



STARWOOD TOWN CENTER

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Introduction

This site is located in Nashville, Davidson County, Tennessee, with great access to a major collector, Murfreesboro Pike, and only 1.6 miles from Interstate 24.

The site was rezoned in 2008. Since that time, it has been called Starwood Commons. With this amendment, we are changing it to Starwood Town Center.

Site Info

- 65.1 Acres
- 3839 Murfreesboro Pike
- Nashville, Tennessee
- Council District 33 - Sam Coleman
- Urban Services District
- Existing Zoning: SP (See Ordinances BL2008-137 and BL2010-805)
- According to FEMA Flood Insurance Rate Maps (FIRM) 47037C0386F, this property does not contain any floodplains or floodways.

Owner/Developer

- Ron Buck, Inc.
- Ronald D. Buck Investments, Inc.
- 5200 Old Harding Road
- Franklin, TN 37206
- Phone: (615) 799-0933

Planner/Civil/Landscape Architect

- Barge Design Solutions, Inc.
- 615 Third Avenue South
- Nashville, TN 37210
- Phone: (615) 254-1500



Project Background

For more than 20 years, the 65.1-acre site at the corner of Hobson Pike and Murfreesboro Road in Antioch was the home of Starwood Amphitheater, the city's premier outdoor live entertainment venue. Internationally known rock, blues, gospel, and other acts made the site a mecca for live musical performances until owner Live Nation sold it in 2007. Vastland Development purchased the property in 2007 and went through the SP rezoning process in 2008. In 2017, development company Ronald D. Buck Investments, Inc., purchased the property and has undertaken the process of reimagining the potential for Starwood Town Center in the current market.

Purpose of the Amendment Application

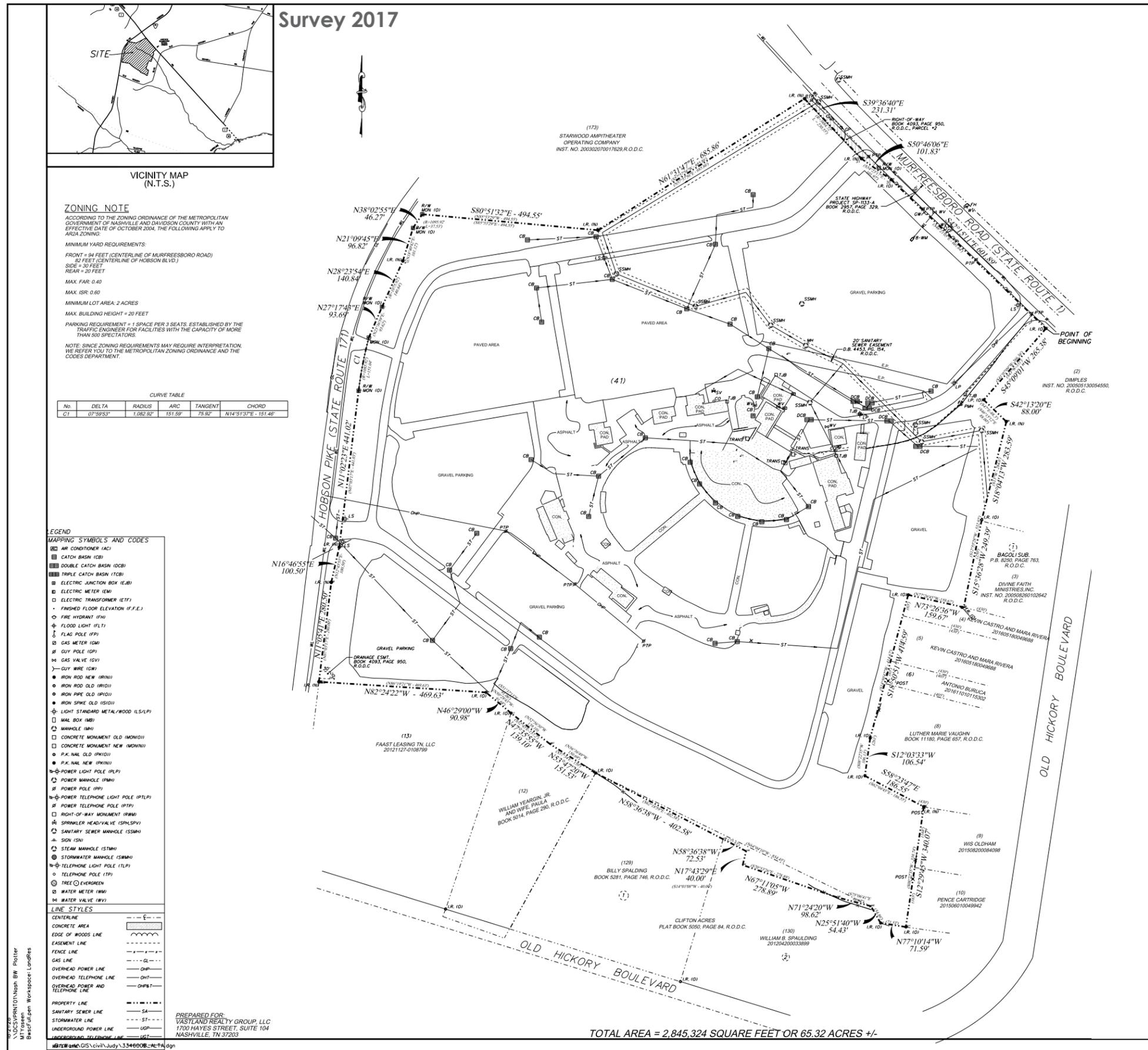
In 2008 when the original plan was approved, a condition was added to BL2008-137, which required at least 80% of the commercial site improvements be constructed before any residential permits could be issued. This condition has caused the development to be unfeasible since rooftops are needed to support commercial businesses. This amendment seeks to remove that condition as well as revise the plan layout and mix of uses. Additionally, the market has changed and the plan has been revised to reflect the current needs of the community and the residential types needed to support the commercial area. The proposed plan also includes a large open space.

Metro and Community Meeting

A pre-application meeting was held with Metro Planning staff on March 14, 2017, with a follow-up on May 23, 2017. In addition, several other meetings and correspondences have occurred with Metro Planning staff, Public Works and the District's Metro Council Members related to this amendment.

On May 11, 2017, a community meeting was held in cooperation with then Council Member Sam Coleman and his constituents on at 6 PM at Mt. View Elementary School, 3820 Murfreesboro Pike. Comments from the community included a desire for more restaurants and fewer residential units. Following the meeting, Sam Coleman was appointed to a judgeship and the project was put on hold until a new Council Member was elected.

A second community meeting was held on November 13, 2017, at 6 PM at Mt. View Elementary School. This meeting was held following the election of Council Member Antoinette Lee. At the meeting, the design team presented changes to the plan, which included more restaurant space and fewer residential units. Meeting minutes and a summary of the comment cards are available upon request.



Community Character Policy

The Starwood Town Center site is in the Antioch-Priest Lake Community Plan, adopted June 22, 2015. The site is classified as Suburban Community Center (T3-CC) Policy of the Community Character Manual (CCM). The Suburban Community Center Policy envisions centers that are pedestrian-friendly areas, generally located at prominent intersections that contain mixed-use, commercial, and institutional land uses, with residential land uses in mixed-use buildings or serving as a transition to adjoining Community Character Policies.

