

# MODERA GERMANTOWN



Mill Creek Residential  
3102 West End Avenue  
Suite 780  
Nashville, TN 37203

Contact: Luca Barber  
LBarber@MCRTrust.com  
615.762.3825



Cooper Carry  
191 Peachtree Street, Suite 2400  
Atlanta, Georgia 30303

Contact: Kyle Reis  
kylereis@coopercarry.com  
678.539.4680



Kimley Horn  
214 Oceanside Drive  
Nashville, Tennessee 37204

Contact: Brendan Boles  
Brendan.Boles@kimley-horn.com  
615.564.2720



Hawkins Partners, Inc  
110 South 10th Street Second Floor  
Nashville, Tennessee 37206

Contact: Brian Phelps  
b.phelps@hawkinspartners.com  
615.255.5218



# CONTEXT

# SITE UNDERSTANDING

The Modera Germantown property is located in Nashville, Tennessee in the Germantown neighborhood immediately north of Downtown Nashville. Germantown was established in the 1850s by European immigrants and was considered one of Nashville's first suburbs. The neighborhood was originally settled by a diverse socioeconomic community with brick townhomes adjacent next to worker cottages. It was and still is a picture of truly mixed-use and diverse community for Nashville. This historic community has continued to grow and thrive. In recent years, it has experienced a rebirth becoming a destination for local boutiques, artisans, and renowned restaurants. The neighborhood continues to be a hub of cultural activities housing both the Nashville Farmer's Market and the Nashville Sounds baseball field, First Tennessee Park, as well as host to a number of neighborhood festivals throughout the year.

The eastern area of Germantown has seen an increase in quality mixed-use development, and we intend to build on that momentum to help support the enhancement of a mixed-use neighborhood, which helps meet the vision of the North Nashville Community Plan. While the northern portion of the property is designated as District Industrial, the recent and current development trend in the area is creating a walkable, urban, mixed-use district. Our parcel is a key part in building on that momentum. While it is bound by Metro Water to the north, the activity to the west and south of this property makes it an important development parcel to help connect Germantown to the Cumberland River.

The Modera Germantown property is approximately 12 acres and is bound on the north by the Metro Water facility, on the east by the Cumberland River, on the south by the parcel owned by Gateway TBR Hammer Mill, LLC and on the West by Adams Street. The property is located in District 19 and is identified as "D Industrial" (D IN) in the North Nashville

Community Plan document adopted June 22, 2015 and amended August 24, 2017. The site is currently zoned as Industrial General District (IG) and it is the intent of Mill Creek Residential ("Client") to have the property rezoned through the Specific Plan (SP) District Ordinance.

Mill Creek intends to re-develop the existing industrial property into a mixed-use community with apartments and retail. The project will be developed in two phases. Phase I will be approximately 7 acres and will include a maximum of 400 multifamily units. Phase II will be approximately 5 acres and will include a maximum of 400 multifamily units with 10,000 – 16,000 square feet of retail located in the southwest corner of the site. The retail in Phase II will enhance the emerging retail district at the intersection of Van Buren and Adams Street. The two phases will be independent of one another with their own respective amenity and clubhouse space, but will share the open space between the two buildings and access to the Greenway Trail.

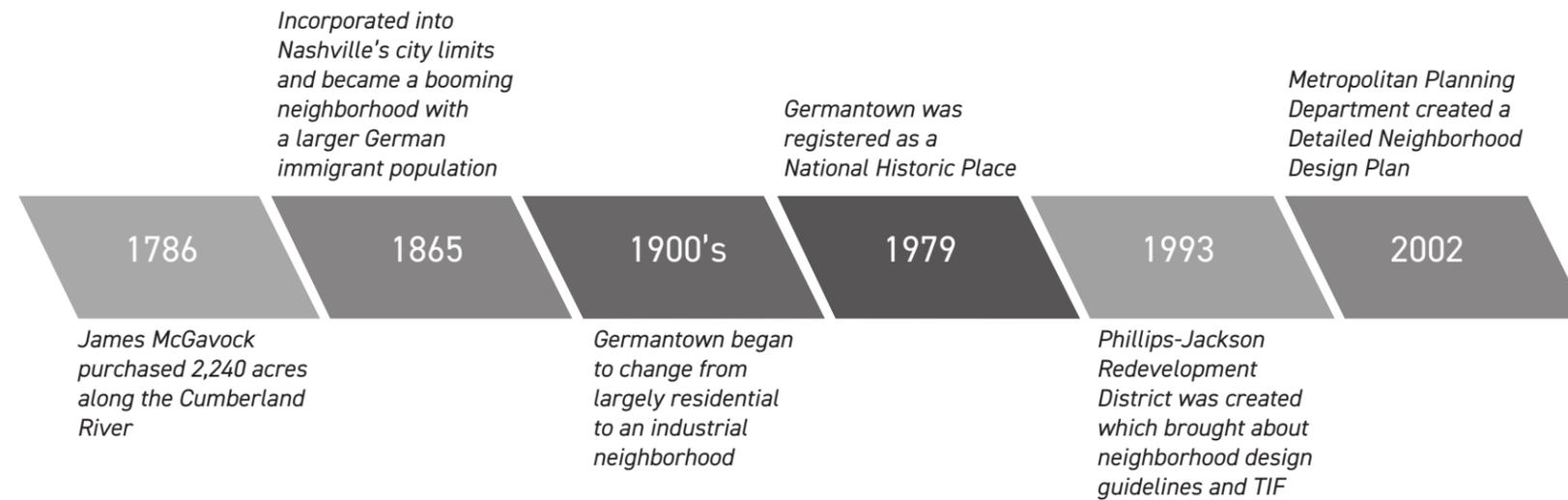
The Metro Water Facility on the Northern edge provides an opportunity to leverage the existing structures as feature elements for the community. The Mill Creek team will work to cooperate with the applicable departments to further investigate the potential of elevating the exterior of the tanks through a number of public art installations. Iconic murals and public art are prevalent throughout Nashville. Painting the tanks would provide an iconic attraction while also contributing to a vibrant corridor along Adams Street.



