface and about 50 percent infiltrates the soil to replenish groundwater flow and base flow to streams. Plant uptake and evapo-transpiration account for about 40 percent. Stormwater flows from higher points on the land to lower points, and is best understood from a “watershed” perspective. A watershed is a natural region defined by the land area from which precipitation drains into a particular body of water — a river or lake. One watershed may be part of a larger one, or contain several smaller sub-watersheds. All watersheds, no matter what their size, function in the same manner. Precipitation falls on the land, and then drains from the higher areas over and through the soil until it eventually reaches rivers, streams, lakes, or the ocean.

As land is covered with more impervious surface (streets, driveways and rooftops), larger quantities of runoff, traveling faster, carry more pollutants from the pavement to our waterways. As stormwater runoff travels across the land surface, it picks up a number of pollutants that can pose serious health risks to humans and can disrupt and seriously damage water ecosystems.

All development sites are required to comply with Metro's Stormwater Management Manual for water quality and water quantity to ensure that the adverse impacts of new development are mitigated. This mitigation traditionally has included the use of detention

and retention ponds and manufactured water quality units. The maintenance of these devices can be costly for homeowner associations over time. In this plan, an emphasis has been placed on retaining significant portions of open space in an undisturbed state to serve as a piece of the Rural Hill-Moss Road community’s future stormwater infrastructure network.

Non-structural “Best Management Practices” can enhance the function of structural controls (detention and retention ponds, etc.) by reducing the amount of work structural controls have to perform. When filtration of water can be accomplished through the use of vegetated swales or strips, particulates and sediment can pre-treat runoff before it reaches retention or detention areas. Preserving natural vegetation promotes filtration and infiltration. The use of native vegetation in new plantings is recommended.

Principles for future development projects:

- Minimize disturbance of the site and retain natural features that perform stormwater functions;
- Minimize impervious surfaces; and
- Structural stormwater management facilities should only be used in conjunction with non-structural practices and not as stand alone solutions.

Amenity

Throughout the community meetings, attendees consistently expressed a desire to preserve the natural beauty of the community. The comprehensive open space network is a key component of achieving this aspect of the community’s vision and setting the Rural Hill-Moss Road Community apart from other areas. The proposed open space network preserves environmental features such as steep slopes, stream corridors, and viewsheds and ties the community together and to adjacent points of interest such as commercial centers, parks, community facilities, and adjoining neighborhoods. The open space network acts as a focal point and organizing feature of the community to help further the goal of building a sense of identity and creating a unique community in Antioch. The form and character of the new residential neighborhoods of this community in the future will be shaped by the open space network. Opportunities will often be provided

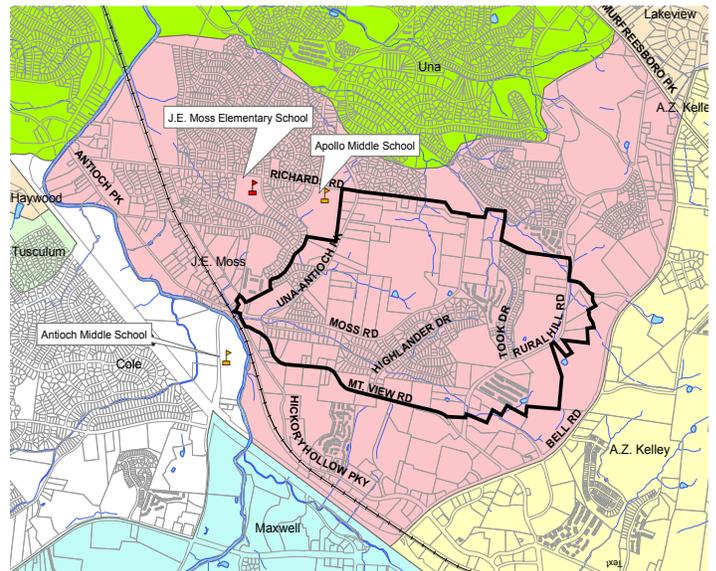


Figure 22: Elementary Schools

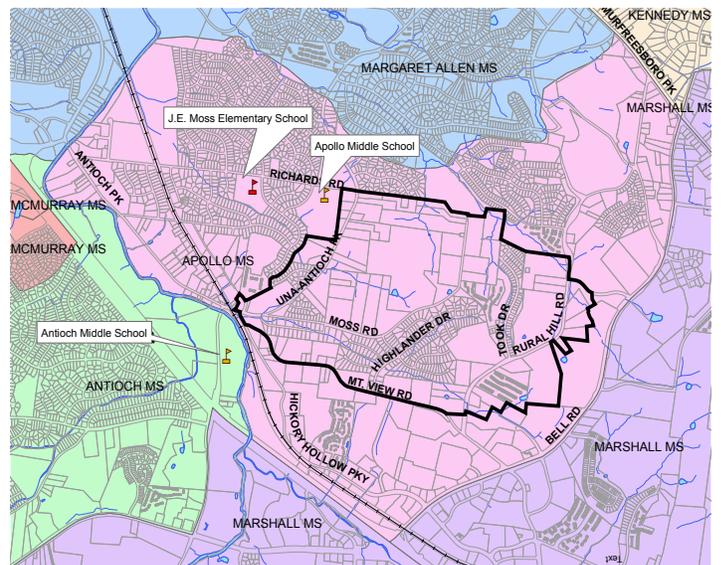


Figure 23: Middle Schools

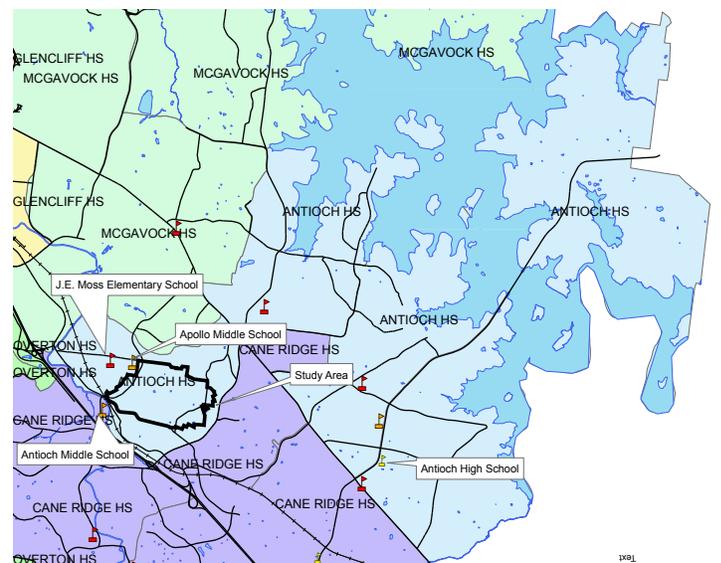


Figure 24: High Schools

for houses to front onto the open space, lending a distinctive identity, sense of place, and value to the residential environment.

Public Schools

School-age children in the Rural Hill-Moss Road community who attend public schools are currently zoned for J.E. Moss Elementary School on Richards Road, Apollo Middle School on Richards Road, and Antioch High School on Hobson Pike. These schools are shown on Figures 22-24.

Capacity

J.E. Moss Elementary School is currently operating at 113 percent of capacity. Adjacent elementary schools are also operating at or above capacity: A.Z. Kelly at 105 percent, Cole at 132 percent, Lakeview Design Center at 123 percent, Henry Maxwell at 98 percent, and Una at 154 percent. Apollo Middle School is currently operating at 83 percent of capacity. Antioch High School is currently operating at 97 percent of capacity.

Construction

Cole Elementary School is currently adding 14 classrooms. Two of these are music rooms. Two existing rooms are being converted to art rooms with a completion date of July, 2009. J.E. Moss Elementary School is currently adding four classrooms and one art room with a completion date of August, 2009. Apollo Middle School is currently adding 16 classrooms and four in the existing building for a total of 20 classrooms with a completion date of December, 2009. Una is currently adding 16 classrooms.

School Growth Needs Generated by this Detailed Design Plan

The development anticipated by this DDP will add enough students to warrant an additional elementary school that will need to be provided in or near the study area. In addition, the growth will affect capacity at the middle and high schools serving the study area. The Planning Department will work with the Schools Department to find appropriate locations to serve these needs.

Chapter 5: Implementation

Implementing the Plan

With this plan, community members have a vision for the future of the Rural Hill-Moss Road area. Making this vision a reality is possible through the efforts and cooperation of public and private stakeholders. Implementing the community vision will take residents, businesses, property owners, institutions, private developers and the public sector working together to measure future public and private investments and development by the Detailed Design Plan (DDP). Many of the changes in the community will occur over time as the result of individual zone changes and development projects. The community can, however, be proactive in implementing the plan by working with private property owners, developers, Council members and Metro Government, to ensure that all investments in the Rural Hill-Moss Road community implement the DDP.