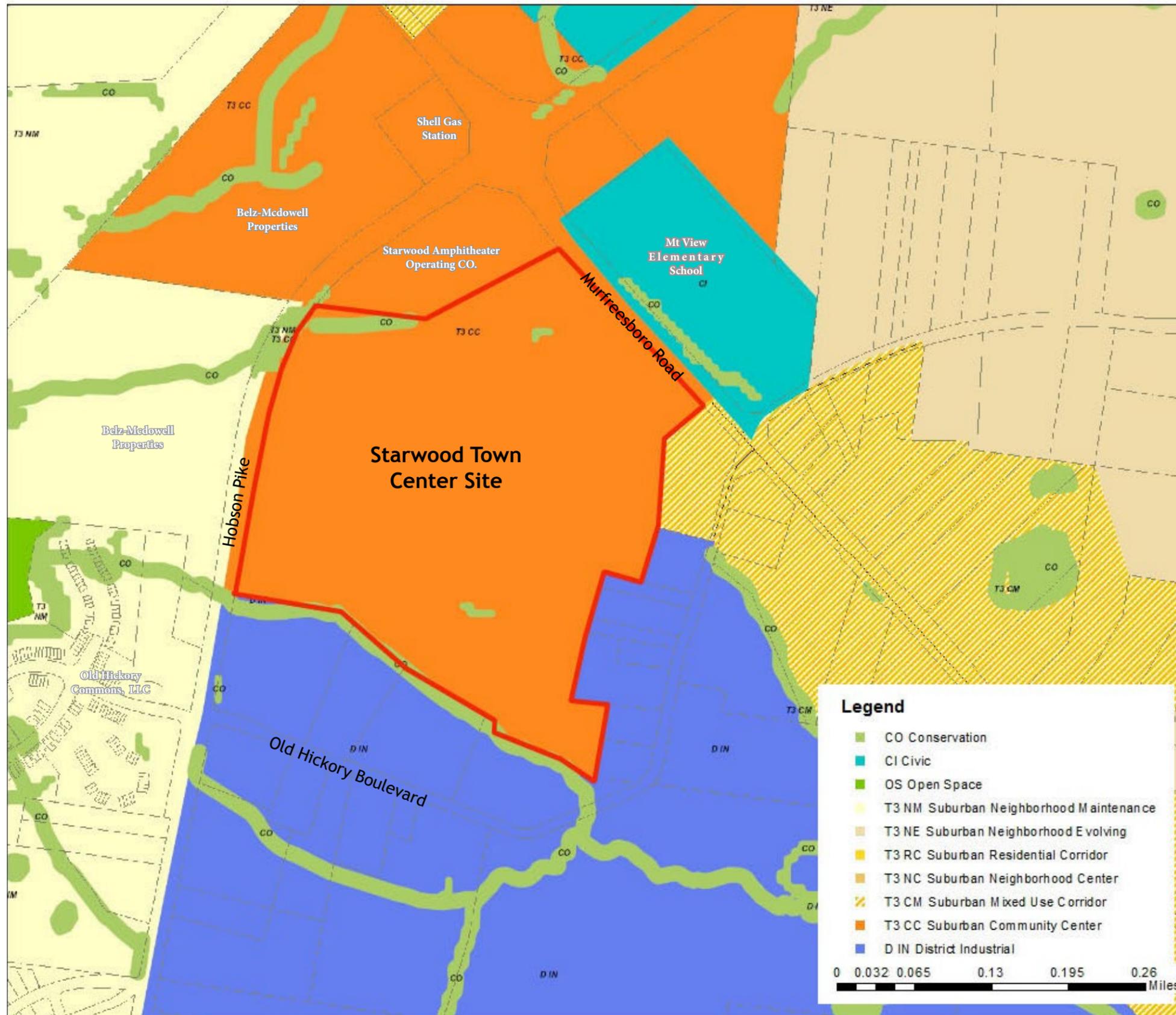
Suburban Community Center Characteristics:

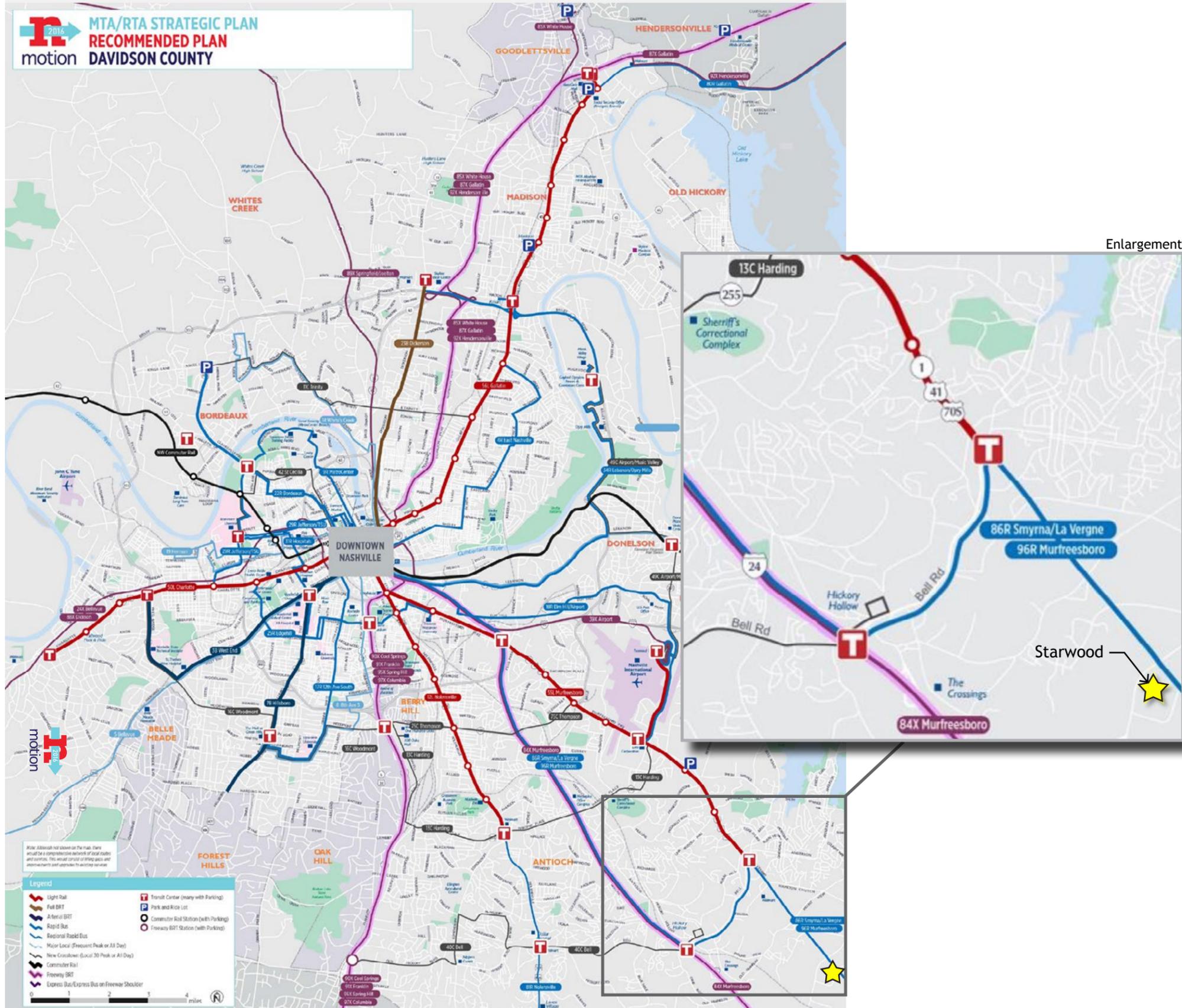
- Nonresidential buildings are regularly spaced and generally built to the back edge of the sidewalk.
- Parking is generally behind or beside the building or on-street.
- The public realm and streetscape feature consistent use of landscaping and lighting.
- Setbacks may be deeper to accommodate two rows of parking in front of buildings and the placement of out parcel development as associated with moderately, and in exceptional cases, large-scaled buildings.
- Centers are served by highly connected street networks and sidewalks and existing or planned transit leading to surrounding neighborhoods and open space.
- The edges of the centers are firm with residential transitions between the center and less intense residential areas.