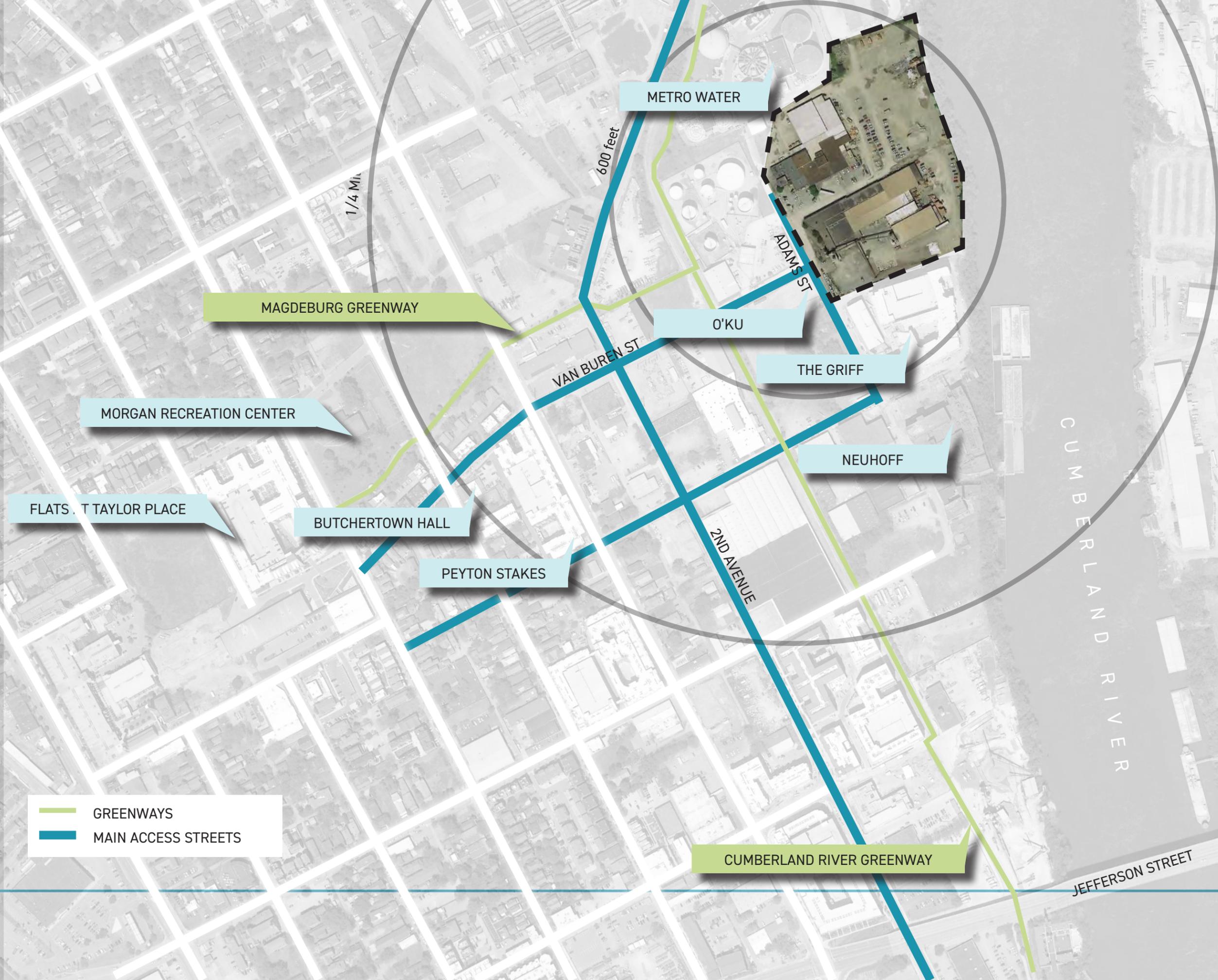
Historic Germantown



Existing Site Conditions



# VICINITY MAP



— GREENWAYS  
— MAIN ACCESS STREETS

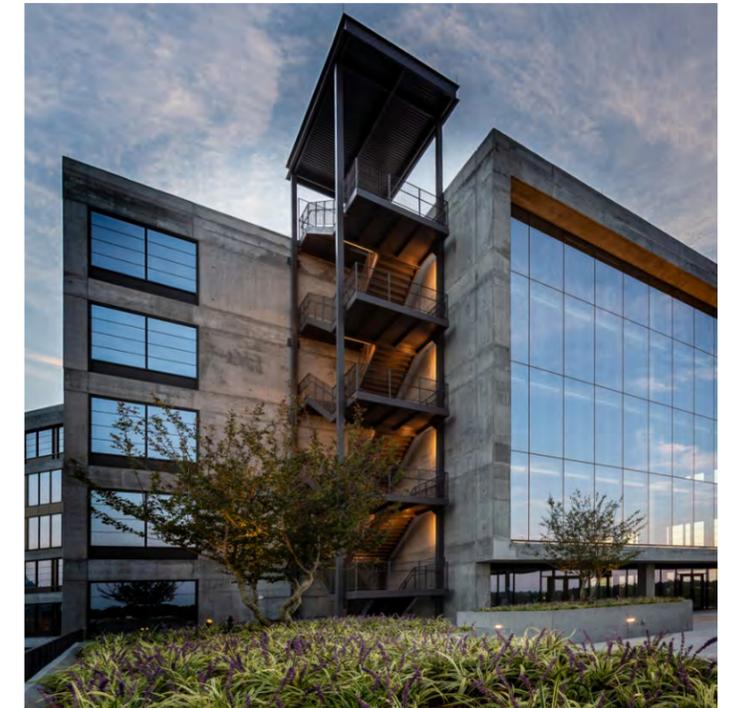
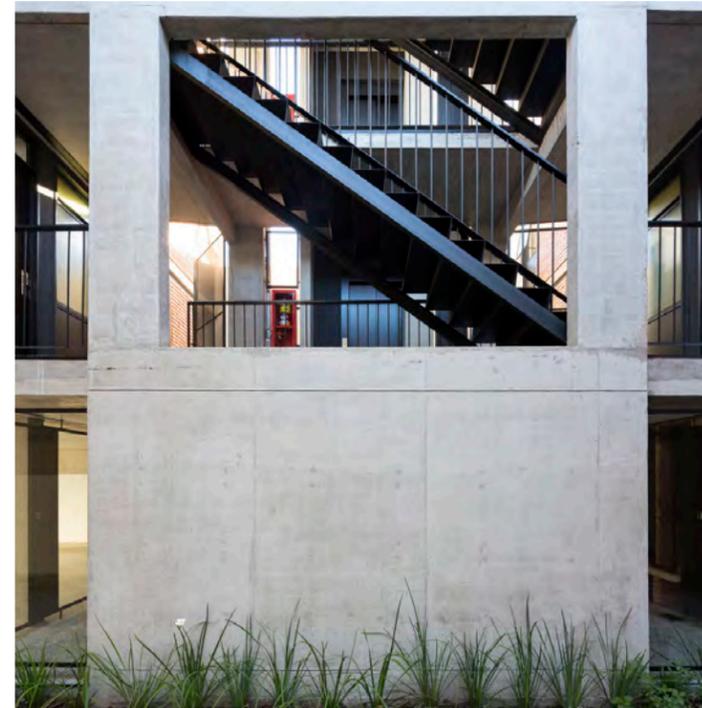
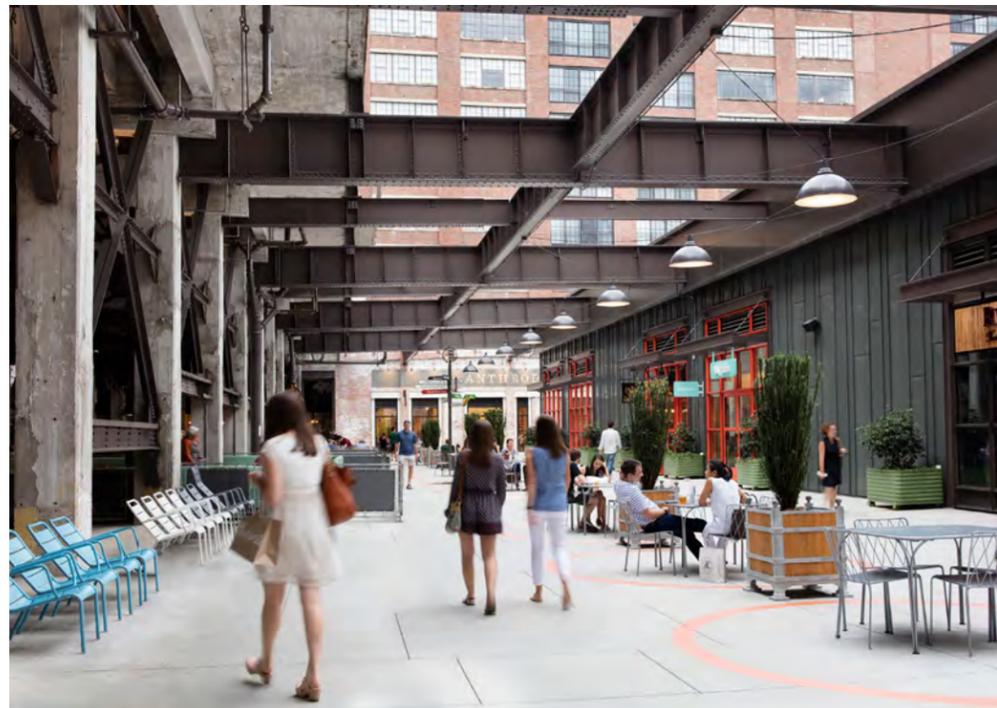
# DEVELOPMENT VISION

## VISION

The Vision for the Modera Germantown site is to create a mixed-use environment that reflects the community in both architecture and urban design while elevating the waterfront. The current waterfront is not accessible or even visible to residents; however with neighboring developments that are creating accessible river frontage, our hope would be to support and enhance the riverfront through dynamic spaces for residents to enjoy.

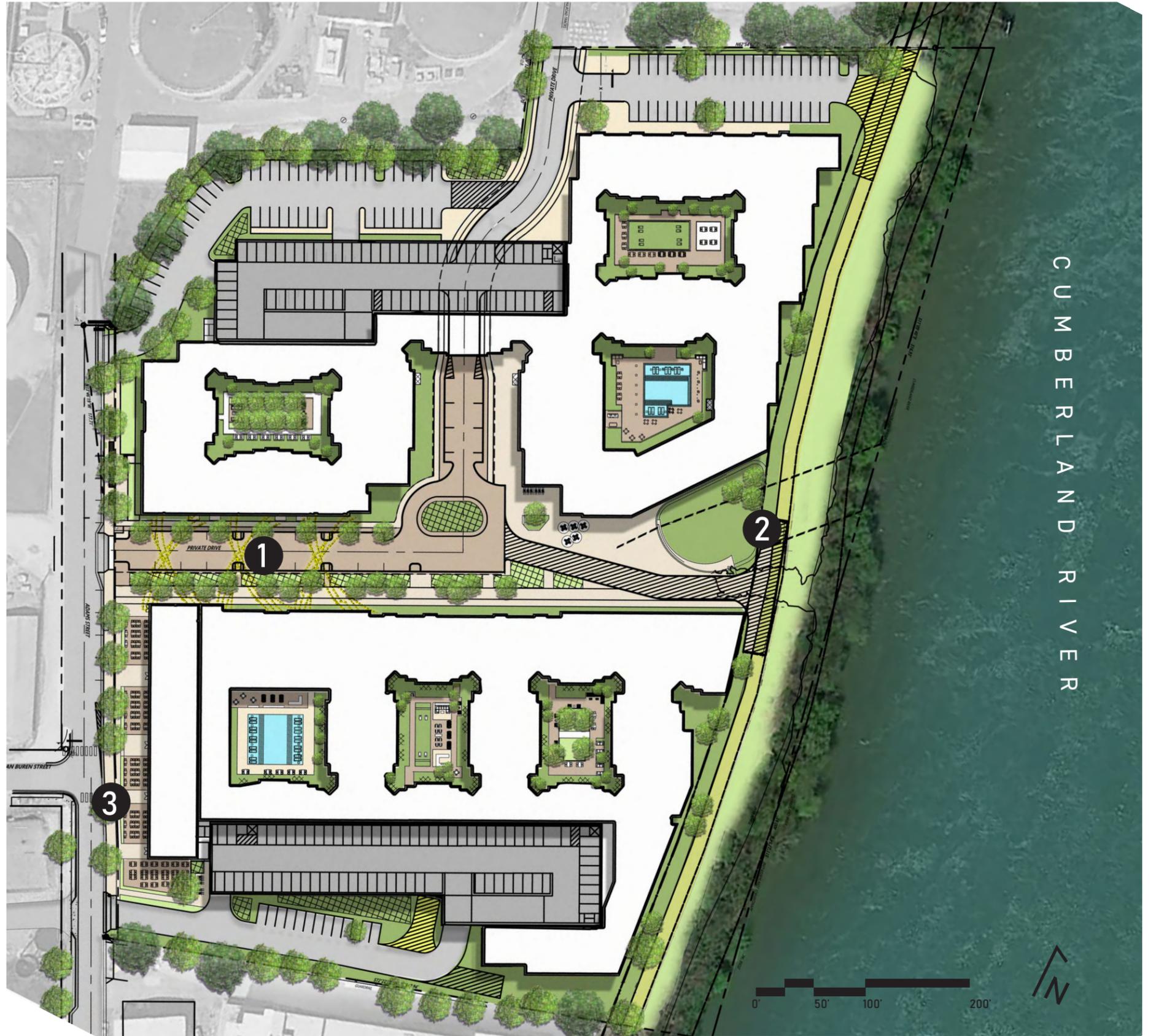
## INSPIRATION

Germantown itself is steeped in architectural history and character. The juxtaposition of old and new, and industrial and modern have created a unique and diverse community. The Modera Germantown site has operated as a number of uses in the past from meat dressing facility to stone manufacturing to now mixed-use living. The architectural inspiration draws from the neighboring buildings and artistic community that is Germantown.