First: Continue to Work Together

The community members within the Rural Hill-Moss Road area include property owners, business owners, residents, institutional representatives, and Council members. Each of these stakeholders have a vision and goals for their property or their neighborhood, but these individuals are encouraged to champion the common vision, goals and objectives within this DDP. These individuals and groups came together collectively to create this DDP and are encouraged to continue to work together to be stewards of the DDP, and the Rural Hill-Moss Road area, in the future. By working together with partners in the private and public sector, these community members can ensure that the Rural Hill-Moss Road area remains “home” to current residents and businesses and welcoming to future residents and businesses.

Second: Work with government agencies to begin implementation of the goals and objectives.

The stakeholders mentioned above can work with their Council members and the public sector to seek funding and support for implementing portions of the DDP. This may take a variety of forms. For instance, the DDP

includes recommendations for bikeway, sidewalk and greenway improvements. Working with the Metro Public Works and Parks Departments and Council members to fund various improvements is an option.

Third: Work with the district Council members and Metro Planning staff to implement the plan through design-based zoning.

The DDP will largely come to fruition if and when properties redevelop. Redevelopment often involves rezoning. As rezonings are requested by property owners, they will be measured for consistency with the Rural Hill-Moss Road DDP. Future rezonings are expected to be in the form of a Specific Plan District. A template for the Specific Plan is included in this chapter.

Alternately, community members and their Council members may decide to implement the DDP through rezoning of all or part of the area to fit the DDP. If the community decides to proceed with a rezoning, Metro Planning staff can assist the Council members in leading a separate series of community meetings on the rezoning. Rezoning properties is a separate, public process that will involve more discussions and community stakeholder involvement.

Infrastructure Deficiency Area Policy

There are areas within the Rural Hill-Moss Road community where existing infrastructure and facilities are insufficient to handle current or expected population needs. Public facilities and services for which this is particularly true are streets, sidewalks and bikeways. Infrastructure and facilities are, in limited cases, provided by the public sector. More frequently, however, they are provided in conjunction with new development, i.e., when a new development is proposed, it is required to improve streets, build new streets, provide sidewalks and bikeways, etc. The Rural Hill-Moss Road Detailed Design Plan (DDP) proposes to harness future development to address infrastructure deficiencies in the area. The Rural Hill-Moss Road community will, with the adoption of this DDP, be named an “Infrastructure Deficiency Area”, and new development will be responsible for assisting the community in upgrading its needed infrastructure.

The Need for an IDA

The Rural Hill-Moss Road area is appropriately named an Infrastructure Deficiency Area (IDA) due to its existing street network and design. The Rural Hill-Moss Road area’s lack of connectivity, street layout, and street design, reflect a rural development pattern rather than the emerging and planned suburban pattern. The rural street network characteristics create problems for traffic capacity, circulation, and residential livability for existing and future development. The rural pattern and design of streets places an undue burden on the few streets that do exist, and contributes to traffic congestion on major streets and speeding on local, collector, and major streets. While the Rural Hill-Moss Road DDP calls for improvements to be made to the transportation system that would alleviate some of the existing and anticipated problems, the timing of those improvements will affect the ability to implement the community character policies of this plan.

Closely related to the deficiencies of the vehicular transportation system are deficiencies in the pedestrian and bicycle transportation system for the area. Although sidewalks will be required on both sides of all new public and private streets constructed in the area, some portions of existing streets will remain without needed sidewalks unless additional provisions are made to construct them. The lack of sidewalks in the area

is especially unfortunate given that the area is near an existing greenway, two public schools, a community center, and commercial areas to which it lacks appropriate pedestrian and bicycle access. The Rural Hill-Moss Road DDP calls for access to be provided through a greenway and multi-use path system, part of which will be located within the community’s planned open space network and part of which will follow existing streets.

Implementation of the IDA

To a degree, vehicular, pedestrian and bikeway improvements will occur through the development process as rezonings and subdivisions take place and housing is built according to this plan. However, there will be cases in which such redevelopment does not occur for a long time or does not occur at all. In those cases, the needed vehicular, bicycle, and sidewalk improvements must wait for other funding sources and gaps will appear in the infrastructure network.

Therefore, approval of any development proposal within the identified IDA (see Figure 25) is required to make the following improvements. A “development proposal” is defined as including, but not limited to, rezoning, UDO, PUD, SP or other overlay as well as a subdivision of property or Special Exception. Any recommendation for rezoning within this designated IDA will include the recommendation language to the Metro Council that is listed below. In the review of any subdivision proposal within this designated IDA, the Metropolitan Planning Commission will include the following recommendation as a condition of the approval of the subdivision or other development proposal. A “subdivision proposal” is defined as including, but not limited to, site plans reviewed by the MPC as part of a PUD, UDO, SP or other overlay as well as a subdivision of property or Special Exception.

Vehicular, Pedestrian, and Bicycle Transportation Improvement Requirements for Existing Streets

- Una Antioch Pike:
 - Requires developer construction of swale and multi-use greenway path on the east side; see Figure 26 for locations.

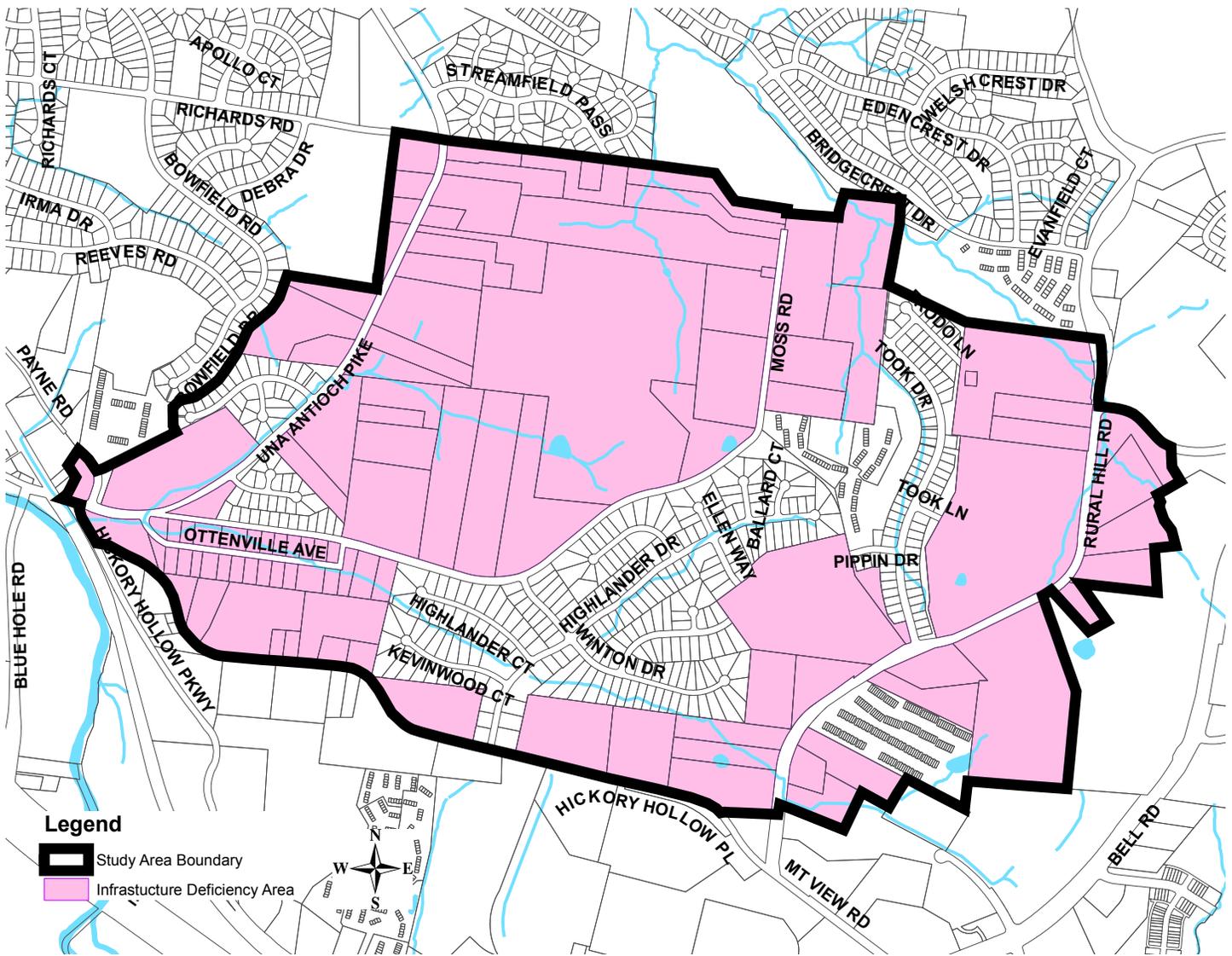


Figure 25: Infrastructure Deficiency Area Map

- Moss Road:
 - Requires developer construction of standard curb and gutter sidewalk on both sides, see Figure 26 for locations.
 - Rural Hill Road:
 - Requires developer construction of swale and multi-use greenway path on the west side and standard curb and gutter sidewalk on the east side, see Figure 26 for locations.
2. All site-related roadway, pedestrian, and bicycle improvements required by the Department of Public Works shall be constructed.
 3. Any required right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
 4. In order to accommodate additional traffic volumes and population growth, the applicant shall contribute to improving the above-identified roadways within the identified infrastructure deficiency area as specified above at a level acceptable to the Department of Public Works.