The enclosed SP amendment application provide a plan that meets a vision set out by the community for a mixed-use, walkable development and is consistent with creating the built environment envisioned in the T3 Suburban Center Policy.

Relationship to the Murfreesboro Pike Urban Design Overlay

This site is located within the boundary of the Murfreesboro Pike Urban Design Overlay (UDO); however, the UDO does not apply to the subject property. Page 7 of the Murfreesboro Pike UDO states, "As long as the SP is in place, the SP will be the controlling zoning; however, if the SP is removed, then the UDO will apply." This SP is not being removed, only amended. Several standards of the UDO have been incorporated into this SP amendment to ensure that it is consistent with the design intent of the UDO.





As part of the 2016 approval of the nMotion Strategic Plan, Murfreesboro Pike was identified as one of the top five High Capacity Corridors (HCT). Below is a description of vision for the corridor.

The HCT network will deliver faster, reliable transit service between centers. The routes will intersect to provide a network of frequent and attractive services to very high numbers of residents, workers, and visitors. HCT projects will be an important element of revitalizing Nashville's pikes.

The network will:

- Provide enhanced service.
- Be frequent. With service operating every 10 minutes throughout most of the day, riders will be able to use HCT services without a schedule.
- Provide faster service. Depending on the corridor, travel times will decrease by 10% to 30%.
- Provide reliable service, with trains and buses operating as scheduled.
- Connect neighborhoods to downtown, to each other, and to jobs throughout the region.
- Be comfortable, pleasant, and easy to use. All aspects of the service will be easy for the rider to understand.
- Be safe and secure at stops and on the vehicle.
- Provide connections to other MTA and regional routes, and provide easy connectivity between buses and other modes of transportation.
- Support healthy lifestyles by providing options for car-less travel, encouraging walking and biking, reducing pollution, and increasing riders' connections to their communities.

Source: <http://nmotion2015.com/hctdplan/>

Given the potential of improved transit, along with the Community Character Policy supporting a transit-oriented development, this site has been reimagined as a walkable, mixed-use development. To become a viable transit-oriented development, the development would need to increase the number of units to meet the recommendations in the Community Character Policy, which is currently not supported by the community.

Funding for the transit improvements is contingent on several steps, including the approval of a referendum planned for May 1, 2018.

Major and Collector Street Plan

The site is bound by two streets, Murfreesboro Pike and Hobson Pike. The Metro Nashville Major and Collector Street Plan (MCSP) identifies the streets with the following designations. The street section will comply with the requirements set forth in the Metro Nashville Major Street and Collector Plan (MCSP).

Murfreesboro Pike: T3-M-AB5

- Standard right-of-way: 100.0 ft
- Half of standard right-of-way: 50.0 ft
- Adopted bikeways & sidewalks plan facility: buffered bike lane planned
- Median width: 0.0 ft
- On-street parking requirement: 0 side(s)
- Bikeway buffer: 2.0 ft
- Bikeway width: 6.0 ft
- Planting strip width: 6.0 ft
- Sidewalk width: 8.0 ft
- Frontage Width: 0.0 ft

This project will add a planting strip, sidewalk and frontage zone to complete the MCSP's requirements on Murfreesboro Pike.

Existing Murfreesboro Pike



Southbound View. Image source: Google Streetview

Future Murfreesboro Pike



Design Tool Source: <http://streetmix.net>

Hobson Pike: T3-M-AB5-S

- Standard right-of-way: 92.0 ft
- Curb-to-curb: 56.0 ft
- Travel lanes: 11.0 ft
- Curb and gutter: 2.5 ft
- Median width: 11.0 ft
- On-street parking requirement: 0 side(s)
- Planting strip width: 6.0 ft
- Multi-use Pathway: 12.0 ft south side

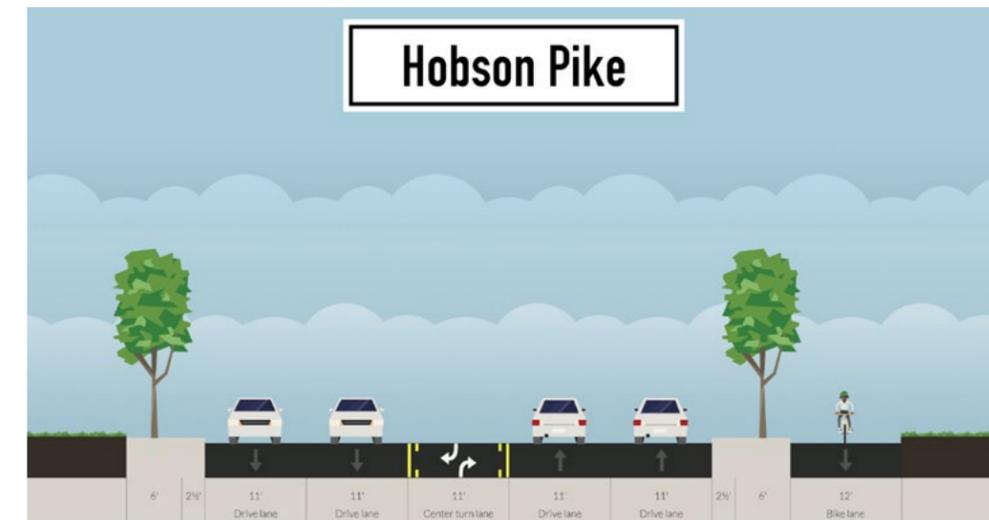
This information was last updated in 2017.

Existing Hobson Pike



Image Source: Google Streetview

Future Hobson Pike



Design Tool Source: <http://streetmix.net>



This regulatory plan lays out the general street network and boundaries for the subdistricts. The permitted land uses and bulk standards for the subdistricts are described on Development Standard sheets in this document. Final street layouts and building locations will be determined with the Final SP site plans. The Executive Director of the Metro Planning Department may modify standards within this SP insofar as the standards meet the general intent of the SP guidelines and comply with the Community Plan Policy. Modifications that change the permitted land uses and/or project density shall require the approval of Metro Council.

The plan creates a pedestrian-friendly environment within the site through the connected street and sidewalk network, the inclusion of streetscape improvements, landscaping, and open space.

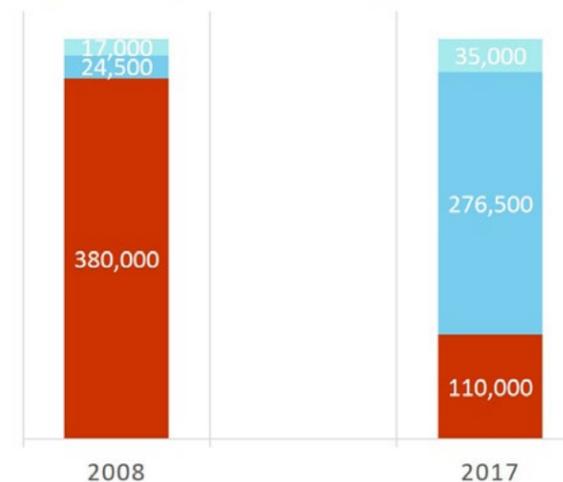
The central open space could be flanked by a public benefit use such as a senior center, community center, or church, in addition to mixed-use and residential development. The space itself would be a central gathering space for the community.

A summary of uses for the development compared with the 2008 approved SP zoning is provided in the charts below.