# MASTER PLAN

- 1 CENTRAL DRIVE
- 2 THE WATERFRONT
- 3 THE RETAIL PLAZA



# THE CENTRAL DRIVE

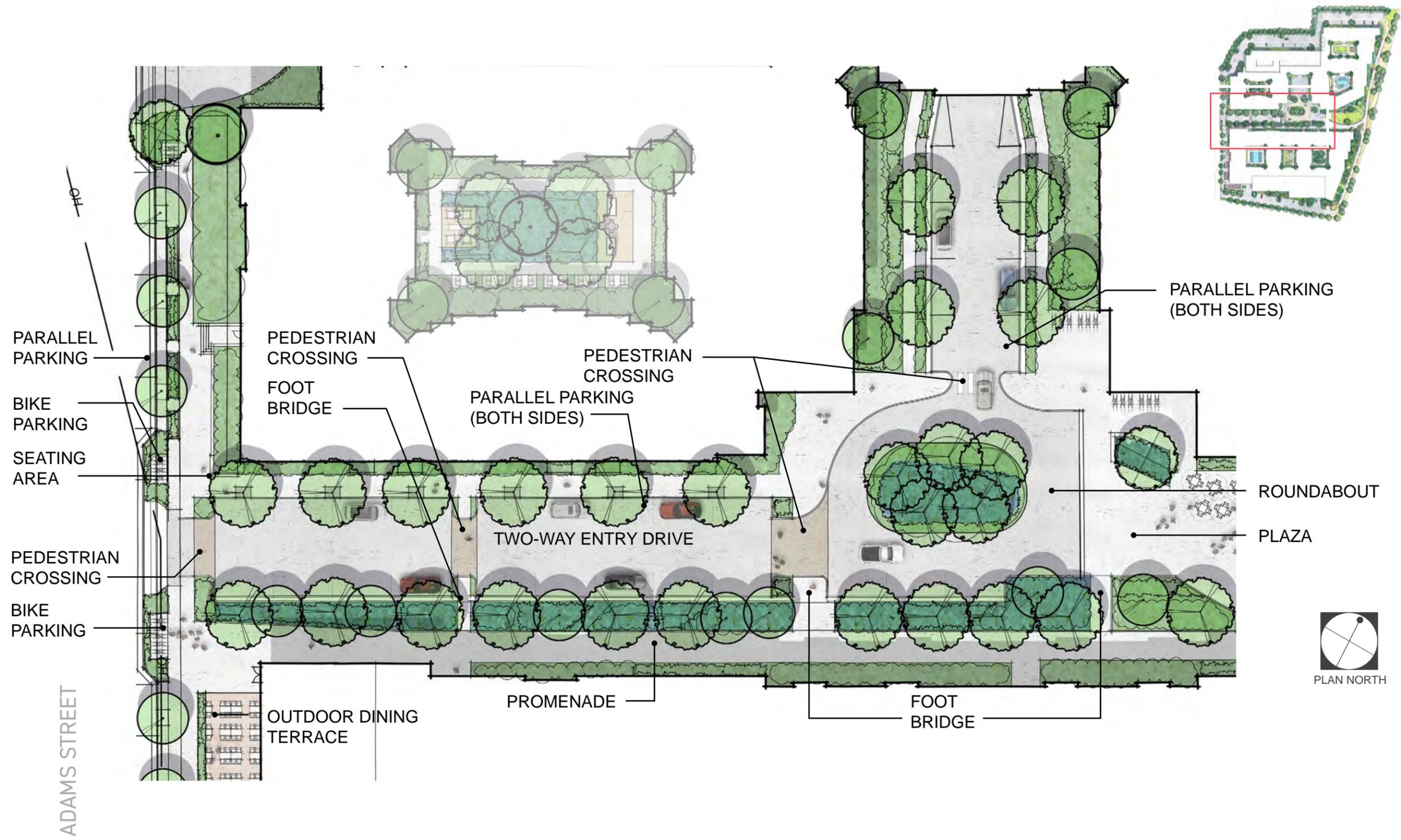
A visible separation between Phase 1 and Phase 2, the Central Drive creates a welcoming entrance for both pedestrians and vehicles. Wide sidewalks, parallel parking and attractive landscaping enhance the pedestrian experience creating a gateway to the waterfront.

Note: Images shown are meant to be inspirational in nature



- 1 SIDEWALK AND BUILDING RELATIONSHIP
- 2 SIDEWALK EXPERIENCE DOWN THE CENTRAL DRIVE
- 3 STORMWATER MANAGEMENT
- 4 PLAZA TREATMENT AND SEATING OPTIONS

# THE CENTRAL DRIVE

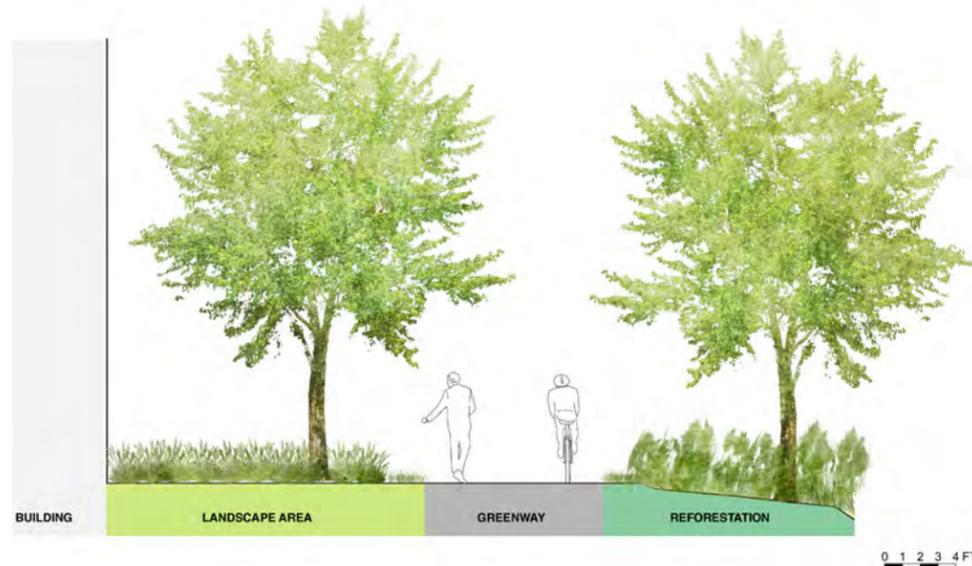
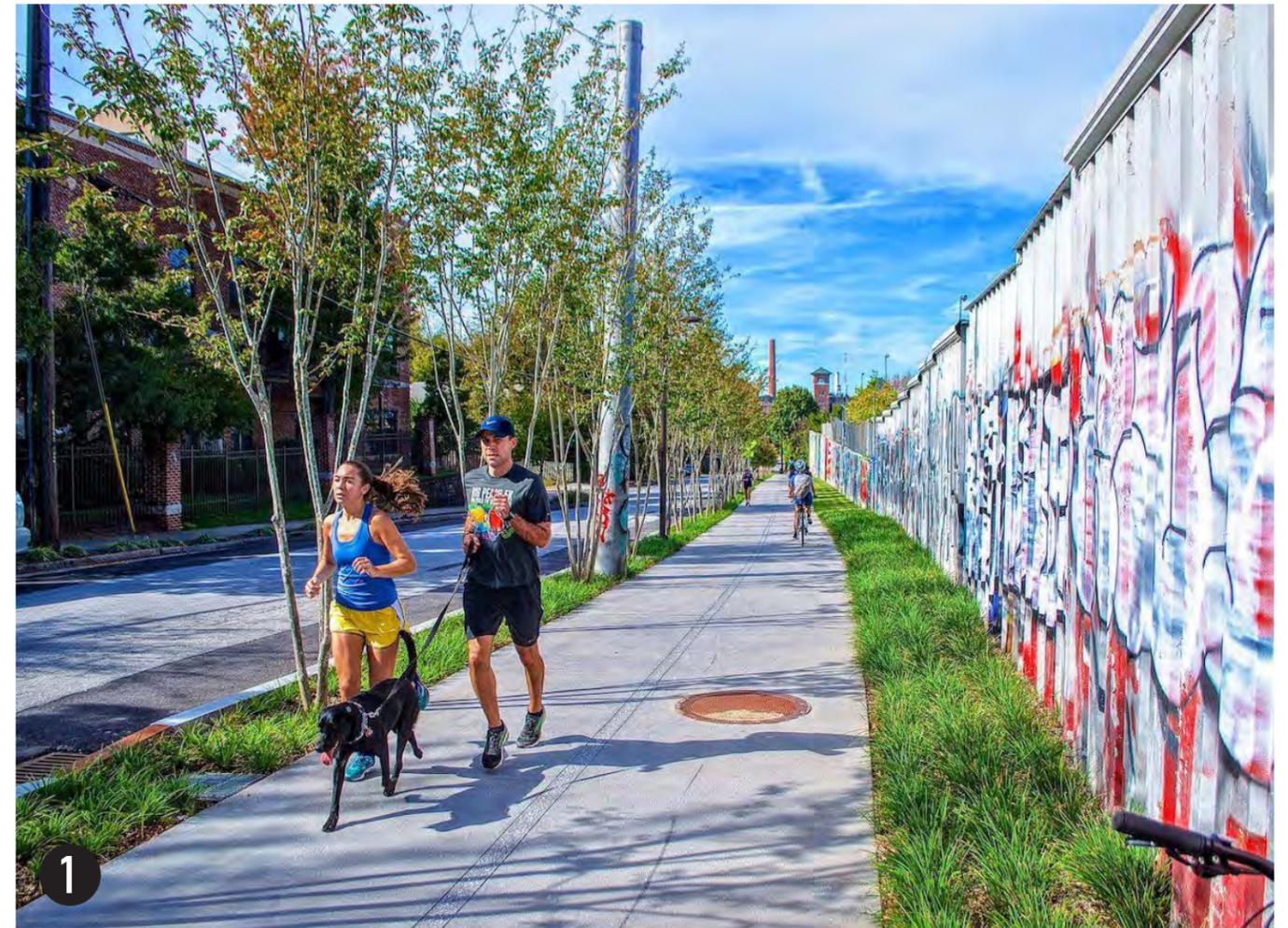


# THE WATERFRONT

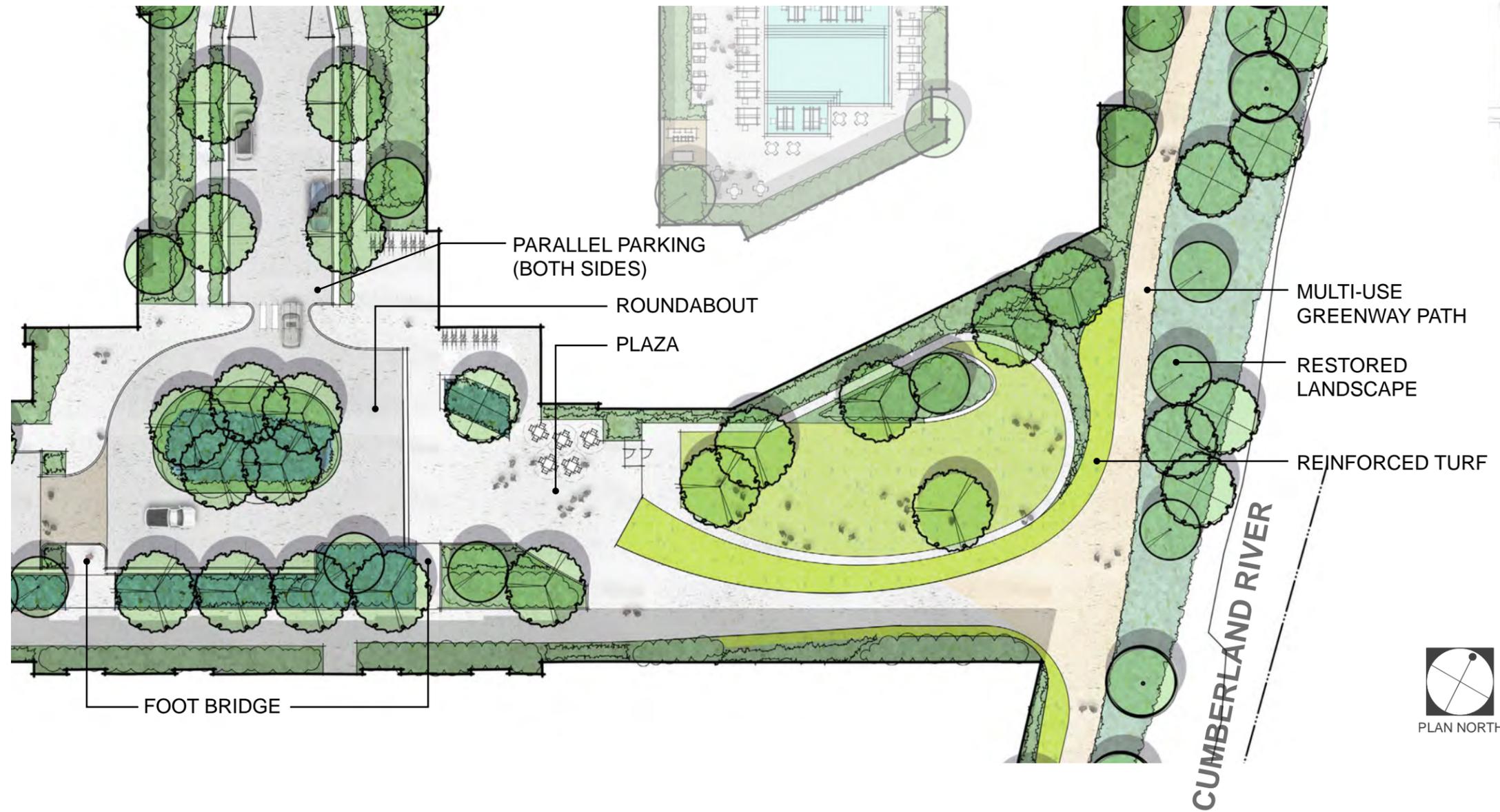
Situated on the Cumberland River, Modera Germantown has a unique opportunity to enhance the waterfront experience for Germantown residences. The waterfront is accessed via the Central Drive and provides amenities like a dog park, walking trail, and places for residences to congregate and gather. Attractive landscape areas also double as creative solutions to bio-retention helping to mitigate stormwater and potential runoff. Over the years, the vegetation within the 50 foot stream buffer has been continually mowed down providing Modera Germantown the opportunity to not only restore this area, but to also enhance the landscape along the waterfront.