Additional Requirements:

1. All collectors, required street connections, pedestrian and bicycle facilities, and greenways identified on the adopted Detailed Design Plan and within the subject property shall be constructed.

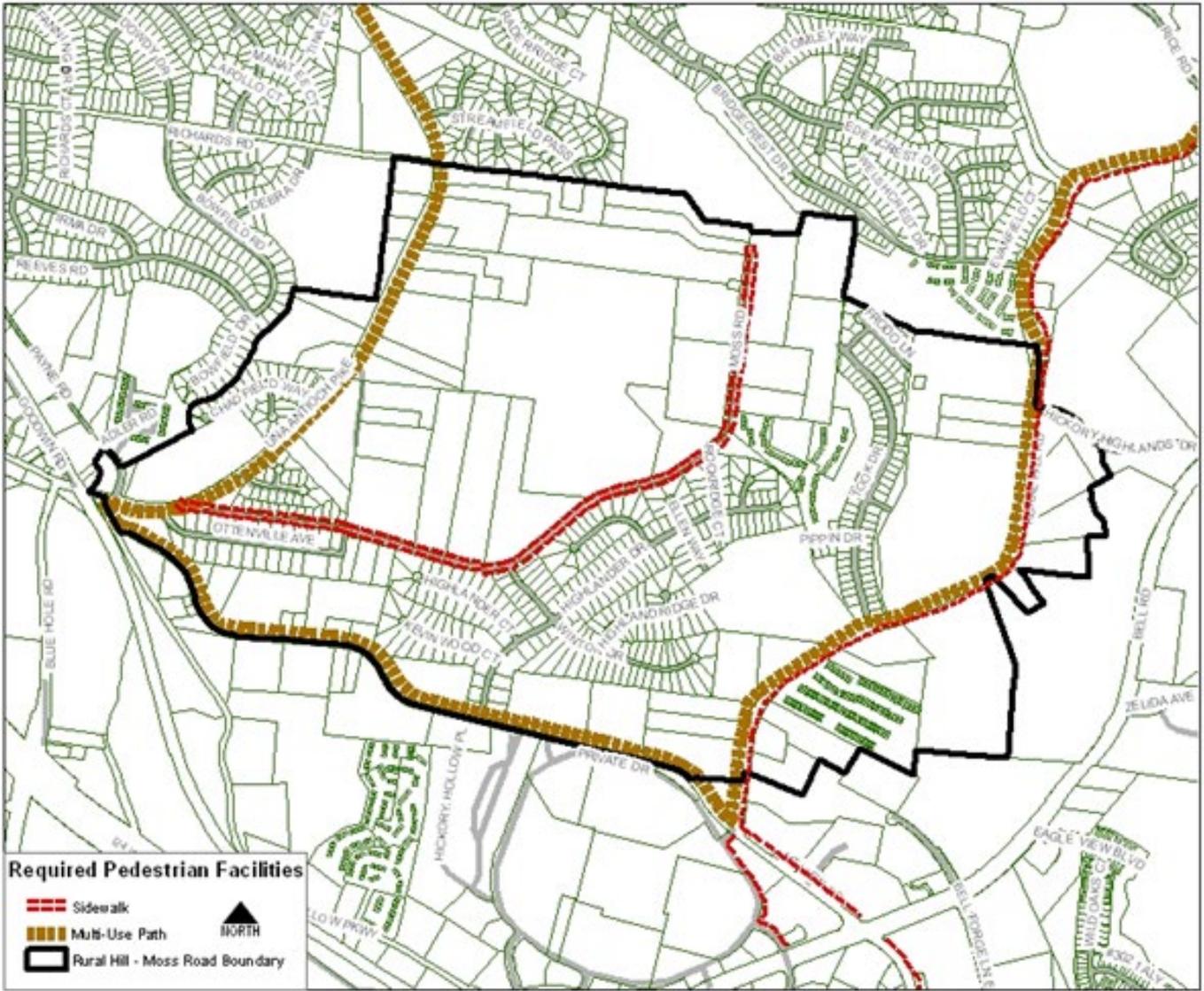


Figure 26: Required Pedestrian Facilities

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Specific Plan Zoning District Template

It is expected that all new zone changes within the Rural Hill-Moss Road Detailed Design Plan (DDP) will be submitted in the form of a Specific Plan or “SP” Zoning District. “SP” refers to a type of base zoning district, not an overlay, which is not subject to traditional zoning districts’ development standards. Instead, design standards are established *for the specific development in its specific context* and are written into the zone change ordinance, which becomes law. Elements that can vary from district bulk standards include height and size of buildings, setbacks, buffers, signage, and materials. The Rural Hill-Moss Road DDP has detailed the form that future development should take and these details are to be included as components in future SP rezoning requests. Developers who use SP zoning must still follow subdivision and stormwater regulations, and the goals and objectives of the General Plan, which include the Rural Hill-Moss Road DDP. As a result, the SP zoning district is the best tool to implement the vision of the DDP.

Below are the elements that, at a minimum, are to be included in any SP zoning application (please refer to the current SP application <http://www.nashville.gov/mpc/appsfees/index.asp> for all current submittal requirements):

- A development plan,
- A description of existing conditions on the site,
- An explanation of the purpose and intent of the proposed SP District,
- An explanation of the proposed plan’s consistency with the goals/objectives of the Rural Hill-Moss Road DDP,
- Density/intensity of development and land uses,
- Bulk standards per the SP templates,
- Any site specific development standards,
- Landscape plans,
- Light Imprint Stormwater Concepts,
- Greenway or Open Space designations and dedications per the DDP,
- IDA requirements, road and sidewalk improvements, and
- A development phasing and construction schedule.

Subdistricts and Bulk Standards Templates

The Rural Hill-Moss Road DDP was designed with a set of building types with specific design characteristics intended to implement the overall community vision. The different subdistricts call for different types and sizes of buildings. Refer to Figure 26: SP Template Map to determine the particular subdistrict each property is located in and then look up that “bulk standards” (basic design standards) for each subdistrict.

This plan promotes growth that is both coordinated and compatible. To that end, the bulk standards templates are designed to guide the design of individual future development projects and to promote a compatible and similar built environment throughout the DDP area. For each building type listed below there is a corresponding bulk standards template on the following pages. These standards are recommended to guide design and may be varied to suit an individual development request as long as the overall proposal complies with the intent of the DDP.

Appropriate Building Types by Subdistrict

Subdistrict 1

House

Subdistrict 2

Alley House
House
Cottage Court

Townhouse
Townhouse Courts
Manor House
Stacked Flats
Courtyard Flats

Subdistrict 3

Alley House
Cottage Courts

Subdistrict 5
Mixed Use/Commercial
Live/Work
Stacked Flats
Courtyard Flats

Subdistrict 4

Alley House
Cottage Courts

Building Materials

Building walls shall be finished in brick, stone, wood siding, shingles, fiber cement siding/shingles, or stucco. The use of highly durable and sustainable products and materials is encouraged.