2008		2017		Change
Retail	380,000	Retail	110,000	-270,000
Office & Bank	24,500	Office & Bank	276,500	+252,000
Restaurants	17,000	Restaurants	35,000	+18,000
Commercial Total	421,500 sf	Commercial Total	421,500 sf	No change in sf
Townhomes	250	Townhomes (single-family attached and/or detached)	200	-50
		Senior Housing	150	+150
		Flats with Retail/Office	200	+200
Residential Total	250 units	Residential Total	550 units	+300 units

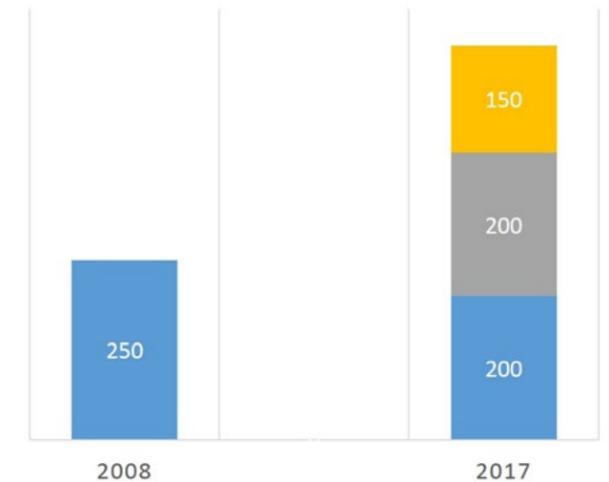
COMMERCIAL SQUARE FOOTAGE

■ Retail ■ Office & Bank ■ Restaurant



RESIDENTIAL UNITS

■ Townhomes ■ Mixed Use ■ Senior Housing



Note: Final SP design may vary from this illustrative plan. See Regulatory Plan and Bulk Standards for requirements.

(173)
STARWOOD AMPHITHEATER
OPERATING COMPANY
INST. NO. 20030201017628, R.O.D.C.



Legend

- Commercial/Office/Mixed-Use (200 units)
- Senior Housing (150 units)
- Single-family Attached (±182 units)
- Single-family Detached (±18 units)

Total Acreage: ±65 acres
 Total Residential: 550 units
 Commercial/Office: 421,500 sf
 Open Space: 15% or greater
 Note: Illustrative Concept Plan subject to change

The illustrative concept plan shows a possible development scenario that utilizes all of the guidelines in this document. Building footprints shown are for illustrative purposes only and are subject to change with final site plan submittals. Character images on the following pages provide examples of the desired urban form of the structures and materials.

Given the community's concern that this development be high quality, the following six items/methods will be included to support such a development.

1. **Architectural Standards:** Metro's Urban Design Overlay standards from the Murfreesboro Pike UDO were incorporated into the SP to address items such as material requirements prohibiting vinyl, E.I.F.S., and other low-quality materials on the ground floor; parking lots to be behind the building or screened; enhanced landscaping requirements; and additional restrictions on signage.
2. **Underground Utilities:** All new public utilities lines within the site will be underground (does not include Murfreesboro Pike or Hobson Pike frontages or existing utilities). Additionally, the developer will encourage the use of renewable energy, such as solar panels and wind power, as part of the redevelopment.
3. **Commercial Requirement:** The first phase submitted for final development shall include a minimum of two commercial or office spaces (minimum of three acres), and it must be approved in conjunction with or prior to the first residential phase (approximately 100 single-family attached and/or detached). In addition, the developer will encourage and prioritize restaurants on Murfreesboro Pike as part of phase 1.
4. **Open Space:** Open space shall be protected against building development by conveying to the city, homeowners' association, and/or a land trust, an open space easement restricting the area in perpetuity against development or removal of natural features, except as the Metro Planning Commission determines is consistent with conservation or recreational purposes. Fifteen percent (15%) of the total 65-acre site is to remain open space, and no more than 20% of the open space area will be devoted to structures or other impervious surfaces.

Development Phasing Schedule

We anticipate that the first phase of the development will be commercial along Murfreesboro Pike with the single-family attached and detached area. Throughout the process, the applicant will work with Metro Public Works and others on the phased improvements.

Addition items - Unregulated by Metro Planning

- **Home Owners Association (HOA):** The residential area will have an association of persons or entities formed by the owners of the development under restrictive covenant or a declaration of restrictions by the owners to address the maintenance of common elements and common concerns related to the development.
- **Art:** The developer will encourage art throughout the development. With the unique music history on the site, the developer plans to name streets after artists, have monuments to the history, and incorporate art sculptures throughout the development.

Nonresidential Character Images

Commercial/Mixed-Use Area



Office/Mixed-Use Area



The character images on this page indicate the general architectural character of the development. The main materials will be fiber cement board with brick or stone. Glazing requirements are listed in the Bulk Standards of this document. The building exteriors should be of similar design as shown in these character images.

Commercial



Residential Character Images

Single-Family Detached Residential



Single-Family Attached Residential



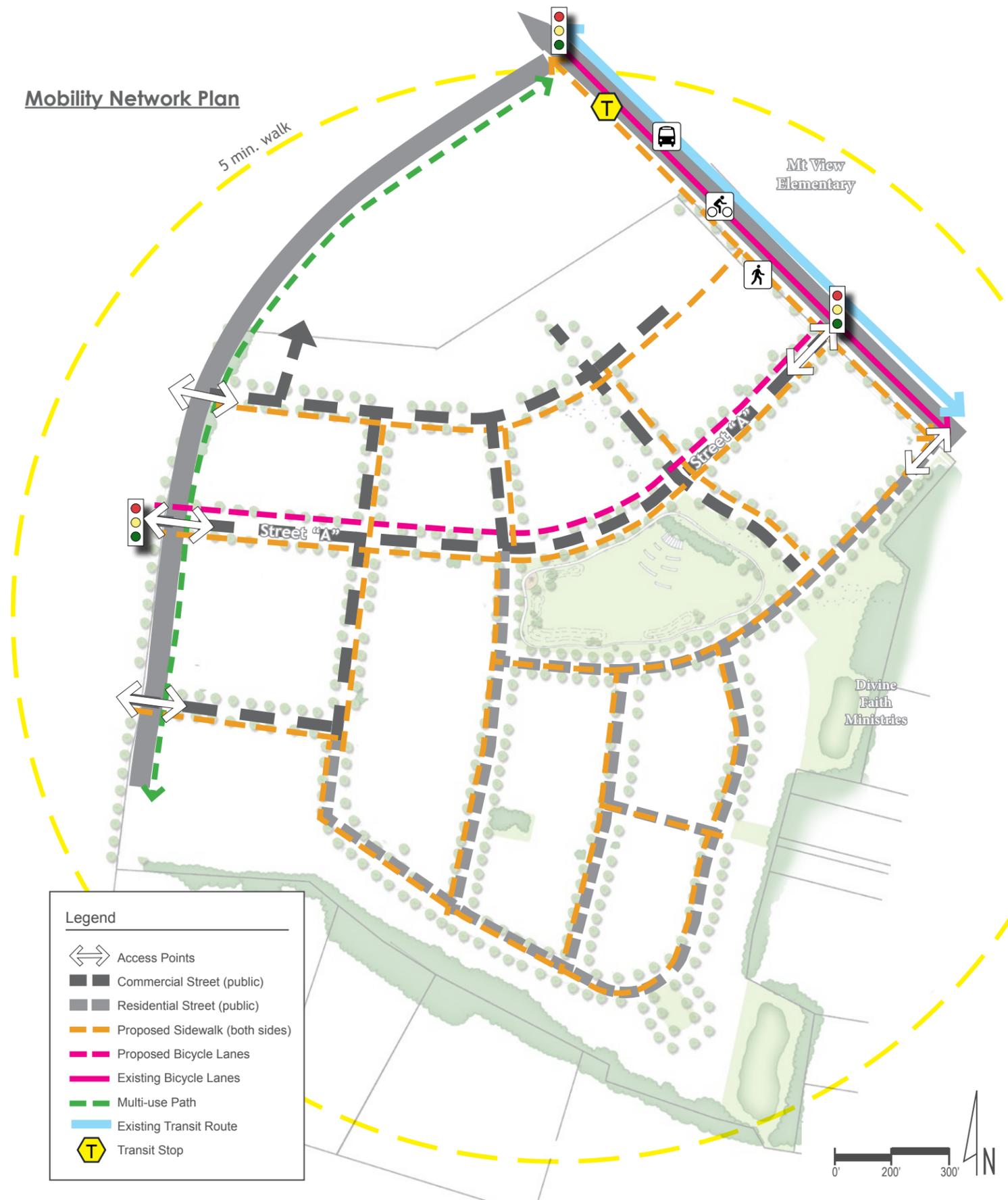
The character images on this page indicate the general architectural character of the development. The main materials will be fiber cement board with brick or stone. Glazing requirements are listed in the Bulk Standards of this document. The building exteriors should be of similar design as shown in these character images. Front loaded garage buildings shall be design to minimize the visual impact of the garage from the street by being recessed from the front façade by a minimum of seven feet.