Note: Images shown are meant to be inspirational in nature

- 1 ACTIVITY ALONG THE WATERFRONT
- 2 OPEN SPACES FOR USES LIKE A DOG PARK OR GATHERING SPACE
- 3 RESTORED AND ENHANCED LANDSCAPE ALONG WATERFRONT



# THE WATERFRONT



# THE RETAIL PLAZA

Historically, this site and the Neuhoff Slaughterhouse were not only neighbors but also reliant on each other's services. Neuhoff provided the function of a slaughter house to the cattle that arrived at its dock, and from there, the Modera Site would process and dress the meat. More than a century ago the two sites expressed a synergy along Adams Street, which will once again be re-established but in a new form.

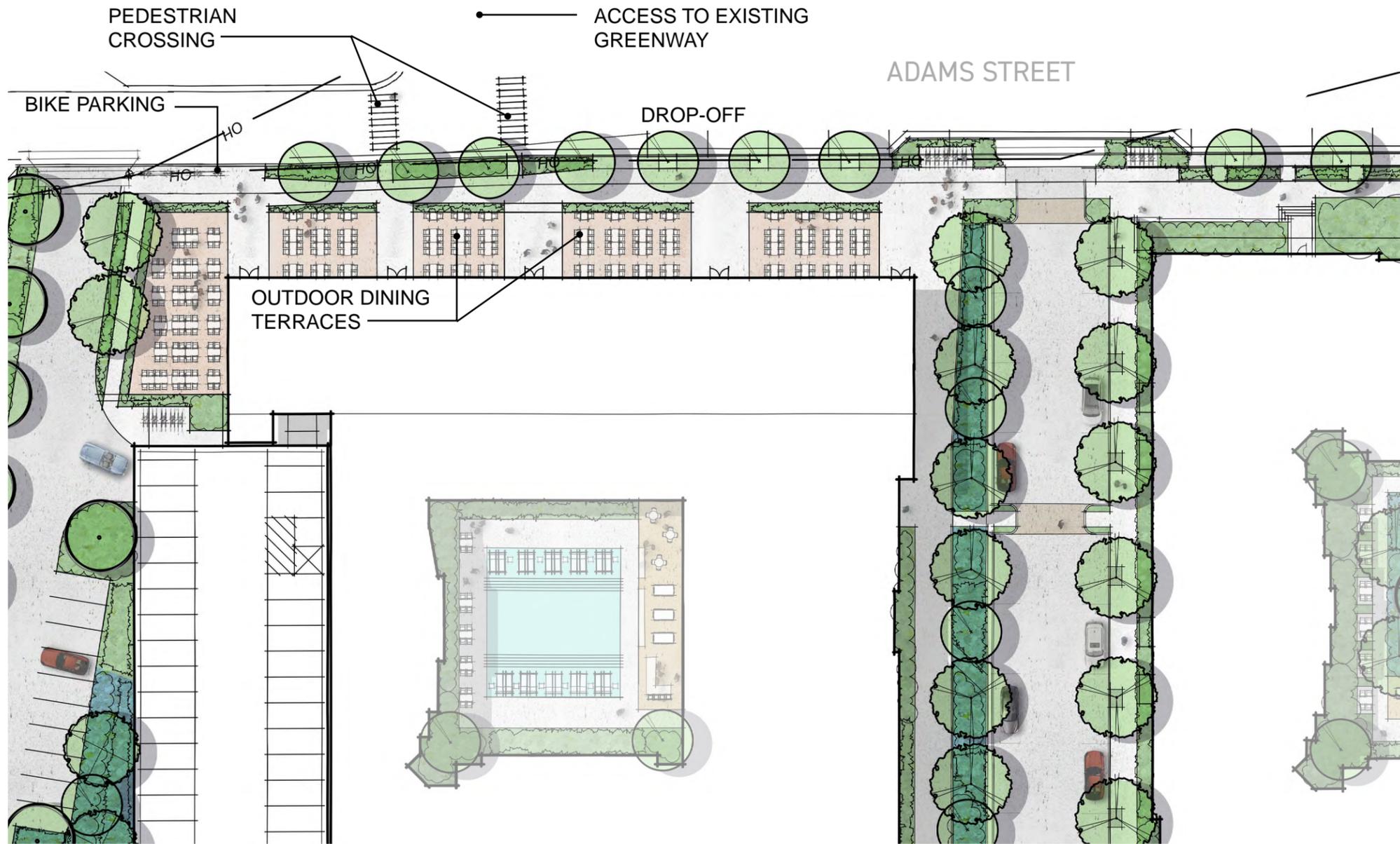
The Modera Germantown development is part of a lively retail district that is taking shape along Adams Street. With the continued addition of top tier restaurants like O'Ku and Ford Fry restaurants coming to both the Hammermill and Neuhoff, the Modera development will provide similar retail and dining options to further the energy along Adams Street.