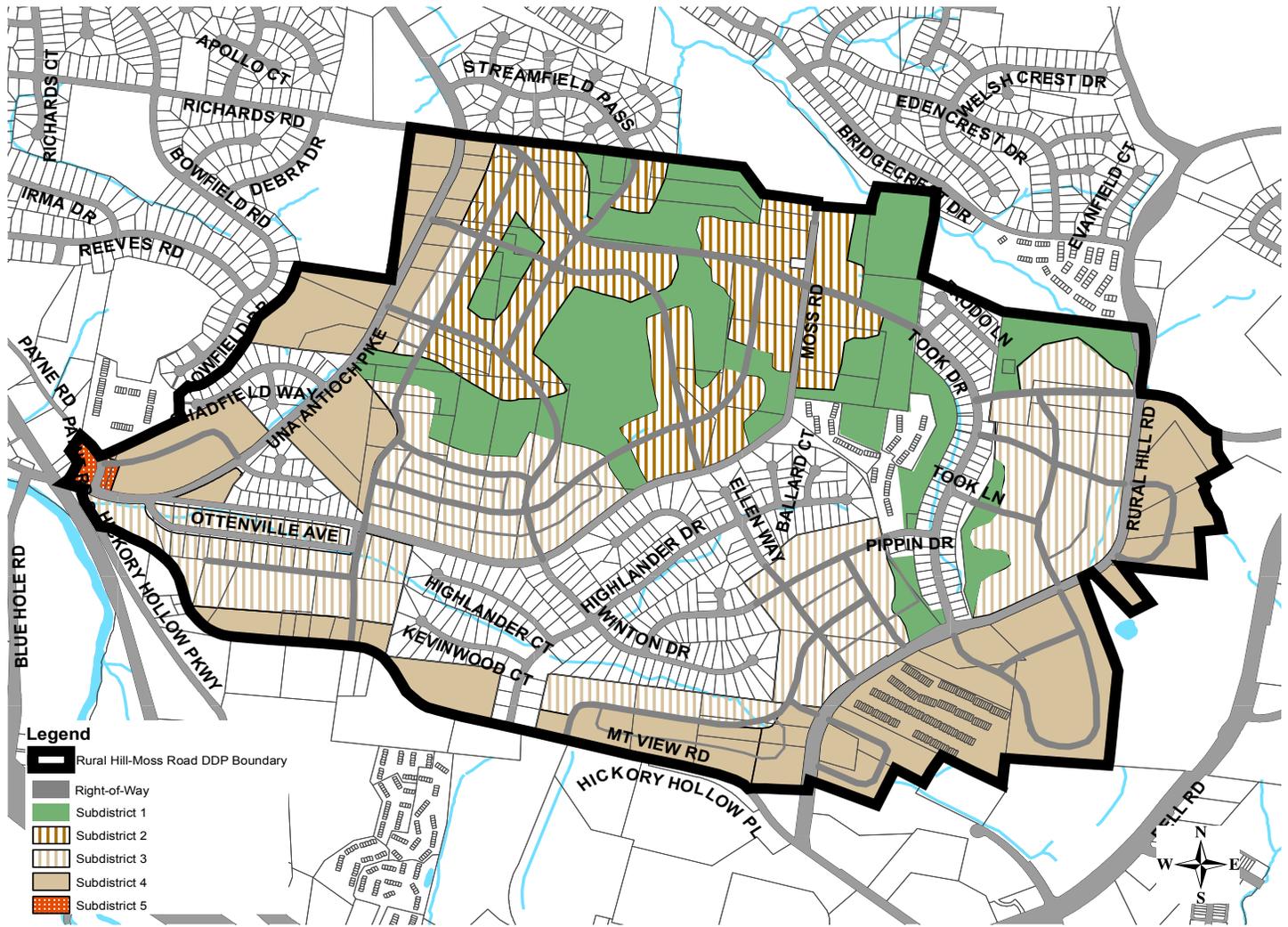


Figure 27: SP Template Map

Subdistrict 1

Bulk Standards: House

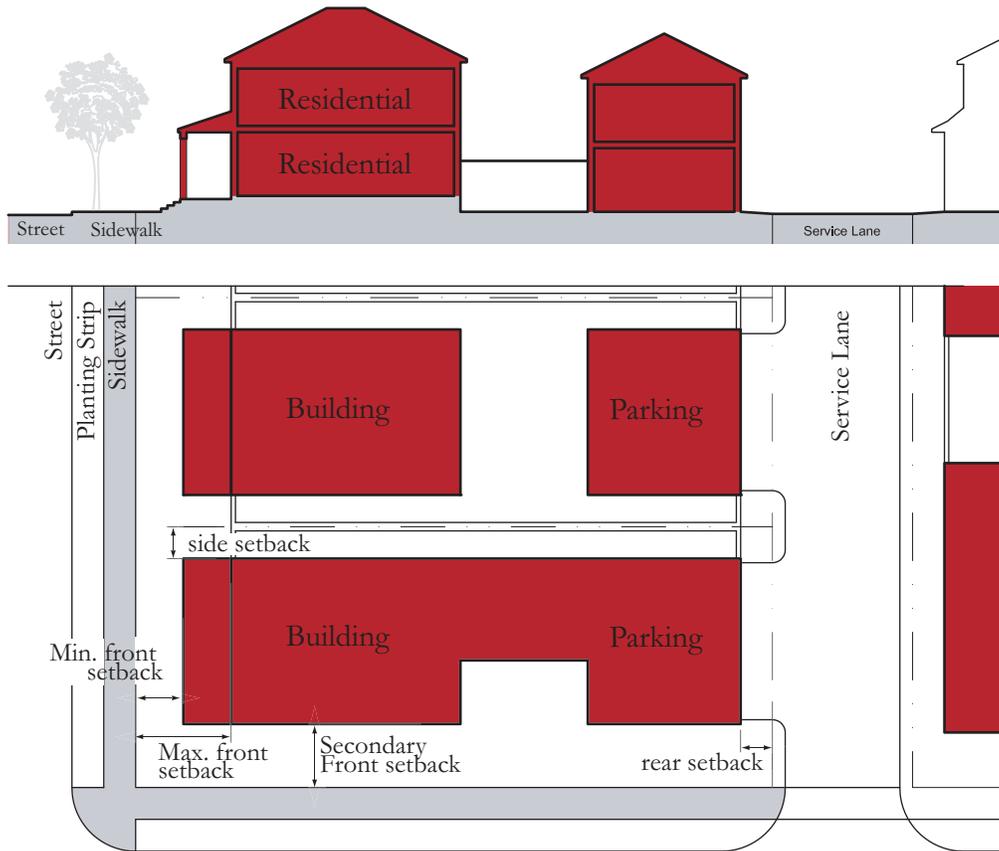
BULK PROVISIONS - Refer to the AR2a District in the Zoning Ordinance

PARKING, LOADING & ACCESS - Prefer to the AR2a District in the Zoning Ordinance

ADDITIONAL STANDARDS -

Raised Foundation: 18” minimum, 48” maximum, as measured from at highest point along front property line.

Bulk Standards: Alley House



BULK PROVISIONS

- Min. Lot Area: 7,500 sq. ft.
- Min. Lot Width: 50'
- Setbacks:
 - Front: 15' min. / 20' max.
 - Side: 10 ft. min on corner, 5 ft. min. on interior.
 - Rear: 5' min. or > 15'
- Additional Setback Encroachments:
 - Covered Porches: 8'
 - Covered Stoops & Balconies: 6'
 - Bay windows: 2'
- Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

- Parking Required: According to UZO
- Parking Access: Side street or service lane.
- Parking Location: Behind, beside, or beneath building.

ADDITIONAL STANDARDS

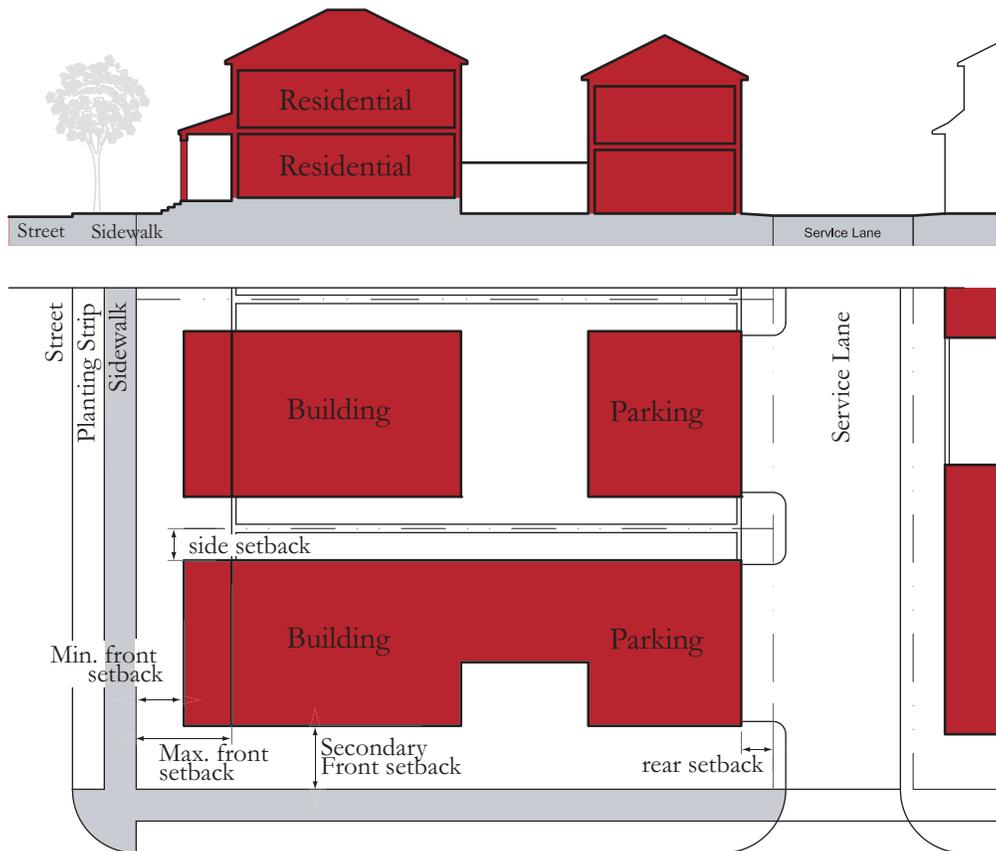
- Raised Foundation: 18" minimum, 48" maximum, as measured from highest point along front property line.



Sitophocus

Subdistrict 3

Bulk Standards: Alley House



BULK PROVISIONS

Min. Lot Area: 7,500 sq. ft.