Residential Flats

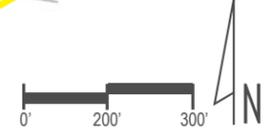


Mobility Network Plan

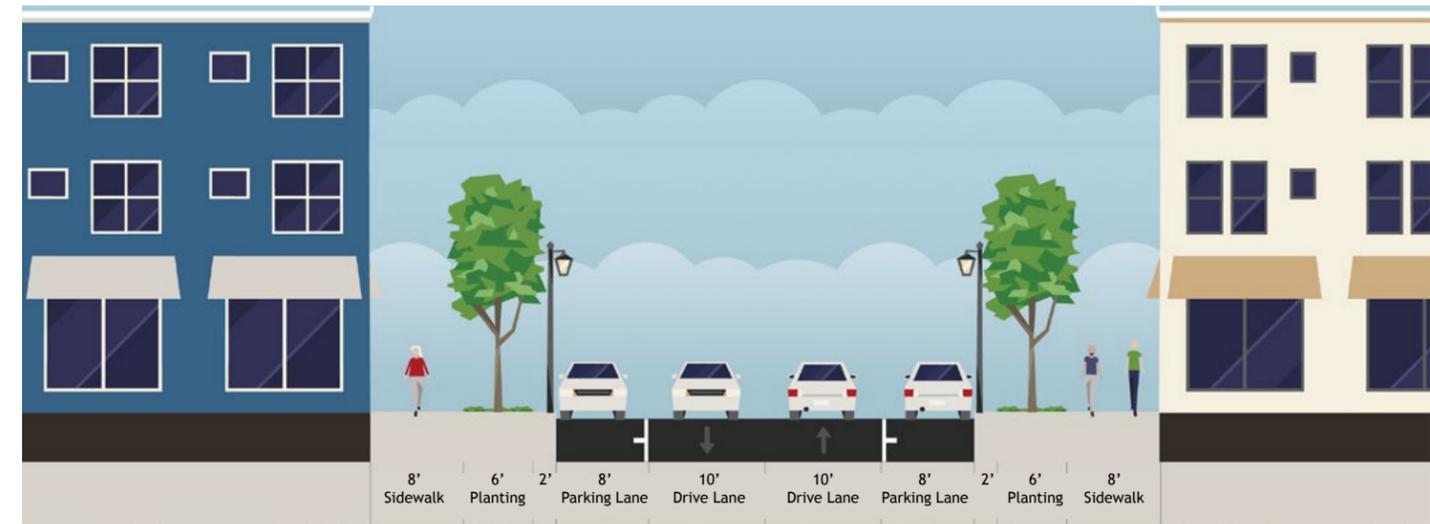


Legend

- Access Points
- Commercial Street (public)
- Residential Street (public)
- Proposed Sidewalk (both sides)
- Proposed Bicycle Lanes
- Existing Bicycle Lanes
- Multi-use Path
- Existing Transit Route
- Transit Stop

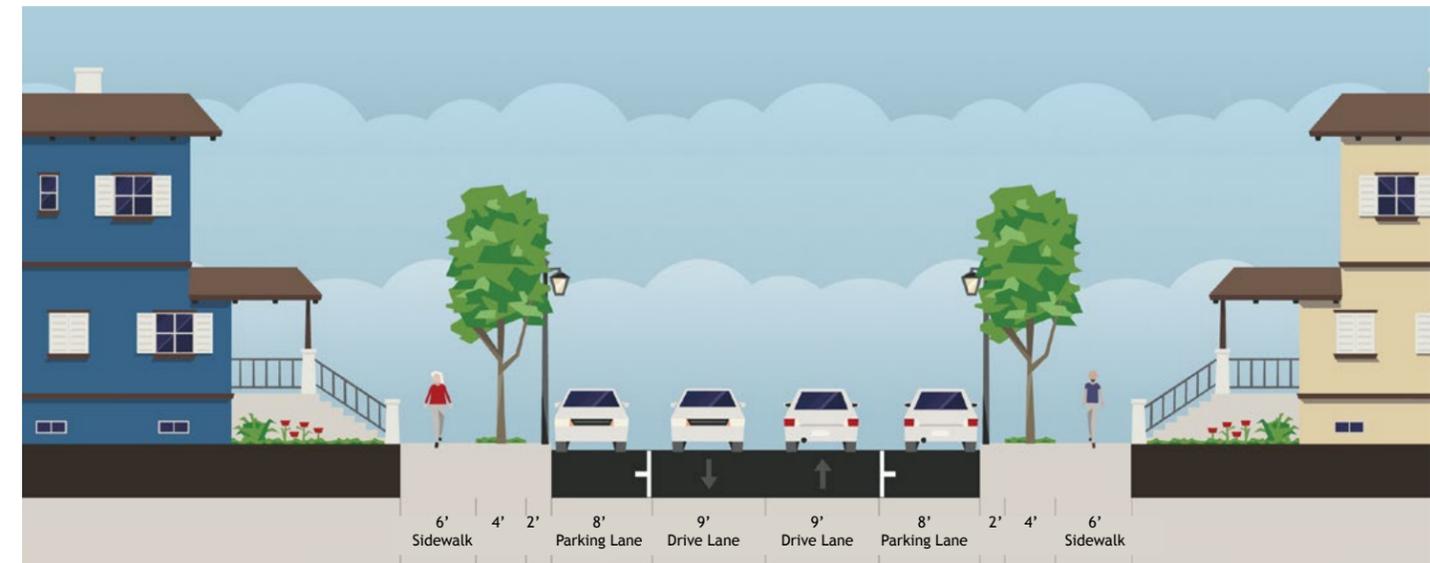


Typical Commercial/Office/Mixed-Use Street Section



Note: The main street through the development may include bike lanes as indicated on the plan. Wider sidewalks may be required by Metro Planning staff depending on the adjacent land use. Design Tool Source: <http://streetmix.net>

Typical Residential Street Section



Design Tool Source: <http://streetmix.net>

Note: The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.

Land Use Table

<p>Commercial All permitted and conditional uses defined by the MUL-A zoning district (at the time of building permit application) including multifamily residential shall be allowed in the mixed use component of this development, with the following uses prohibited.</p> <p>Prohibited Uses:</p> <ul style="list-style-type: none"> • Adult entertainment & retail • Cash advance and check-cashing establishments • Flea markets • Pawn shops & auction houses • Title loan shops • Transient housing • Warehousing & storage <p>Note: A temporary commercial amusement outdoor use is permitted, but limited to a maximum of 12,000 people. See Ordinance BL2010-805 for description and restrictions.</p>
<p>Residential Residential buildings include single-family homes, cottages, townhouses, and stacked flats.</p> <p>Note: Any development standards, regulations, and requirements not specifically addressed in this submittal and/or included as a condition of Commission or Council approval for this property shall be subject to the standards, regulations, and requirements of the MUL (for mixed-use subdistrict) and RM-9 (for residential subdistrict) zoning district effective at the date of the applicable request or application.</p>

Key Map



Development Standards

Development Standards are based on the existing Murfreesboro Pike Urban Design Overlay prepared by the Metropolitan Nashville Planning Department with some variations to provide more detail.