Note: Images shown are meant to be inspirational in nature

- 1 SUGGESTED WAYFINDING OR SIGNAGE
- 2 LIVELY STORE FRONTS AND PATIOS
- 3 PATIO AND PLAZA ALONG ADAMS



# THE RETAIL PLAZA



# PLAZA PERSPECTIVES

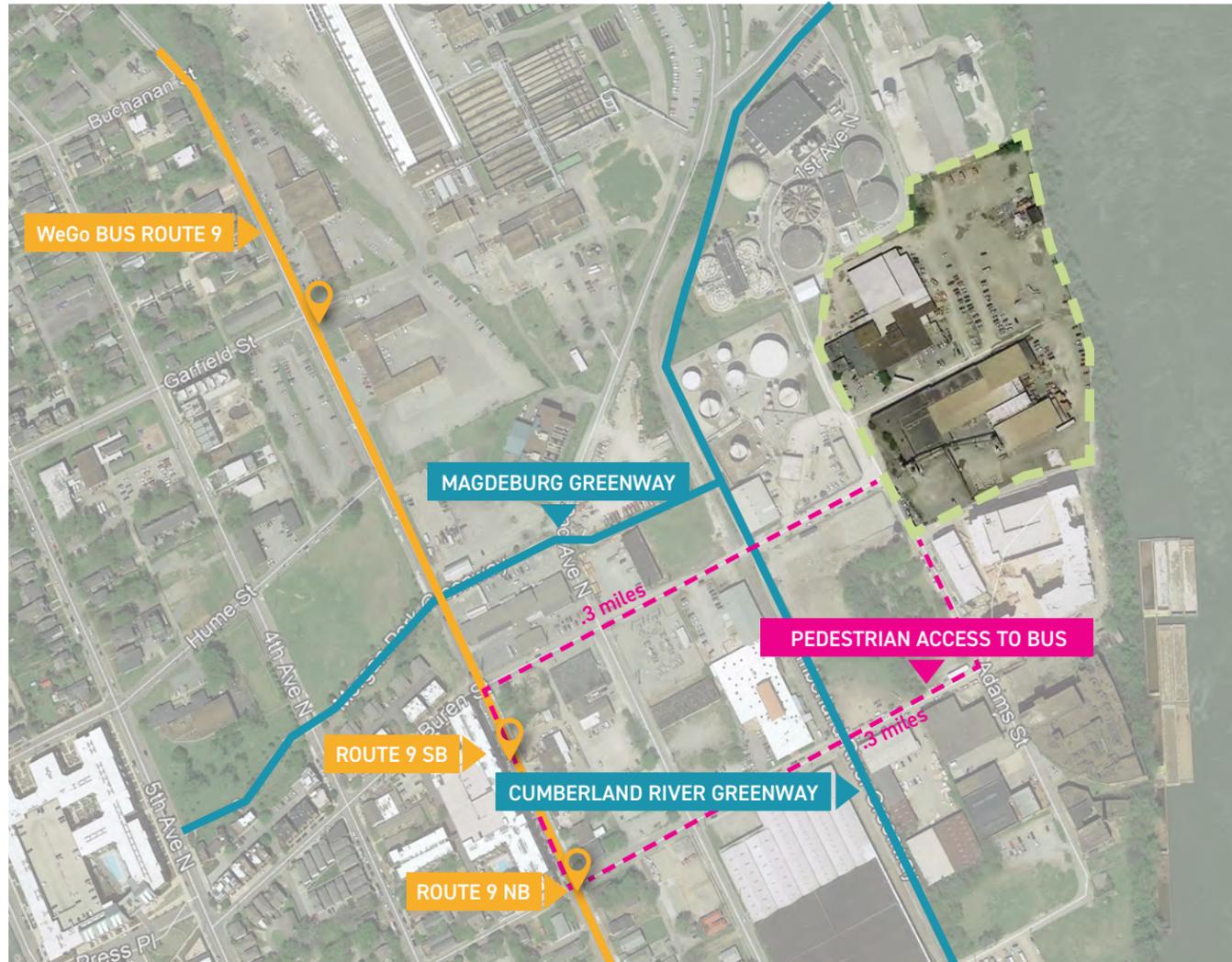
CENTER RETAIL PLAZA



COMMUNITY ENTRY PLAZA



# ACCESS & CONNECTIVITY



Connectivity: Bus, Trail, & Pedestrian Access

## ACCESS

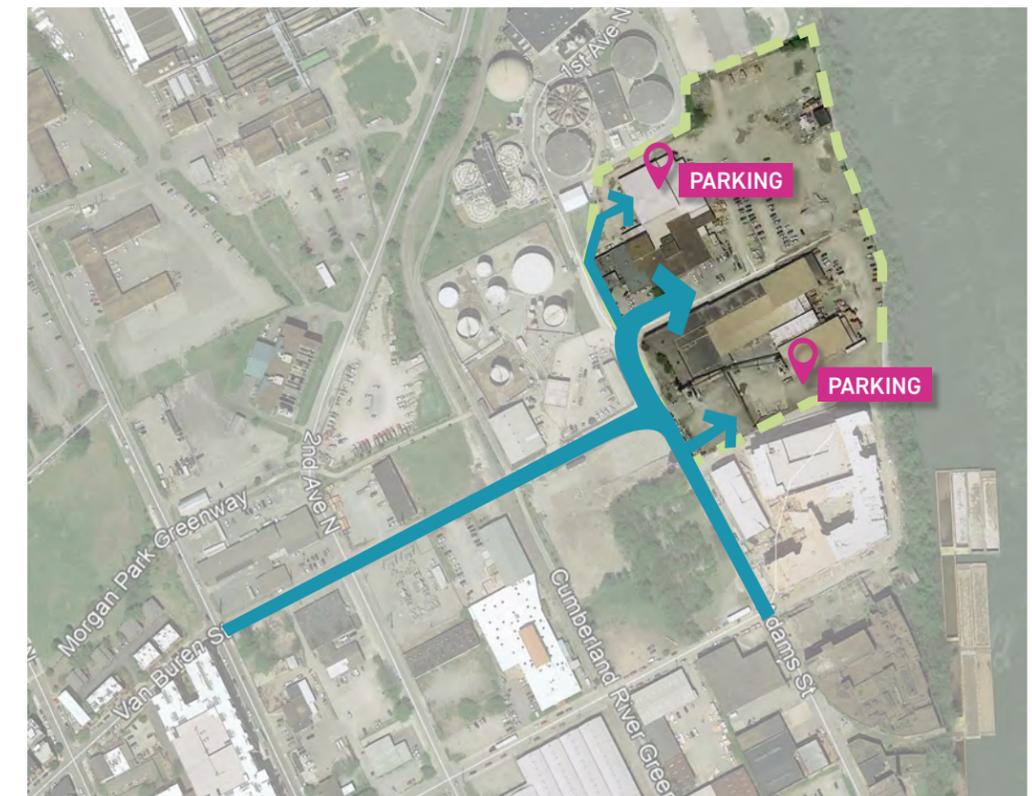
The site's primary access point would be by way of Van Buren and Adams Street. Neuhoff's intent to transform a portion of Adams Street into pedestrian supports the intent for Adams Street to be primarily pedestrian activity and will support the intended retail along this street. With Adams Street moving largely to a pedestrian street, previous 18-wheeler traffic will be significantly reduced, further enhancing the street activity and walkability of the area.

The main entrance would lay the foundation for a vibrant pedestrian experience and access to the river. It would also allow for circulation through Phase I and another access point for the parking structure. An additional entrance would be located at the end of the block to allow for a back entrance into parking and service areas as well as direct access to the surface parking for Phase I. Parking and service access for Phase II would be accessed through a south entrance which aligns with the entrance to O'Ku's parking across the street. Additionally, Phase II will offer a minimum of 3 spaces for every 1,000 square feet of retail in order to provide ample parking for the neighborhood and area for residents visiting the development.

## CONNECTIVITY

Located adjacent to the Cumberland River and on the northern edge of the Germantown neighborhood. The Modera Germantown site is in close proximity to Downtown, First Tennessee Park, and a mile away from Broad Street and Bridgestone Arena. Despite close proximity to Nashville's major attractions, Germantown is separated from the activity due to the Jefferson Street bridge and several blocks of disinvestment along the river. Germantown can be accessed from Downtown via 3rd and 4th Avenue by vehicular access and by the Route 9 WeGo Bus. Both the Magdeburg Greenway and the Cumberland River Greenway connect the Modera Germantown site to other areas of the city. There are two stops within .3 miles of the Modera Germantown site and can be accessed via Van Buren and Taylor.

The intent is to enhance the riverfront of the site and continue to bring investment along the Cumberland River. The intent of the Modera Germantown project is to create active and open space along the river and to create more opportunities for bike and pedestrian connections. In addition, Adams Street is increasingly becoming more of a pedestrian and retail corridor that will only further connect Germantown and the established retail along 2nd Avenue.



Accessibility: Entrances & Parking

# REGULATIONS



## DEVELOPMENT STANDARDS

Council District 19: Freddie O'Connell

Owners of Record:

Baugh & Pardue Properties, L.L.C.  
50 Van Buren Street  
Nashville, Tennessee 37208

SP Name: Modera Germantown

SP Number:

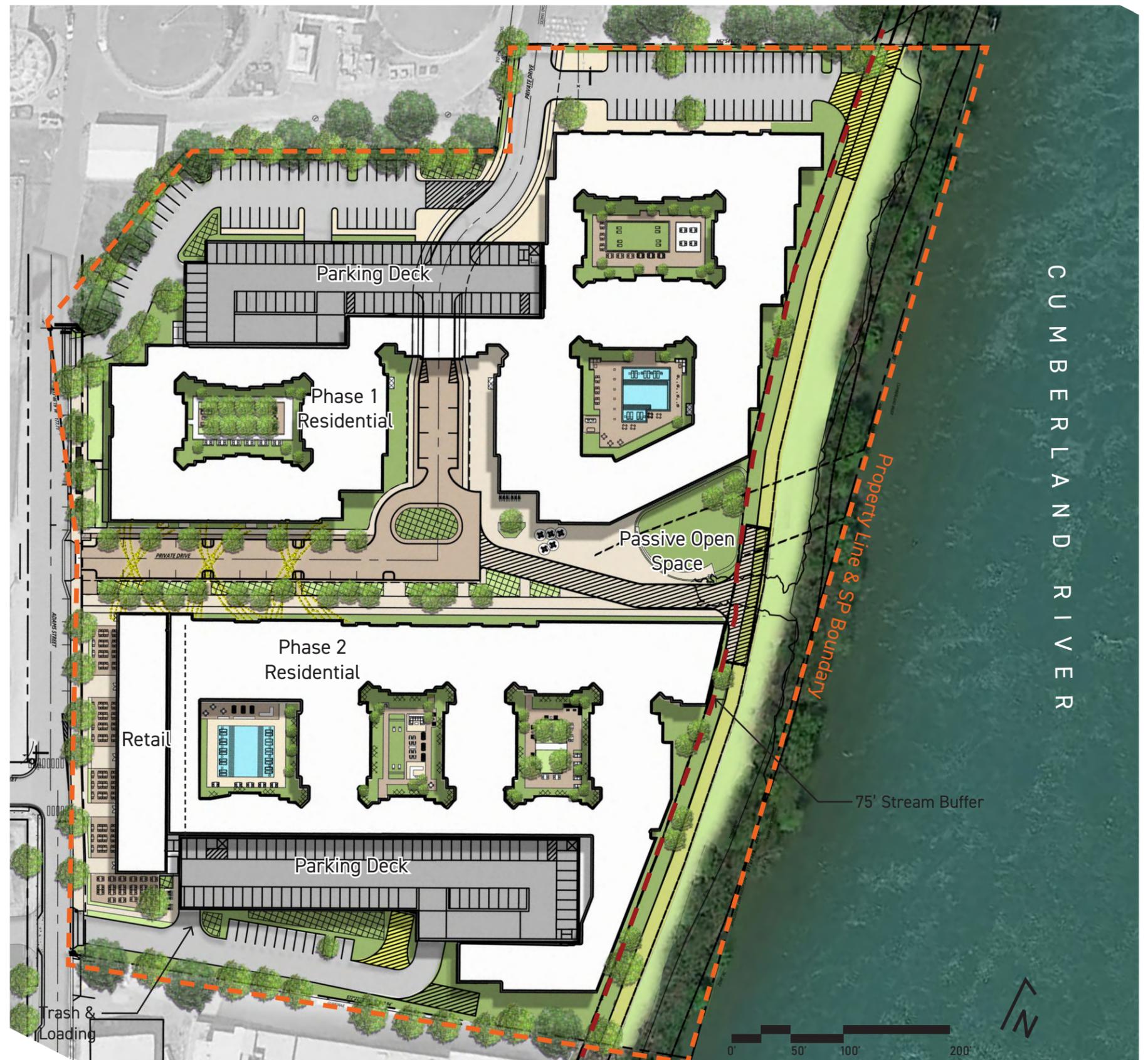
Plan Preparation Date: December 31, 2019

Design Professional:

Kyle Reis, Principal  
Cooper Carry  
191 Peachtree Street, Suite 2400  
Atlanta, Georgia 30303  
404.237.2000

Development Standards			
	Recommended Standards (Full Build Out)	Phase I	Phase II
Total Acreage	12.03 acres	7	5
Approx. Acreages by Use:			
Residential	3.47 acres	2 acres	1.47 acres
Retail	.36 acres		.36 acres
Passive Open Space	.67 acres	.45 acres	.22 acres
FAR	3 Max.		
Building/ Structure Type	Type III; Podium Type I		
Max. Building Height	6 Stories; Max. height 85 feet		
Impervious Surface Ratio	Max. .9	Max. .9	Max. .9
Building Coverage	40%	37%	44%
Front Setbacks		15 ft. min.	15 ft. min.
Side Setbacks		10 ft. min.	0 ft. min. (North) 10 ft. min. (South)
Rear Setback	75' Stream Buffer		
Landscaping	4' min. Landscape Strip		
Parking	Min. 1.25 per Residential Unit Min. 3 per 1,000 sf of Retail		
Permitted Uses:	All uses permitted per MUG-A, Community Garden, Mobile Vendor, Artisan Distillery, Micro Brewery, Tasting Room		
Prohibited Uses:	Short- Term Rental Property (STRP), Owner- Occupied and Short Term Rental Property (STRP) not Owner Occupied uses shall be prohibited		
Residential Uses	800 dwelling units Max.	400 dwelling units max.	400 dwelling units max.
Non-Residential uses	16,000 GSF Max.		16,000 GSF Max.
Default Zoning District	MUG-A		

Note: The Final Site Plan/ Building Permit Site Plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.



## STANDARD NOTES

1. The purpose of this SP is to receive preliminary approval to permit the development of 12.03 acres of mixed-use development as shown.
2. For any development standard, regulations and requirements not specifically shown on the SP plan and/or included as a condition of Council approval, the property shall be subject to the standards, regulations, and requirements of MUG-A base zoning as of the date of the application request or application.
3. Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its design based upon final architectural, engineering, or site design and actual site conditions. All modifications shall be consistent with the principles and further objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by the Metro Council that increase the permitted density and floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance.
4. The development is currently planned to be constructed in multiple phases and planning and design for both the Phase I and Phase II Final SP will occur after approval of the preliminary SP by Metro Planning Commission. The Final SP for Phase I and Phase II will be submitted separately.
5. Short term rental property (STRP), Owner- Occupied and Short Term Rental Property (STRP) not Owner- Occupied uses shall not be permitted.
6. Building frontage along the eastern side of the property will contain active uses.

## FEMA NOTES

7. Phase I and Phase II will separately meet the requirements of the development standards.
8. Portions of the property lie in an area of minimal flood hazard Zone X according to Federal Emergency Management Agency Flood Insurance Rate Map Panel Number 47307C0241H, dated April 5, 2017.