Min. Lot Width: 40'

Setbacks:

Front: 15' min. / 20' max.

Side: 10 ft. min on corner, 5 ft. min. on interior.

Rear: 5' min. or > 15'

Additional Setback Encroachments:

Covered Porches: 8'

Covered Stoops & Balconies: 6'

Bay windows: 2'

Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Side street or service lane.

Parking Location: Behind, beside, or beneath building.

ADDITIONAL STANDARDS

-Raised Foundation: 18" minimum, 48" maximum, as measured from highest point along front property line.

Bulk Standards: Cottage Court



BULK PROVISIONS

Min. Area: 10,000 sq. ft.

Setbacks:

Front: For buildings that front the street: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all others.

For buildings that front central open space: 5' min. / 10' max. from property line adjacent to open space or edge of walk.

Side: 5' min.

Rear: 10' min. from back of building to side property line abutting neighboring lot; 20' min. from back of building to rear property line abutting neighboring lot; 5' min. or > 15' for garage.

Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Side street or service lane.

Parking Location: Behind, beside, or beneath building.

ADDITIONAL STANDARDS

- Raised Foundation: 18" minimum as measured from highest point along front property line.

- Buildings shall abut both sides of the central open space.

- Parking, driveways, and detention areas shall not be located within the central open space.

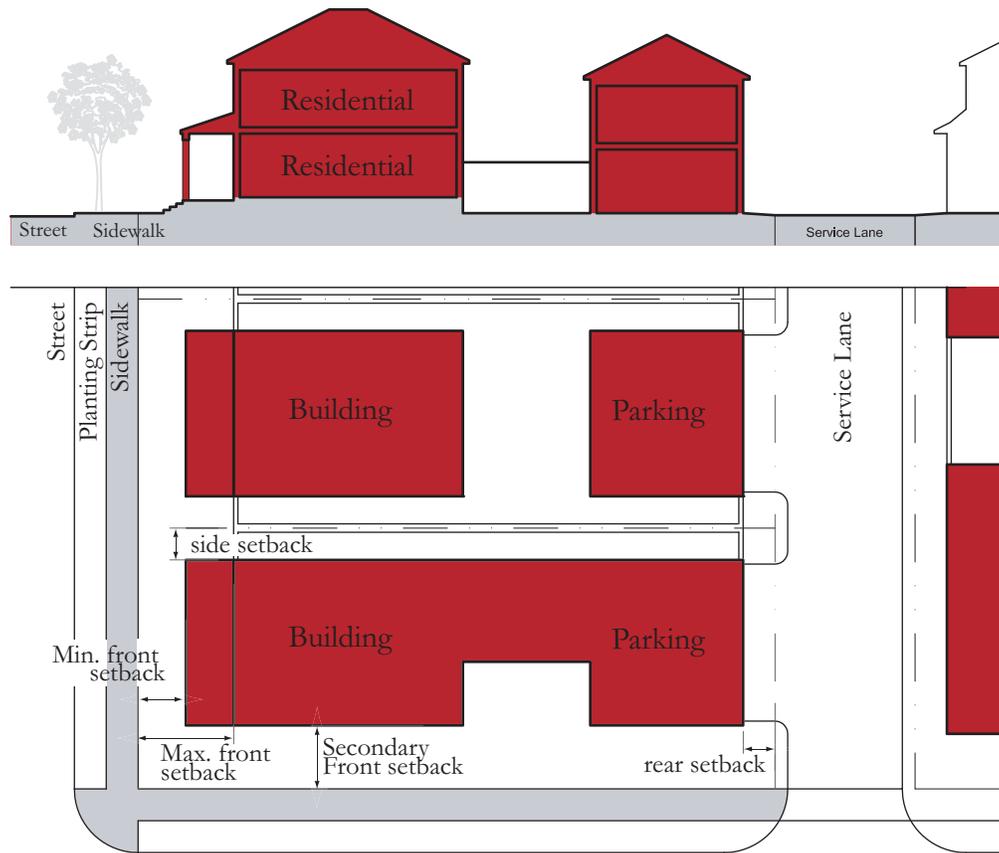


- Parking adjacent to the central open space shall be separated from it by landscaping or an architectural screen.

- The width of the central open space, measured between buildings, shall be no less than the height of the buildings, measure to the bottom of the eave or top of parapet.

Bulk Standards: Alley House

Subdistrict 4



BULK PROVISIONS

Min. Lot Area: 3,750 sq. ft.

Min. Lot Width: 35'

Setbacks:

Front: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all others.

Side: 10 ft. min on corner, 5 ft. min. on interior.

Rear: 5' min. or > 15'

Additional Setback Encroachments:

Covered Porches: 8'

Covered Stoops & Balconies: 6'

Bay windows: 2'

Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Side street or service lane.

Parking Location: Behind, beside, or beneath building.

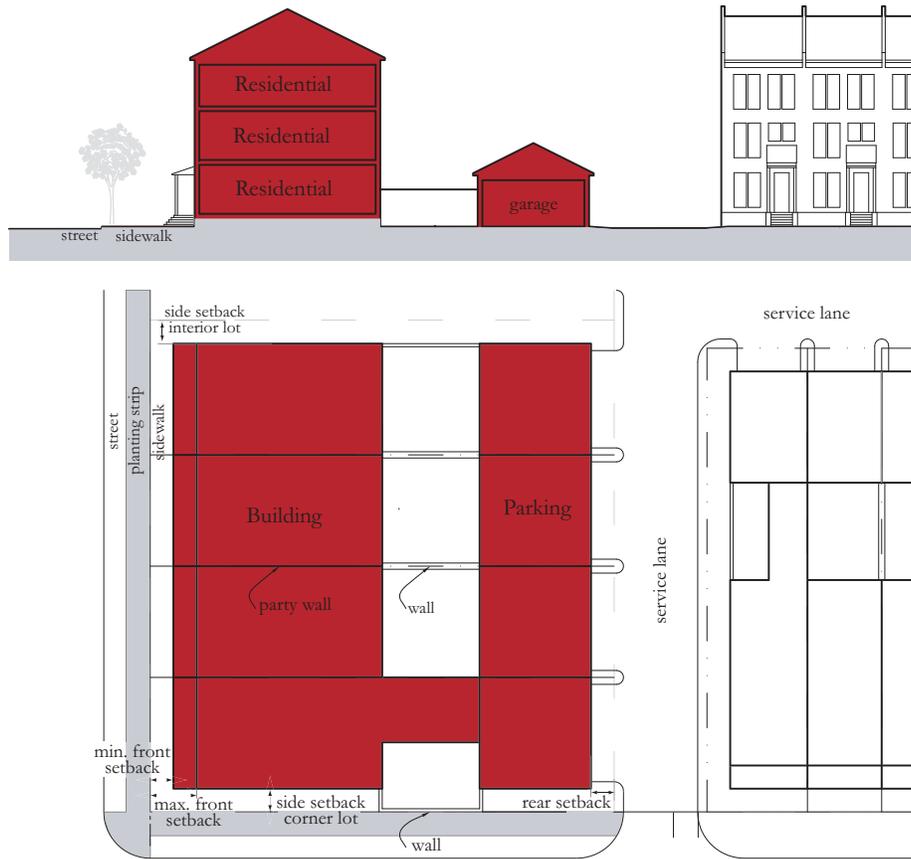
ADDITIONAL STANDARDS

-Raised Foundation: 18" minimum, 48" maximum, as measured from highest point along front property line.

Secondary Dwelling-

-There shall be no more than one secondary dwelling per lot.





BULK PROVISIONS

- Min. Lot Area: 1,080 sq.ft.
- Setbacks:
 - Front: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all others.
 - Side: 5' min. on all corner lots, 0' min. on interior lots.
 - Rear: 20' min.; 5' min. or > 15' for garage.
- Additional Setback Encroachments:
 - Covered Porches: 8'
 - Stoops & Balconies: 6'
 - Bay windows: 2'
 - Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

- Parking Required: According to UZO
- Parking Access: Side street or service lane.
- Parking Location: Behind, beside, or beneath building.

ADDITIONAL STANDARDS

- Raised Foundation: 18" minimum as measured from highest point along front property line
- There shall be no more than one secondary dwelling per lot

Bulk Standards: Town House Courts

Subdistrict 4



BULK PROVISIONS

Min. Lot Area: 10,000 sq. ft.

Setbacks:

Front:

For buildings that front the street: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all other streets.

For buildings that front central open space: 5' min. / 10' max. from property line adjacent to open space or edge of walk.

Side: 5' min.

Rear: 10' min. from back of building to side property line abutting neighboring lot; 20' min. from back of building to rear property line abutting neighboring lot; 5' min. or > 15' for garage.

Height: 3 Stories max. at highest point along front property line

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Side street or service lane.

Parking Location: Behind, beside, or beneath building.