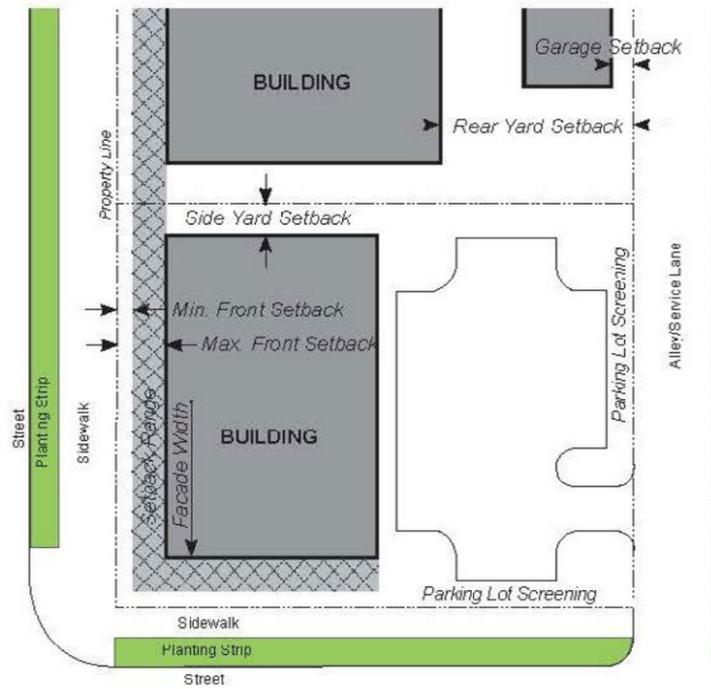
Development Standards - Bulk Standards

	Residential Subdistrict	Commercial/Office Mixed-Use Subdistrict
Description and uses include, but not limited to:	Residential buildings include single-family homes, cottages, townhouses, and stacked flats.	Mixed-use buildings refers to a vertical mix of two or more uses. Typically, commercial and/or office are on the majority of the first floor, though residential may be included. Upper-stories are typically dwelling units or office.
Minimum lot area:	No minimum sf	
Building setbacks:		
Front: ¹	26 ft from property line (6 ft encroachment allowed for porches, stoops and balconies)	0 to 10 ft from public ROW build-to line Up to 60 ft setback along Murfreesboro Pike (due to slope)
Front facing garage setback	7' behind front façade	N/A
Side: ^{2,4}	5 ft with exception	None required
Rear: ³	20 ft or 5 ft for garage	20 ft
Maximum square footage or units:	550 units	421,500 sf
Maximum floor area ratio (per lot):	N/A	N/A
Maximum impervious surface ratio:	N/A	0.80
Maximum height at setback:	2.5 stories (45 ft)	5 stories (75 ft)
Maximum height:	2.5 stories (45 ft)	5 stories (75 ft)
Slope of control plane:	N/A	1.5 (V) to 1.0 (H)
Minimum required parking:	Per Metro Standards	Per Metro standards
Fenestration/glazing:	N/A	First Floor: 40% minimum (colored or mirrored glazing is not appropriate) Upper Floors: minimum 25% glazing Applies to every street facing façade.
Grade relationship to the street	The finished floor elevation of the single-family (attached or detached) housing shall be a minimum of 1 ft above the top of the curb where it aligns with the front door of each unit.	
Additional design standards	Additional design standards are found on the following pages regarding architecture, parking & access control, landscaping, screening, etc.	

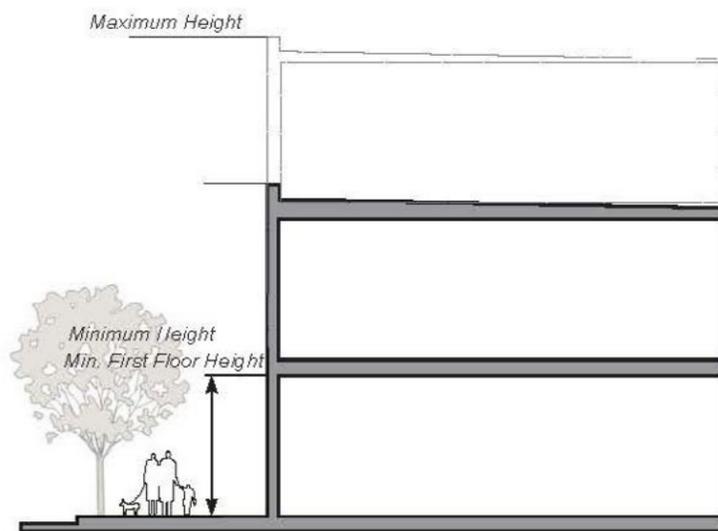
¹ EXCEPTIONS TO FRONT YARD SETBACK: Exceptions may be made for recesses up to 5 ft for articulation of the building facade and up to 20 ft for outdoor seating areas.
² EXCEPTIONS TO SIDE YARD SETBACKS: Side yards with street frontage, on corner lots, shall follow front yard setbacks.
³ REAR YARD SETBACK EXCEPTION: Development abutting property with an existing residential use shall comply with the applicable buffer yard standards of Title 17.
⁴ EXCEPTIONS TO SIDE YARD SETBACKS: Setbacks between townhouse units may be 0 ft.

Murfreesboro Pike Urban Design Overlay Prepared by the Metropolitan Nashville Planning Department with Modifications

Bulk Standards Calculations



Building Plan



Building Section

Setbacks

Setbacks are determined by the ground-floor use, residential or nonresidential.

A setback range (minimum and maximum) is indicated for the front yard setbacks.

The setback shall be measured from the back of the sidewalk at the property line. If additional sidewalk is dedicated by the property owner, the setback shall be measured from the back of the new sidewalk. The front yard setback shall also apply to secondary street frontages.

Note: Frontage is the specific way in which the building face orients to the street. Buildings shall front onto a public right-of-way (excluding alleys) or open space with a pedestrian way.

Height

Building height shall be measured from the average grade along the front property line to the top of eave or parapet. When buildings are set back from the back of sidewalk more than 20 feet, height shall be measured from the average grade at the building setback.

Facade Width

The facade width is the length of building facade that is built within the setback range. The required facade width is determined by the subdistrict and street type. The facade width standard shall only apply to the predominant street frontage and no minimum shall be required along a secondary frontage, if the property is located at the intersection of two or more streets.

Architectural and Building Type Standards

Permitted Attachments

- Balconies, stoops, porches, and bay windows are permitted to encroach beyond the front setback 6 feet. Residential single family (attached and detached) facing open space shall have a front porch.
- Porches shall be a minimum of 6 feet deep.
- Stoops and balconies shall be a minimum of 4 feet deep.
- Entry stairs, awnings, and canopies may encroach beyond the front setback, but shall not encroach into the public right-of-way without proper permitting from Metro Public Works.

Glazing

Minimum glazing requirements shall be calculated based on the facade area as follows:

- Nonresidential first floor: facade area shall be measured from the average grade at the sidewalk to 14 feet above grade.
- Residential first floor and all upper floors: facade area shall be measured from finished floor elevation to finished floor elevation.
- The glazing requirements for buildings on corner lots shall return a minimum of 20 feet along the side facade.