## ARCHITECTURAL STANDARDS

9. New buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for thirty (30) linear feet of street frontage:
  - a. A change in building material or building openings
  - b. A horizontal undulation in the building facade of two (2) feet or greater
  - c. A porch, stoop, window or balcony; porches shall be a min. six (6) feet in depth
  - d. A mural or other form of art installation
10. Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing.
11. Windows shall be vertically oriented to a ratio of 1.5:1 or greater except for dormers
12. A raised foundation of 18" - 36" is required for all residential structures
13. Refuse collection, recycling and mechanical equipment shall be screened from public view by the combination of fences, wall, or landscaping

14. Vinyl siding, EIFS, faux brick, untreated wood shall be prohibited for all uses.
15. HVAC units shall be located at the rear half of the side of the unit, behind the unit, or on the roof of each building.
16. Bicycle parking will be provided per the Metro Zoning code. Bicycle parking locations to be identified in the Phase I and Phase II Final SP.
17. Exterior facing building facades will have a maximum of 35% of cementitious siding on average. No minimum amount of cementitious siding will exist for the project. Adams Street facing facades will limit cementitious siding to a maximum amount of 15%.
18. Sidewalks along the western edge of the property that front Adams Street will consist of brick and will meet minimum Metro standard requirements. Designated patios for retail tenants will not be required to consist of brick.

## METRO PUBLIC WORKS NOTES

19. The Phase I and Phase II final site plans and building permits shall depict the required public sidewalks, any required grass strip or frontage zone, and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Where feasible, vertical obstructions are only permitted within the required grass strip or frontage zone.
20. The development totals represented in the Traffic Impact Study represent the current target development scenarios for the maximum residential units and non-residential SF. The final allocation, density and intensity of the proposed uses may adjust based on market conditions prior to FINAL SP application for Phase I and Phase II. If the development program allocation changes, a revised traffic study may be required for Metro review and approval prior to the filling of the Phase I and Phase II FINAL SP applications. All Metro recommended improvements must be provided within the Phase I and Phase II FINAL SP applications per any revised and approved traffic study.
21. All parking regulations to meet UZO parking requirements and standards. A shared parking plan may be approved by the metropolitan traffic engineer based upon a parking and/ or loading study that satisfactorily demonstrates of the approved Transportation Demand Management Plan (TDM).
22. Roadway improvements that are direct result of this specific project as determined by the approved Traffic Improvement Study and the Department of Public Works shall be constructed.
23. Any new improvements within existing public right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
24. Developer will ensure bike lanes are continuous through intersections.
25. The developer's final construction drawings shall comply with the design regulations established by the Department of Public Works. In effect at the time of the approval of the preliminary development plan or final development plan or building permit, as applicable. Final designs for Phase I and Phase II may vary based on field conditions.
26. The design of the public infrastructure is to be coordinated with the Final SP for Phase I and Phase II. The roads, pedestrian infrastructure, bicycle routes, etc are to be designed and constructed per MPW standards

and specifications.

27. Back of house, loading zones, vehicular and pedestrian access points, and code required bicycle parking will be indicated in the Final SP for both Phase I and Phase II.
28. All construction within the right of way shall comply with ADA and Metro Public Works Standards and Specifications.
29. Where feasible, vertical obstructions shall be relocated out of the proposed sidewalks where applicable.

## LANDSCAPE STANDARDS

30. The developer of this project shall comply with the requirements of the SP and adopted tree ordinance.
31. Street trees shall be provided, irrigated, and maintained by Owner along all street frontages at a minimum spacing average of 40 linear feet. All street trees placed within ROW shall count toward tree density unit credit outlined in Metro Zoning Code 17.24.
32. No landscape buffer requirement of 17.24 shall apply

## FIRE MARSHAL NOTES

33. New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H. To see Table H go to: (<http://www.nashfire.org/prev/tableH51.htm>)
34. No part of any building shall be more than 500 feet from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec. 1568.020 B
297. Phase I and Phase II will have fire department access roads that shall be 18 feet minimum width and shall have an unobstructed vertical clearance of 13.5 feet.
35. If more than three stories above grade, Class I standpipe system shall be installed.
36. If more than one story below grade, Class I standpipe system shall be installed.
37. A fire hydrant shall be provided within 100 feet of the fire department connection.
38. Fire hydrants shall be in-service before any combustible material is brought on site.
39. Fire apparatus access roads shall be provided and maintained in accordance with the adopted fire code and standards.
40. Except as approved by the fire code official; fire apparatus access roads shall extend to within 150 feet of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.
41. Fire apparatus access roads shall have a minimum unobstructed width of 20 feet. Where a fire hydrant is located on a fire apparatus access road the minimum width in the vicinity of the hydrant shall be 26 feet.
42. Buildings over 30 feet in height shall meet fire department aerial apparatus access requirements.
43. Dead end fire apparatus access roads in excess of 150 feet shall be provided with an approved fire apparatus turnaround.
44. All points of the building shall be within 500 feet of a fire hydrant when measured via approved fire apparatus access route.
45. All buildings and/or developments are required to meet the fire-flow requirements listed in the adopted code prior to construction.

46. Fire department connections for standpipe/sprinkler system shall be within 100 feet of the fire hydrant via approved access route.
47. Developments of one- or two-family dwelling units where the number of dwelling units exceeds 30 shall be provided with two separate and approved fire apparatus access roads.
48. Buildings exceeding 30 feet in height or 62,000 square feet in area (124,000 fully sprinklered) shall be provided with two separate and approved fire apparatus access roads.
49. Where two separate and approved fire apparatus access roads are required, they shall be placed a distance apart equal to not less than one-half of the length of the maximum overall diagonal dimension of the property or area to be served, measured in a straight line between accesses. The AHI may approve variations to this requirement in the event remoteness cannot be accomplished.
50. The maximum grade for fire apparatus access roads shall not exceed 10% without approval from the fire code official.
51. Gates across fire apparatus access roads shall comply with adopted code and standards.
52. Approval of a preliminary or final site plan is not an approval for building construction. Full and complete review of building plans is required prior to approval for construction and may require changes to the site.

#### NES NOTES

53. Where feasible, this development will be served with underground power and pad-mounted transformers.
54. New facilities will not be allowed to sit in or to pass through retention areas, including rain gardens, bioretention areas, bioswales, and the like. This includes primary duct between pad-mounted transformer equipment as well as service duct to a meter.

#### STORMWATER NOTES

55. Any excavation, fill, or disturbance of the existing ground elevation must be done in accordance with Stormwater Management Ordinance No. 78-840 and approved by the Metropolitan Department of Water Services.
56. Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.
57. Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual. (Minimum driveway culvert in Metro ROW is 15" CMP).
58. The project intent is for the site to be redeveloped per the requirements of the current Stormwater Management Manual. Detention will not be required for stormwater discharges that outfall to the Cumberland River. Detention will be provided or post developed runoff will be less than predeveloped runoff for outfalls not directed to the Cumberland River.

#### FEDERAL COMPLIANCE

59. All development within the boundaries of this plan will meet the requirements of the Americans with Disabilities Act and the Fair Housing Act

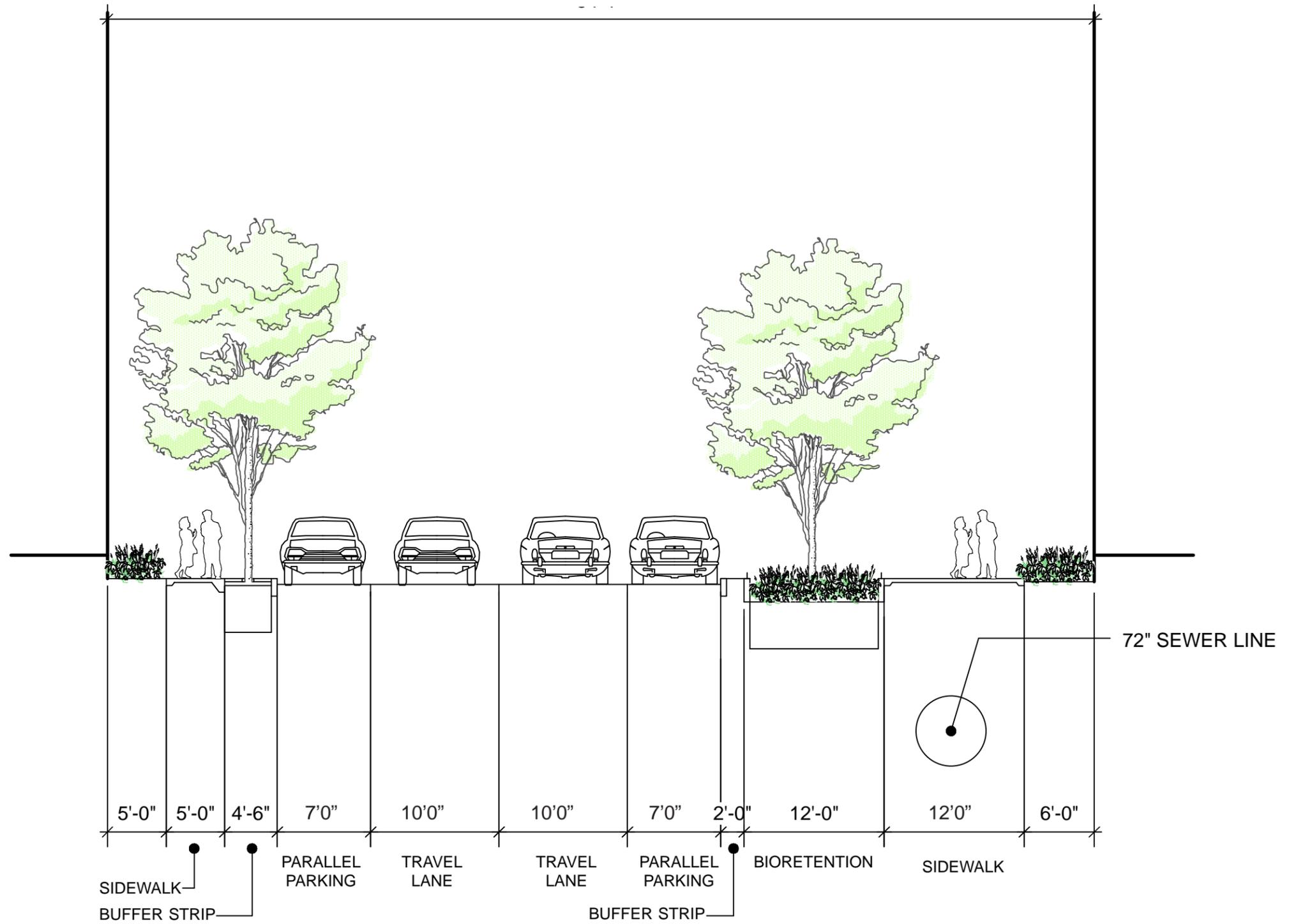
#### PHASING & PERMITTING

60. Permits will be submitted separately for Phase I and Phase II.
61. The Final SP for Phase I and Phase II will be submitted separately.