ADDITIONAL STANDARDS

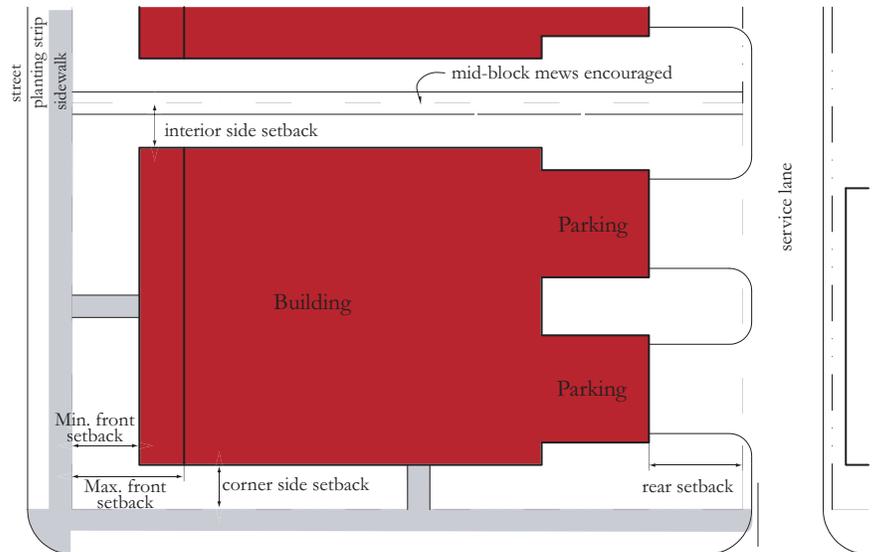
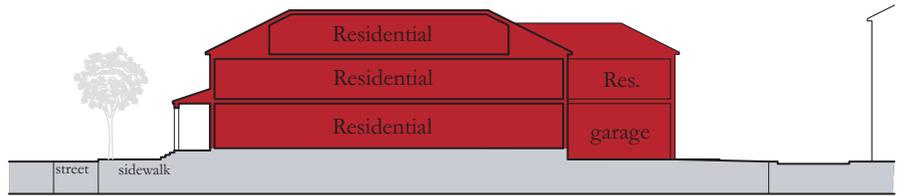
- Raised Foundation: 18" minimum as measured from highest point along front property line

- Buildings shall abut both sides of the central open space.

- Parking, driveways, and detention areas shall not be located within the central open space.

- Parking adjacent to the central open space shall be separated from it by landscaping or an architectural screen.

- The width of the central open space, measured between buildings, shall be no less than the height of the buildings, measure to the bottom of the eave or top of parapet.



BULK PROVISIONS - Limit of 6 units per building

Min. Lot Area: 7,200 sq. ft.

Min. Lot Width: 60'

Setbacks:

Front: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all other streets.

Side: 5' min.

Rear: 20' min.; 5' min. or > 15' for garage

Additional Setback Encroachments:

Covered Porches: 8'

Covered Stoops & Balconies: 6'

Bay windows: 2'

Height: 2 and 1/2 Stories, as measured from the highest point along front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Side street or service lane.

Parking Location: Behind, beside, or beneath building.

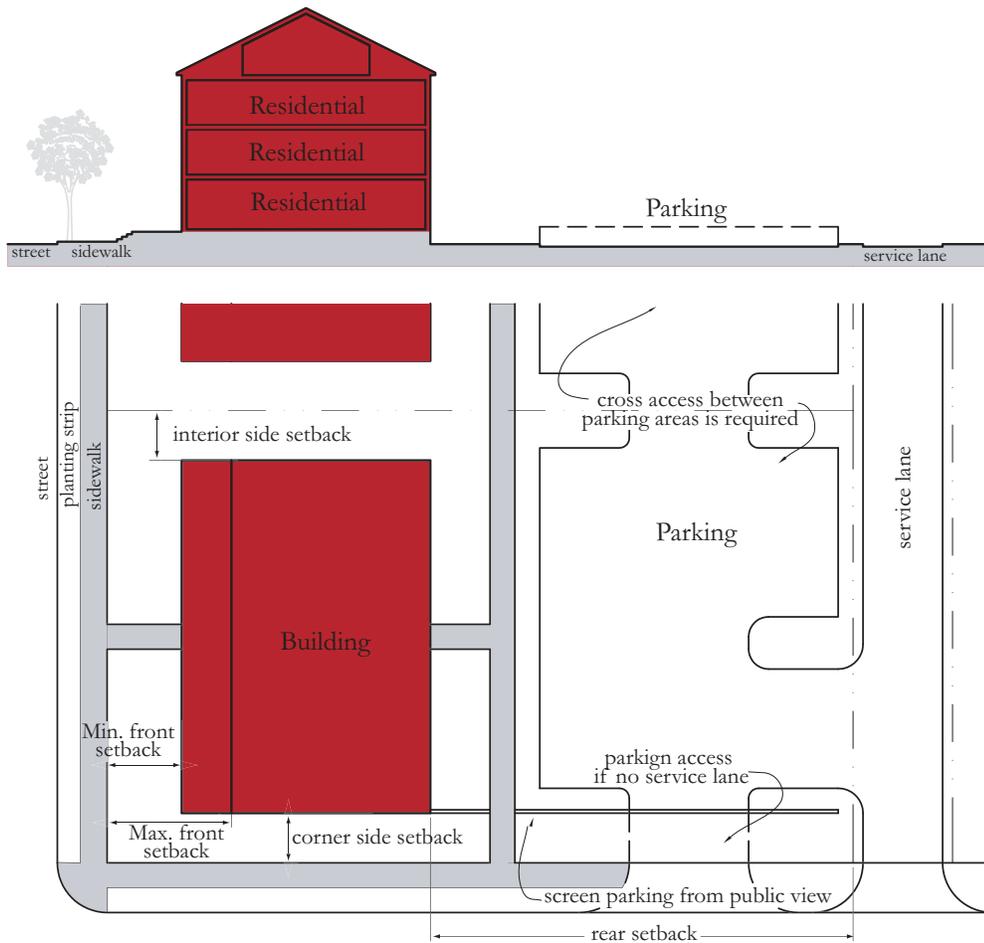
ADDITIONAL STANDARDS

-Raised Foundation: 18" minimum as measured from highest point along front property line.

-There shall be no more than one secondary dwelling per lot.

Bulk Standards: Stacked Flat

Subdistrict 4 & 5



BULK PROVISIONS - Building shall be a maximum of 15,000 square feet

Min. Lot Area: N/A

Setbacks:

Front: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all others.

Side: 5' min.

Rear: 20' min.

Additional Setback Encroachments:

Covered Stoops & Balconies: 6'

Bay windows: 2'

Height: 3 Stories, as measured from the highest point along front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Street, side street or service lane; Cross access between parking areas is required.

Parking Location: Behind, beside, or beneath building.

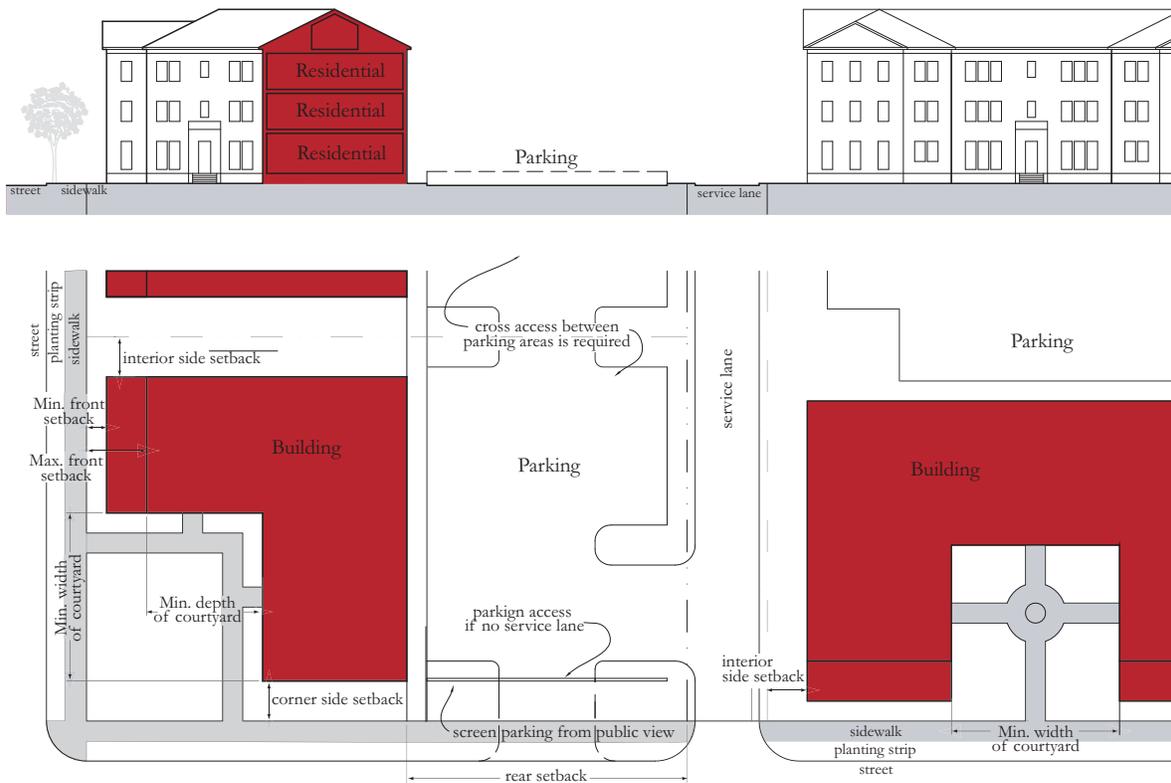
Loading: Behind or beside building

ADDITIONAL STANDARDS

Raised Foundation: 18" minimum as measured from highest point along front property line.