Materials

- Building fronts must be built or clad with a high- finish material, such as (but not limited to): brick, brick veneer, stone, and hardy board
- Prohibited materials: plywood, plastics, unfinished concrete block, metal buildings, vinyl siding, E.I.F.S. (on ground floor).
- Primary facade materials shall not change at outside corners and shall wrap the corner a minimum of 3 feet. Material change shall only occur on inside corners.

Standards Specific to Building Types

Townhouse Courts, Cottage Courts, and Courtyard Flats (Residential Building Type)

Housing types that front a central open space.

- The width of the central open space, measured between buildings, shall be no less than the height of the buildings, measured to the bottom of the eave or top of parapet.
- Dwellings shall abut a minimum of two sides of the central open space.
- Parking, driveways, and detention areas shall not be located within central open space. Modifications may be permitted for stormwater elements such as rain gardens and bioswales when incorporated as a design element.
- Parking adjacent to the open space shall be separated from it by landscaping or an architectural screen.
- Interior sidewalks, a minimum of 5 feet wide, shall connect all units to the sidewalks at the street frontage and the parking areas.

Principal and Minor Frontage

- Every building shall establish one principal frontage and minor frontage(s) for the remaining street(s). Buildings with frontage conditions along Murfreesboro Pike or Hobson Pike shall establish Murfreesboro Pike or Hobson Pike as the principal frontage.

Pedestrian Access

All buildings shall have at least one pedestrian entrance on the principal frontage. This may be access to a lobby shared by individual tenants. Corner entrances are appropriate on corner lots. A direct connection must be provided from the public sidewalk along the principal frontage to the building's primary entrance. Vehicular access points and on-site vehicular circulation shall be designed so that conflicts between pedestrians and vehicles are avoided.

Source: Murfreesboro Pike Urban Design Overlay prepared by the Metropolitan Nashville Planning Department

Parking and Access

Parking Location

- Parking shall be located behind, beside, or beneath buildings on all lots except as follows:
 - One module of parking (two rows and a drive aisle) is permitted between the building and the street.

Parking Setback

- Garages shall be set back from alleys and streets 5 feet or greater than 15 feet but not in between. See bulk standards on page 12 for front facing garage setback requirements.
- Parking areas that accommodate four or more vehicles shall be separated from buildings a minimum of 8 feet in order to accommodate a sidewalk and planting area.

Parking Lot Lighting

- Lighting shall be appropriate in function and scale for both the pedestrian and the vehicle. Lighting that minimizes light trespass, pollution, and uplight shall be utilized.

Drive-throughs

- Drive-throughs should be located to the rear of buildings. If drive-throughs are located to the side of buildings, their width shall be limited to two lanes and they shall be recessed from the front facade by a minimum of 20 feet. In no case shall drive-throughs be located at the corner of two public streets.

Vehicular Drop-offs

- Vehicular drop-off areas shall be articulated through paving patterns, bollards, landscaping, and other means that limit the impact of conflicts between vehicles and pedestrians.

Bicycle Parking

- Multifamily residential development shall provide bicycle parking at a minimum of 2 spaces, or 1 space per 10 bedrooms, whichever is greater. Fifty percent (50%) of the parking shall be covered.
- Nonresidential development shall provide bicycle parking at a minimum of 2 spaces, or 1 space per 15,000 square feet, whichever is greater.

Landscape Requirements

- Streetscapes shall respond to the scale and context of the buildings, both commercial and residential.
- Enhanced pedestrian walkways shall be accented with appropriate plant material in accented planting areas.
- Dumpsters and other trash receptacles for all structures other than single-family or two-family residences shall be screened from public streets or properties which are zoned or policies for residential use. Receptacles shall be placed on a concrete pad and shall be enclosed

by an opaque fence or wall at least six feet in height. An enclosure shall have an opaque gate unless the service opening is oriented away from public streets or adjacent residential properties. The enclosure shall be built of wood, masonry or other permanent materials and evergreen plants may be used in part to meet the requirement of opacity and parking areas adjacent to public streets as required by the Murfreesboro Pike UDO.

- Buffers between this SP zone and surrounding properties which are not currently zoned commercial shall require a minimum density of type “C” buffer, as defined by the Metro Zoning Code 17.24.240.
- Street trees shall be installed at a maximum spacing of 40 feet. Street trees shall be required along all public roads and the major internal streets as shown on the Structural Site Plan. Trees should be selected to achieve a uniform streetscape, provide a broad canopy, prevent sidewalk damage, and conserve water. At installation, a street tree shall have a minimum caliper of three (3) inches. Generally, large-maturing trees shall be installed; where infrastructure incompatibility would result, small-maturing trees are permitted.
- Within commercial parking areas, a minimum of one tree shall be required for every 15 parking stalls.
- All landscaping must comply with the requirements of the landscaping provisions set forth in the Metro Zoning Ordinance and shall be reviewed and approved by Metro’s urban forester.
- Perimeter Landscaping for Surface Parking Areas: Parking areas and driveways shall be separated from the edge of the right-of-way by a perimeter landscape strip a minimum of 10 feet wide and planted with evergreen shrubs to screen parking areas. Parking areas shall be separated from adjacent side lot lines by a perimeter landscape strip at least 5 feet in width.

Site Lighting and Utility Requirements

- Provide appropriate illumination levels in keeping with the needs and safety of the pedestrians.
- Provide pedestrian-scale decorative light fixtures along all enhanced pedestrian walkways and streetscapes, as denoted on the Regulating Site Plan.
- General area and lighting in the commercial area shall utilize a “shielded light source” fixture. Lighting design shall achieve maximum uniformity with as few light poles as practical.
- Lighted nonpublic areas shall utilize a “shielded light source” fixture.
- Utility boxes and aboveground backflow preventers shall be prohibited in the front yard of all residential dwelling units.

Signage Requirements

For signage requirements, see the Murfreesboro Pike UDO. Below is the summary table from the document.

SECTION II: DEVELOPMENT STANDARDS

Signage Standards: Tables

Non-Residential Uses: Building Signage Table

Sign Type	Maximum Display Surface of Individual Signs	Maximum Height	Maximum Projection	Specifications
Wall Mounted Projecting ⁴	12 square feet	Below 2nd Story FFE or below top of parapet/eave, whichever is less	6 feet from building facade	Minimum Clearance is 8 feet or 10 feet within 10 feet of the right-of-way.
Wall Mounted Projecting (Second Story and Above) ⁴	5% of the building facade on the public entry side or a maximum of 64 square feet whichever is less	Below top of parapet/eave	N/A	N/A
Wall Mounted Flush ⁴	5% of the building facade on the public entry side or a maximum of 64 square feet for single story buildings and 84 square feet for multi-story buildings, whichever is less			
Awning ⁴	40% of all visible faces	N/A	N/A	On 1st story only, On a maximum of 2 awning faces
Window	15% of window			On 1st and 2nd story only

Non-Residential Uses: Ground Signage Table

Sign Type	Permitted	Maximum Display Surface of Individual Sign Height	Maximum Height	Minimum Setback from Side Property	Specifications
Monument ^{2,3}	1 per street frontage	48 square feet	7 feet or 2.5 feet within 15 feet of a driveway	3 feet	None of the signage are may extend above the masonry pillars
Freestanding Pillar ^{2,3}	1 per arterial street frontage for buildings with a setback greater than 15 feet		20 feet		

Residential Uses: Signage Table

Sign Type	Permitted	Maximum Display Surface of Individual Sign Height	Maximum Height	Maximum Projection	Specifications
Monument ^{2,3}	1 per street frontage	20 square feet	5 feet or 2.5 feet within 15 feet of a driveway	N/A	A low wall may be substituted for a monument sign.
Building (3-15 units) ⁴	1 flush building sign per street facade	6 square feet	Below 2nd story Finished Floor Elevation (FFE) or below top of parapet/eave		N/A
Building (16 or more units) ⁴		20 square feet			

¹ Any sign that encroaches on a public right-of-way must meet Metropolitan Government’s current clearance standards and the encroachment must first be approved under the mandatory referral process.