#### SITE PLAN NOTES

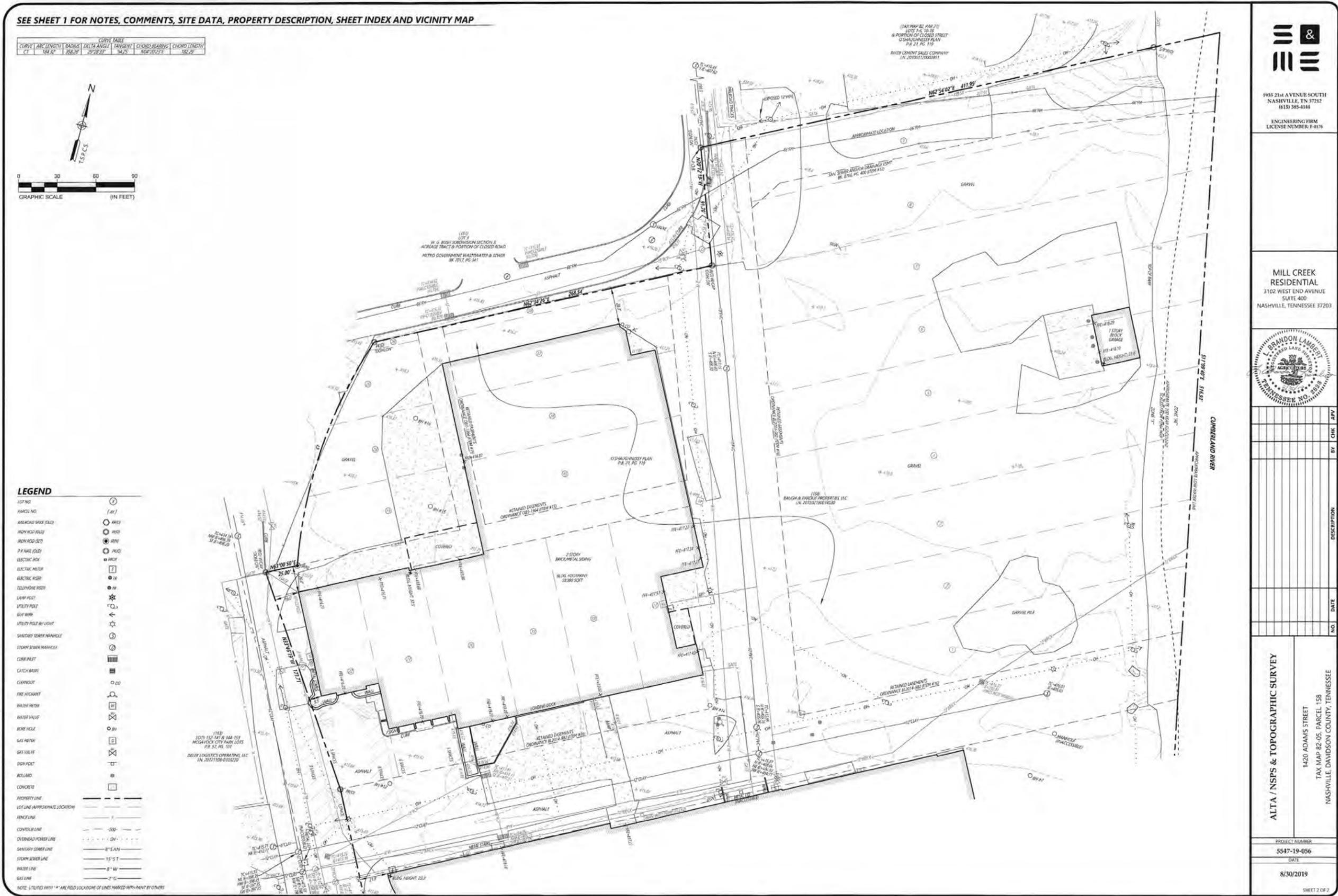
62. As shown on the site plan, a 12' trail along the waterfront will be constructed for each Phase. The path will be designed and constructed in accordance with Metro Greenway Standards.
63. Phase II will accommodate the area required to build a connection point between the southern end of the waterfront path and the future Greenway for the adjoining property to the south.
64. A pathway will connect to the waterfront along the southern side of Phase II.
65. The central drive between Phase I and Phase II will remain open with no gating of sidewalks or the roadway in order to preserve connection through the site and to preserve access to the Greenway Trail. Additionally, the section of street and sidewalk ranging from the central drive roundabout north through the Phase I garage to the northern boundary of the site will also remain open. Please reference notes on the Site Plan.

# CENTRAL DRIVE SECTION



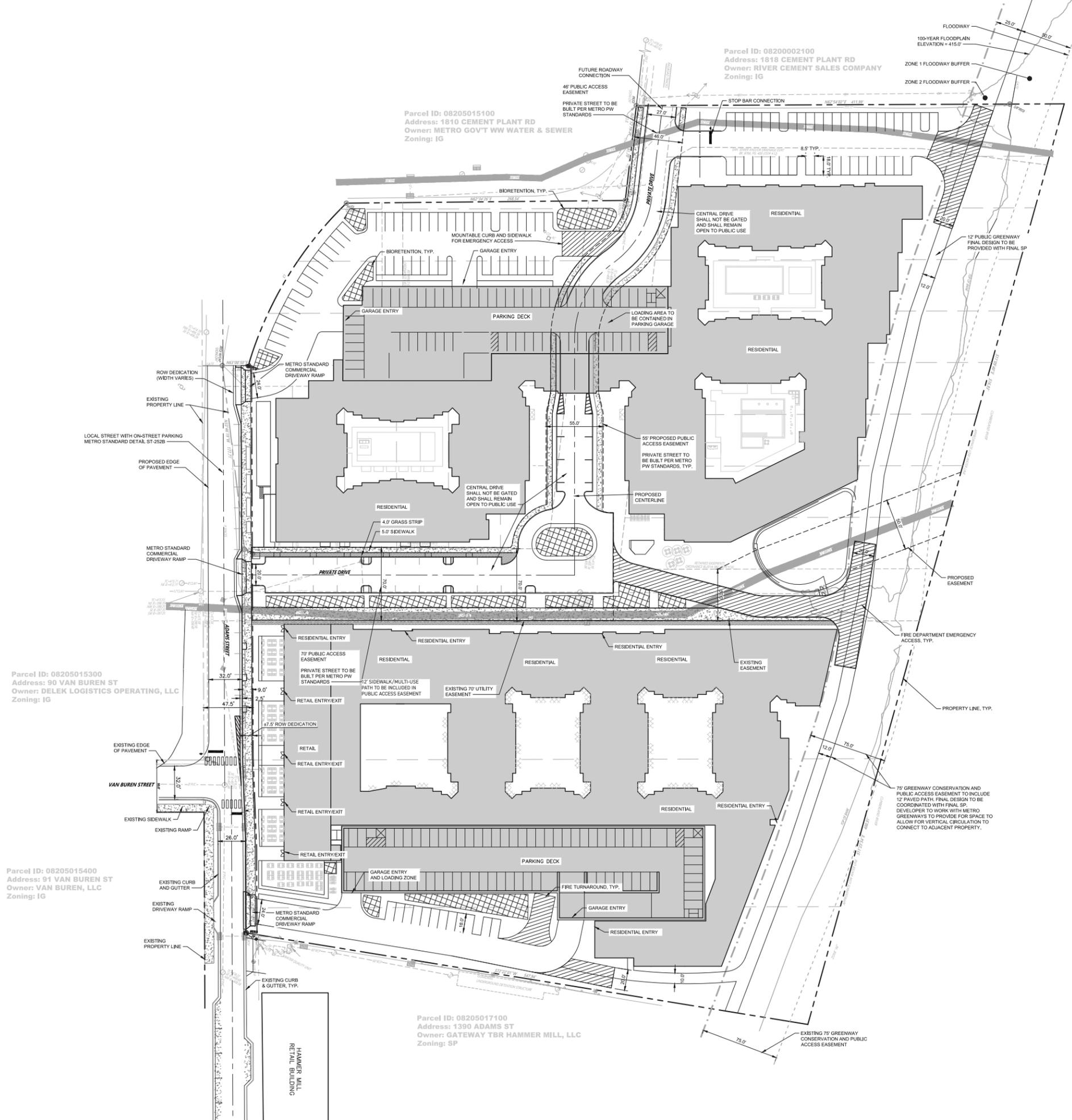
Note: Dimensions are minimums and vary throughout the central drive

# EXISTING SURVEY PHASE I





# SITE PLAN



Parcel ID: 08205015300  
 Address: 90 VAN BUREN ST  
 Owner: DELEK LOGISTICS OPERATING, LLC  
 Zoning: IG

Parcel ID: 08205015400  
 Address: 91 VAN BUREN ST  
 Owner: VAN BUREN, LLC  
 Zoning: IG

Parcel ID: 08205017100  
 Address: 1390 ADAMS ST  
 Owner: GATEWAY TBR HAMMER MILL, LLC  
 Zoning: SP

Parcel ID: 08200002100  
 Address: 1818 CEMENT PLANT RD  
 Owner: RIVER CEMENT SALES COMPANY  
 Zoning: IG

Parcel ID: 08205015100  
 Address: 1810 CEMENT PLANT RD  
 Owner: METRO GOVT WW WATER & SEWER  
 Zoning: IG

DEVELOPMENT STANDARDS	
MAX BUILDING HEIGHT	SEE ARCHITECTURAL PLANS
RESIDENTIAL UNITS	SEE ARCHITECTURAL PLANS
PARKING	PARKING RATIOS SHALL BE PROVIDED AT OR ABOVE THE METRO ZONING CODE PARKING STANDARDS
BUILD-TO-ZONE	SEE ARCHITECTURAL PLANS
FALL BACK ZONING	SEE ARCHITECTURAL PLANS

PUBLIC WORKS STANDARDS	
<ol style="list-style-type: none"> <li>ANY REQUIRED RIGHT-OF-WAY WITHIN THE PROJECT SITE THAT IS IDENTIFIED AS NECESSARY TO MEET THE ADOPTED MAJOR AND COLLECTOR STREET PLANS SHALL BE DEDICATED</li> <li>LOADING AND RIDESHARE AREAS SHALL BE LIMITED TO SPACES INTERIOR TO THE PROJECT SITE AND OFF PUBLIC RIGHT-OF-WAY. ALL CONSTRUCTION IN THE RIGHT-OF-WAY SHALL COMPLY WITH MPW STANDARDS AND SPECIFICATIONS.</li> <li>ALL RIGHT-OF-WAY DEDICATION WILL OCCUR PRIOR TO ISSUANCE OF BUILDING PERMITS.</li> <li>THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS. ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.</li> </ol>	