Bulk Standards: Courtyard Flat

Subdistrict 4 & 5



BULK PROVISIONS- Building shall be a maximum of 15,000 square feet

Min. Lot Area: N/A

Setbacks:

Front: 30' min. / 40' max. along Rural Hill Road, Mt. View Road and Una Antioch Pike; 15' min. / 20' max. along all others.

Side: 5' min.

Rear: 20' min.

Additional Setback Encroachments:

Covered Stoops & Balconies: 6'

Bay windows: 2'

Height: 3 Stories, as measured from the highest point along front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Street, side street or service lane; Cross access between parking areas is required.

Parking Location: Behind, beside, or beneath building.

Loading: Behind or beside building

ADDITIONAL STANDARDS

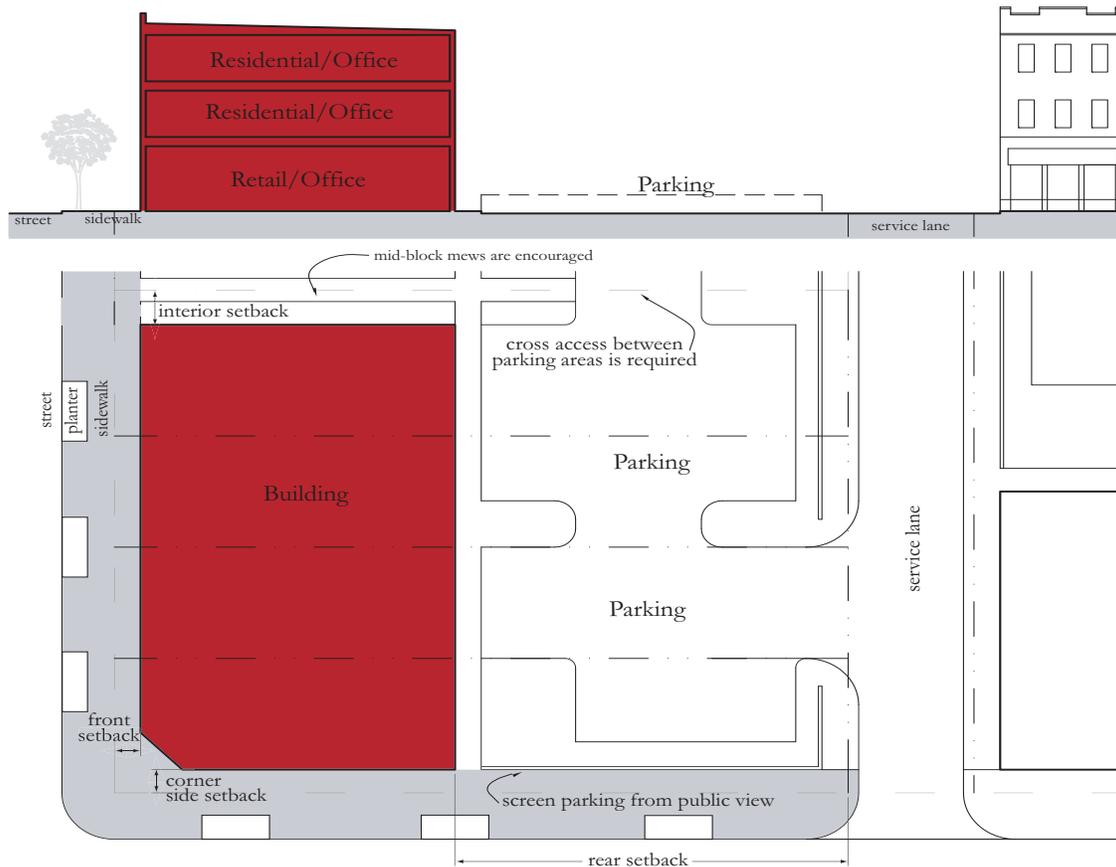
-Raised Foundation: 18" minimum as measured from highest point along front property line.

-Parking, driveways, and detention areas shall not be located within the central open space.

-The width of the central open space, measured between buildings, shall be no less than the height of the buildings, measure to the bottom of the eave or top of parapet.

Bulk Standards: Mixed Use/Commercial Building

Subdistrict 5



BULK PROVISIONS

Min. Lot Area: N/A

Setbacks:

Front: 15' min. / 20' max. along Una Antioch Pike; 50' min. / 60' max. if parking between Una Antioch Pike and the building; 0' min. / 10' max. along internal streets. Setback measured from ultimate right-of-way.

Side: 5' min.

Rear: 20' min.

Maximum Height: 3 Stories, as measured from the highest point along front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

Minimum Height: 14 feet, as measured from the highest point along the front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

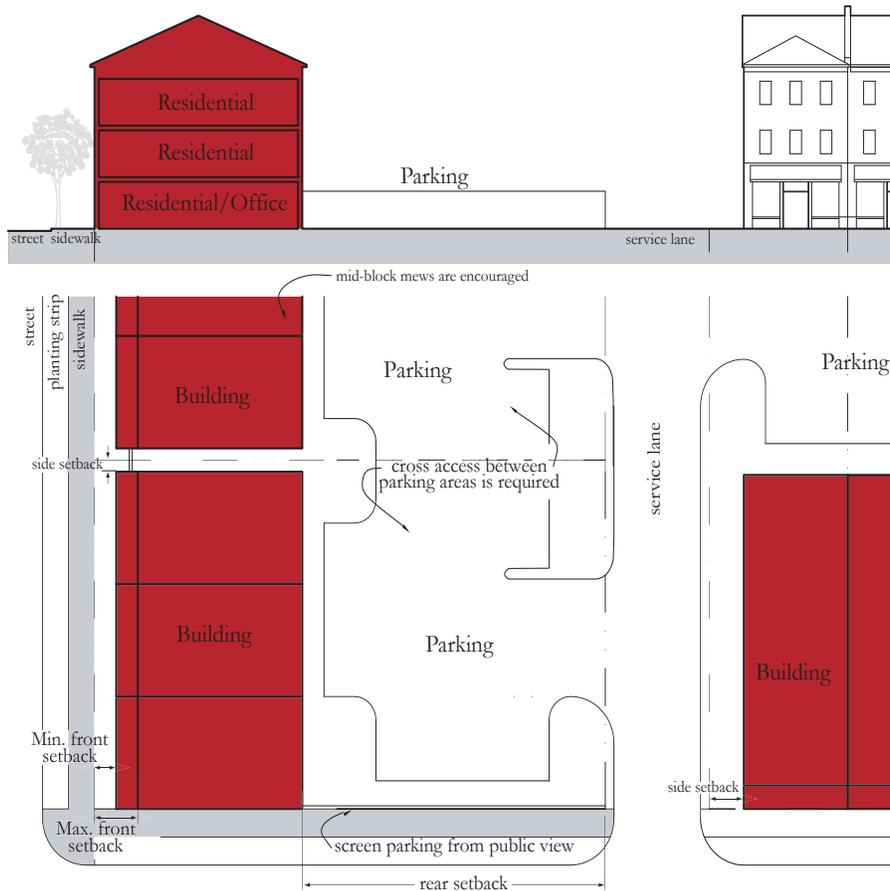
PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Street, side street or service lane; Cross access between parking areas is required.

Parking Location: One double-loaded aisle of parking shall be allowed between Bell Road and building; all other parking shall be behind, beside, or beneath building.

Loading: Behind or beside building



BULK PROVISIONS

Min. Lot Area: N/A

Setbacks:

Front: 15' min. / 20' max. along Una Antioch Pike; 0' min. / 10' max. along internal streets. Setback measured from ultimate right-of-way.

Side: 5' min.

Rear: 20' min.; 5' min. or > 15' for garage

Maximum Height: 3 Stories, as measured from the highest point along front setback to the bottom of the eave of a sloped roof and the top of the parapet of a flat roof.

PARKING, LOADING & ACCESS

Parking Required: According to UZO

Parking Access: Street, side street or service lane; Cross access between parking areas is required.

Parking Location: Parking shall be behind, beside, or beneath building.

Loading: Behind or beside building.