² Maximum of one Ground Sign permitted per street frontage for non-residential development.

³ Properties with greater than 400 feet of frontage shall be permitted two ground signs. Signs on a single property shall be spaced a minimum of 100 feet apart.

⁴ No building sign shall protrude over the top of the parapet or eave of the main roof of a structure

Preliminary Grading & Drainage Plan

Preliminary Grading and Drainage Plan

The preliminary grading and drainage plan provides a general drainage design sloping to the south and southeast side of the site where detention ponds are shown. The plan is conceptual and similar to the 2008 approved plan. Detailed site grading information will be submitted with the Final Plat.

Metro Water Services General Notes:

1. According to FEMA's current flood maps (47037CO411H, Zone X, 4/5/2017), as well as Metro's GIS information, there is no 100-year floodplain within the SP boundary.
2. Drawing is for illustration purposes to indicate the basic premise of the development as it pertains to Metro Stormwater approval/comments only. The final lot count and details of the plan shall be governed by the appropriate stormwater regulations at the time of final application.
3. Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with Stormwater Management Ordinance No. 78-840 and approved by The Metropolitan Department of Water Services.
4. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any Storm water facilities within the property.
5. Size driveway culverts per the design criteria set forth by the Metro Storm Water Management Manual (Minimum driveway culvert in Metro ROW is 15" CMP).
6. Individual water and sewer service lines are required for each home (where applicable).
7. All water and sewer construction shall be in accordance with specifications and standard details of the Metro Water Services.
8. The proposed stormwater management for this project includes collection of the site stormwater on the south and southern edge of the site with two large detention ponds.
9. Final construction plans and road grades shall comply with the design regulations established by the Department of Public Works. Slopes along roadways shall not exceed 3:1.
10. Final SP to show ditches/pipes and provide a Public Utility and Drainage Easement in accordance with the SWMM, Vol. 1.
11. A field hydrologic determination has been conducted on site and revealed the channel is piped. In the Final SP, plans shall include 30' stream buffer in accordance with the SWMM for any non-channelized streams.

Soil Report Summary:

Based on the information provided by the Natural Resources Conservation Service (NRCS) Web Soil Survey, the majority of the site is Rock outcrop-Talbott complex, 5 to 15 percent slopes (± 32 acres), and followed by Talbott silt loam, 2 to 10 percent slopes (± 15 acres). The remainder of the site is under 20 percent slope and is a mix of Hampshire silt loam and Gladeville rock outcrop complex.



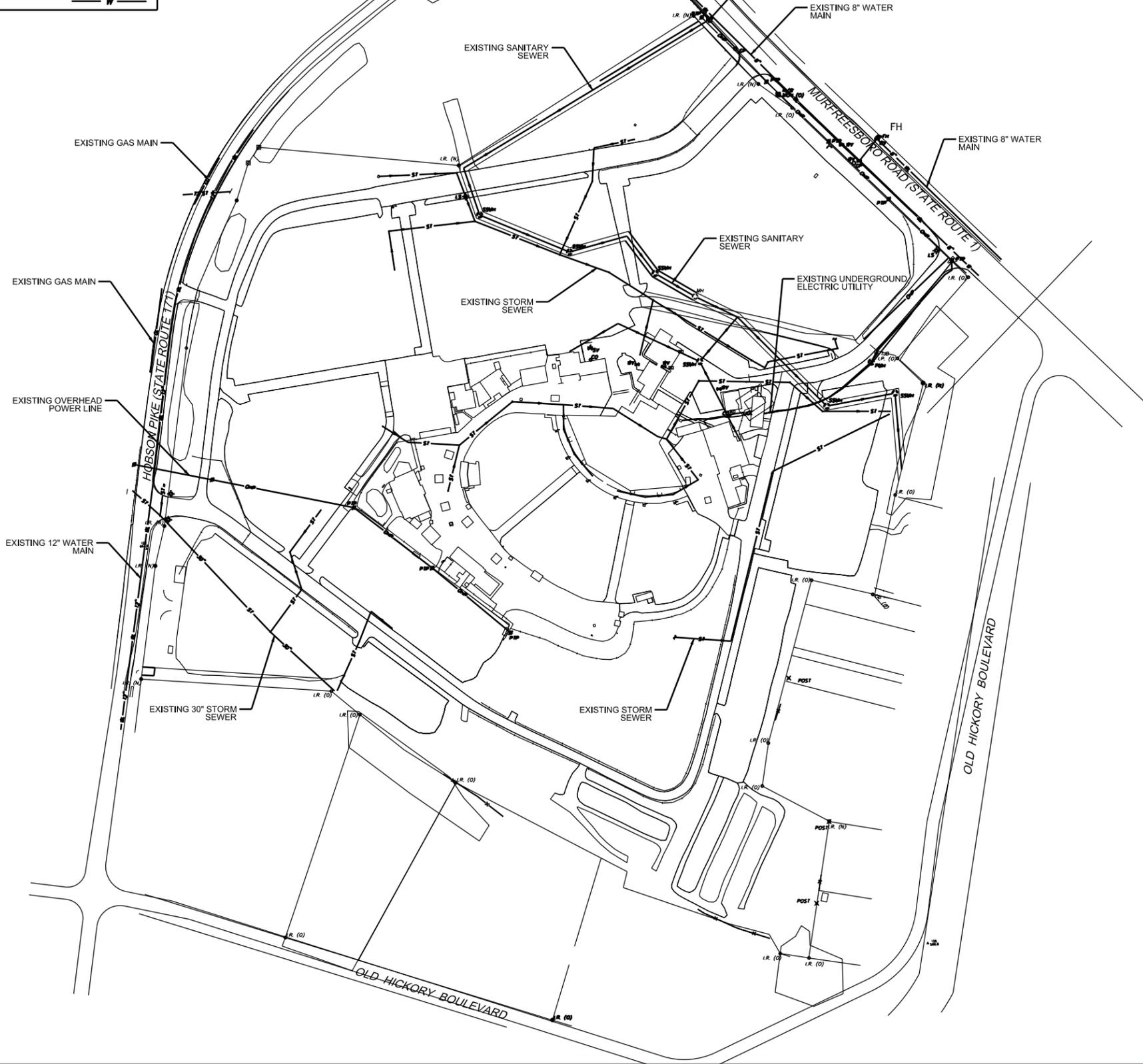
Preliminary Utility Plan

This plan shows existing utilities to the site. Future utilities and easements will be provided as part of the Final SP submittal.

The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of a building permit.

NOTE: ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM FIELD LOCATED VISIBLE APPURTENANCES, ATLAS MAPS AS MAY BE AVAILABLE FROM MUNICIPALITIES OR UTILITY COMPANIES, AND OTHER DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES STATE THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION CONSULTED. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. TO AVOID ANY POSSIBLE HAZARD OR CONFLICT, IT IS A REQUIREMENT OF THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT, FOR ANYONE WHO ENGAGES IN EXCAVATION TO NOTIFY ALL KNOWN UTILITY OWNERS NO LESS THAN THREE (3) OR MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE. CALL TENNESSEE ONE CALL AT 1-800-351-1111.

LEGEND	
SANITARY SEWER LINE	SA
STORM LINE (RCP, CMP, PVC, CPP)	ST
OVERHEAD POWER LINE	OHP
UNDERGROUND ELECTRIC	UGE
GAS LINE	GL
WATER LINE	W



SCALE: NOT TO SCALE