Appendix A

VISIONING WORKSHOP NOTES MONDAY, OCTOBER 20th 2008

Likes

Open Space / Rural

- Rural character (2)
- Mature trees (2)

Neighborhoods

- Affordable houses – good range of housing prices (2)
- Diversity of people (2)
- Quiet (2)
- Diversity of housing
- Know neighbors
- Kid Friendly (in Berkeley Hall)
- Familial

Transportation

- Access/Convenience – to highway, work, business (3)

Character

- Old Community pride [long time residents]
-

Dislikes

Community Facilities and Services

- Overhead utilities
- Infrastructure inadequate
- Infrastructure is not keeping pace with development – streets and septic system issues, lack of sidewalks
- Need for social services and city services, parks, Health Department
- Code enforcement

Neighborhoods

- Apartments (2) – there are just way too many, it is overloading the schools. They are bad clientele and are the reason the mall is collapsing.
- There is no longevity with apartments – it doesn't help make a community

- Newer communities need to be integrated more with older communities/residents
- Lack of walkability
- Issues with old Planned Unit Developments (PUDs) – homes built cheaply and fast (2)
- Overcrowded housing
- Yard parking

Centers

- Business/restaurants are lacking [limited choices]

Corridors

- Messiness of Bell Road – businesses moving out, properties and medians not being maintained

Transportation

- Congestion [traffic] @ Blue Hole & Hickory Hollow Parkway
- Lack of walkability
- Need sidewalks (2) [Rural Hill Road, Una Antioch Pike]
- Roads are dangerous
- Need for alternate transportation

Other

- The crime rate is up - Tagging
 - Perception of Antioch (2)
 - Noise from incompatible uses
 - Flooding during rain events at the Una Antioch Pike, Mt. View Road, Hickory Hollow Parkway, railroad intersection
 - Dumping of trash in the area
-

Preserve

Open Space / Rural

- Country atmosphere/rural character (5)
- Green space, trees (2)
- Views from hill – preserve view sheds
- Natural habitat
- Open Space

Neighborhoods

- Single family homes
- Character [older homes]

Centers

- Convenience – concern as mall dies, area may not be convenient without close areas to shop

Other

Enhance

Appendix B

Community Issues Survey Results

Rural Hill – Moss Road Kick-Off Meeting

October 8th, 2008

Number of Surveys Completed: 5

What are 3 things you **really like** about your community?

1. Convenience to interstate; Convenience; Diverse; Good mixture of housing but future development needs to be more consistent with character; affordable housing
2. Quiet
3. Family-oriented

What are 3 things you **do not like** about your community?

1. Lack of community; Area turning into rental housing; Not enough parks, bikeways, bicycle lanes, and open greenspace; Safety.
2. Too many apartments; Crime; Lack of a Regional recreational Community center; Schools.
3. Crime; Lack of community; Not enough walk-up small businesses with outdoor tables and patios, lacks natural character – greenspace/ walking/biking; Neighborhood.

What street do you live on? Highlander Court (2); Rice Road

Any additional comments: Would love to see same Transect as the new Hill Center around Hickory Hollow Mall area. This could help move the area towards revitalized development around the mall. Could the library have a neighborhood park attached to it? It would promote more families utilizing the library.

Appendix C

Rural Hill-Moss Road IDA Calculations

Development Potential Under the Plan Based on the SP Zoning Template

Category	Acres	% of Acres	Units	% of New Units	Units Per Acre
Potential Open Space	92	32.08	46	3.08	0.50
RM Areas*	110.3	38.46	660	44.18	5.98
RS75 Areas	33.3	11.61	194	12.99	5.83
RS10 Areas	141.28	49.26	583	39.02	4.13
MUL Area	1.9	0.66	11	0.74	0.00
Totals	286.78	100.00	1494	100.00	5.21

*Based on an average expected density of 6 dwelling units per acre.

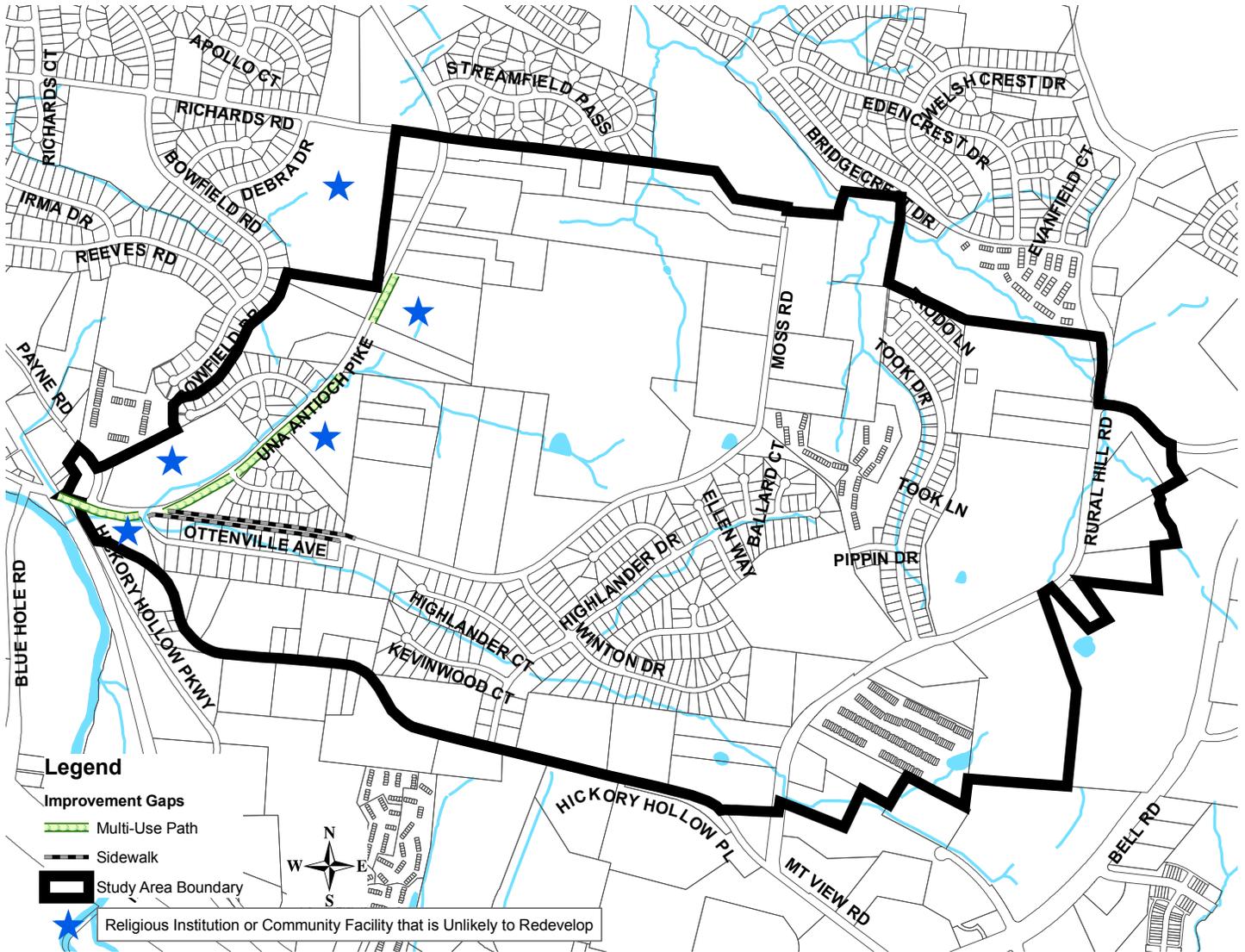
Cost Estimates

In-lieu sidewalk fee per foot	91
Feet of Sidewalk Needed	2,880
Gap cost for sidewalks	262,080

Multi-Use Path Cost per foot	170
Feet of Paths Needed	2,670
Multi-Use Path Cost per foot	453,900

Sidewalks and Paths	715,980
Per Dwelling Unit Contribution	\$479

Appendix D: Expected Infrastructure Improvement Gaps



CREDITS

Metropolitan Planning Commission Commissioners

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Mr. Phil Ponder, Vice-Chairman
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Planning Department

Executive Office / Administration
Rick Bernhardt, Executive Director

Planning

Ann Hammond, Assistant Executive Director / Planning
Jennifer Carlat, Planning Manager II, Community Plans
Hilary Kahnle, Planning Manager I, Design Studio
Bob Leeman, Planning Manager II, Land Development and Design

Operations

Metropolitan Planning Organization / Transportation
Michael Skipper, MPO Director

The production of this plan was primarily the responsibility of the Community Plans and Design Studio Divisions. The Rural Hill-Moss Road Detailed Design Plan team included:

Cindy Wood, Planner III, Community Plans
Kathryn Withers, Planner III, Design Studio
Rebecca Ratz, Planner I, Design Studio
Scott Adams, Planner I, Community Plans
Toks Omishakin, Planner II, Community Plans
Craig Owensby, Public Information Officer

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Metropolitan Planning Department
Metro Office Building
800 Second Avenue South
Nashville, TN 37201

615.862.7150