SP MODIFICATION NOTE	
<ol style="list-style-type: none"> <li>MINOR MODIFICATIONS TO THE PRELIMINARY SP PLAN MAY BE APPROVED BY THE PLANNING COMMISSION OR ITS DESIGNEE BASED ON FINAL ARCHITECTURAL, ENGINEERING OR SITE DESIGN AND ACTUAL SITE CONDITIONS. ALL MODIFICATIONS SHALL BE CONSISTENT WITH THE PRINCIPLES AND FURTHER THE OBJECTIVES OF THE APPROVED PLAN. MODIFICATIONS SHALL NOT BE PERMITTED, EXCEPT THROUGH AN ORDINANCE APPROVED BY METRO COUNCIL, THAT INCREASE THE PERMITTED DENSITY OR FLOOR AREA, ADD USES NOT OTHERWISE PERMITTED, ELIMINATE SPECIFIC CONDITIONS OR REQUIREMENTS CONTAINED IN THE PLAN AS ADOPTED THROUGH THIS ENACTING ORDINANCE.</li> <li>DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION.</li> </ol>	

NES NOTES	
<ol style="list-style-type: none"> <li>WHERE FEASIBLE, THIS DEVELOPMENT WILL BE SERVICED WITH UNDERGROUND POWER AND PAD MOUNTED TRANSFORMERS.</li> <li>NES FACILITIES WILL NOT BE ALLOWED TO SIT IN OR PASS THROUGH RETENTION AREAS INCLUDING RAIN GARDENS, BIORETENTION, BIOSWALES AND THE LIKE. THIS INCLUDES PRIMARY DUCT BETWEEN PAD-MOUNTED EQUIPMENT AS WELL AS SERVICE DUCT TO METER.</li> </ol>	

FIRE ACCESS NOTES	
<ol style="list-style-type: none"> <li>FIRE DEPARTMENT ACCESS ROADS SHALL COMPLY WITH IFC CHAPTER 5 AND APPENDIX D.</li> <li>FIRE DEPARTMENT ACCESS ROADS SHALL HAVE AN UNOBSTRUCTED CLEAR WIDTH OF 20' WHERE A FIRE HYDRANT IS LOCATED ON A FIRE APPARATUS ACCESS ROAD THE MINIMUM WIDTH SHALL BE 26' EXCLUSIVE OF SHOULDERS.</li> <li>'NO PARKING - FIRE LANE' SIGNAGE SHALL BE PROVIDED IN ACCORDANCE WITH IFC D103.6.</li> </ol>	

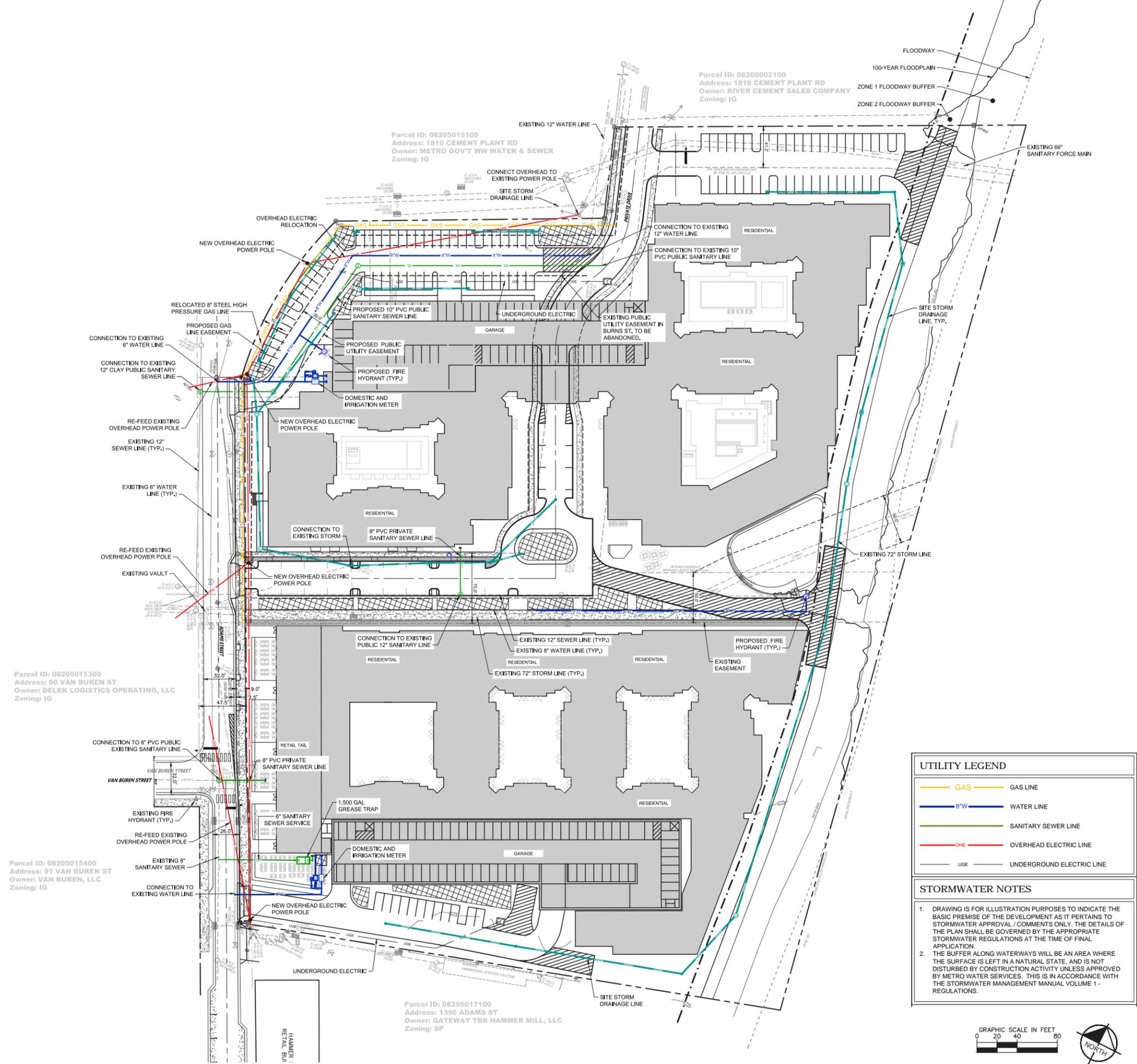
STORMWATER NOTES	
<ol style="list-style-type: none"> <li>THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY UNLESS APPROVED BY METRO WATER SERVICES. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.</li> </ol>	

SP NOTES	
<ol style="list-style-type: none"> <li>THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.</li> </ol>	



# UTILITY OVERALL PLAN



# UTILITY RELOCATION PLAN

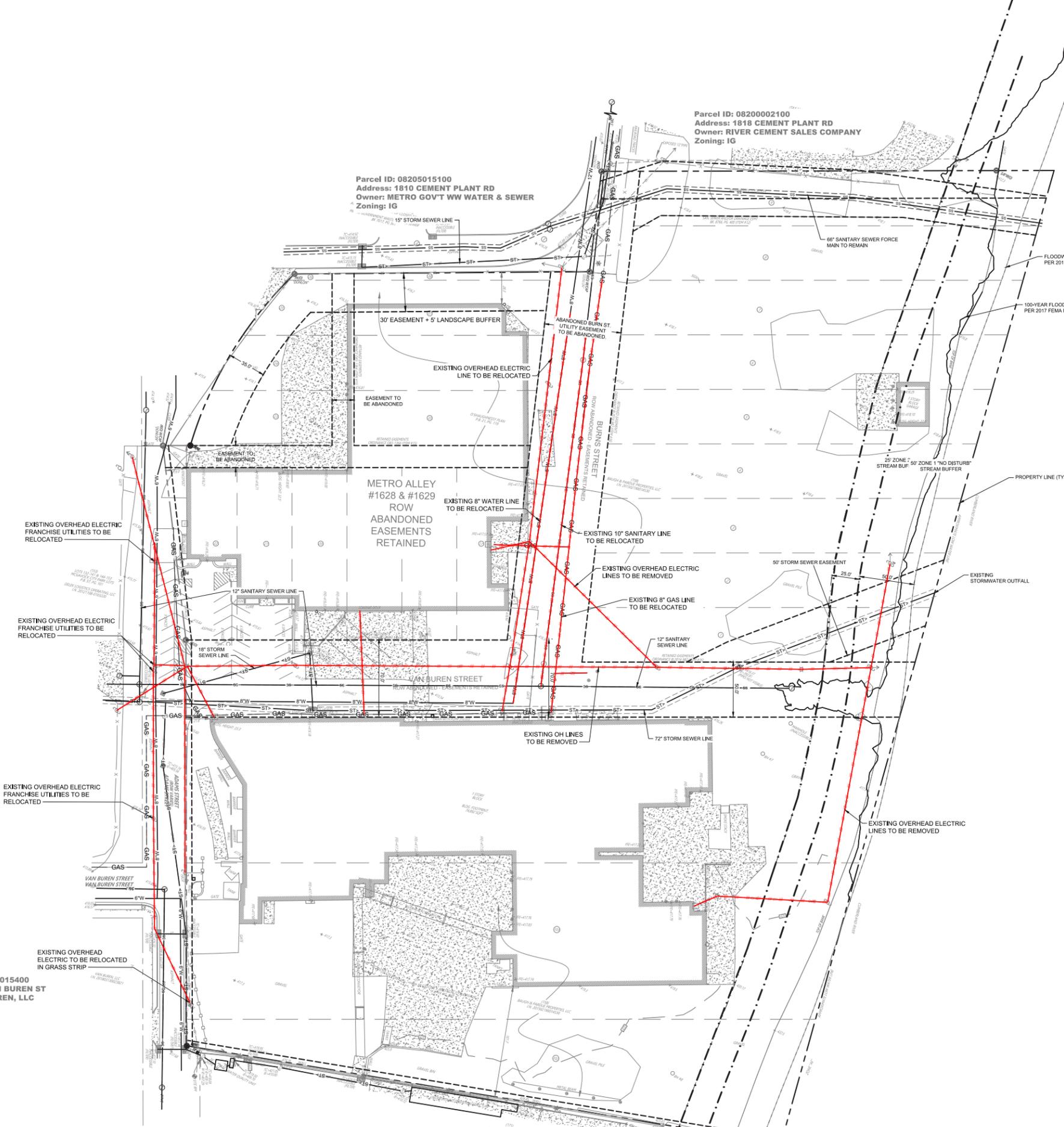
Parcel ID: 08205015300  
 Address: 90 VAN BUREN ST  
 Owner: DELEK LOGISTICS OPERATING, LLC  
 Zoning: IG

Parcel ID: 08205015400  
 Address: 91 VAN BUREN ST  
 Owner: VAN BUREN, LLC  
 Zoning: IG

Parcel ID: 08205015100  
 Address: 1810 CEMENT PLANT RD  
 Owner: METRO GOV'T WW WATER & SEWER  
 Zoning: IG

Parcel ID: 08200002100  
 Address: 1818 CEMENT PLANT RD  
 Owner: RIVER CEMENT SALES COMPANY  
 Zoning: IG

Parcel ID: 08205017100  
 Address: 1390 ADAMS ST  
 Owner: GATEWAY TBR HAMMER MILL, LLC  
 Zoning: SP

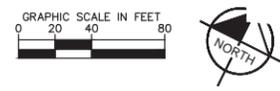


**STORMWATER NOTE**

1. DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION.
2. THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY UNLESS APPROVED BY METRO WATER SERVICES. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.

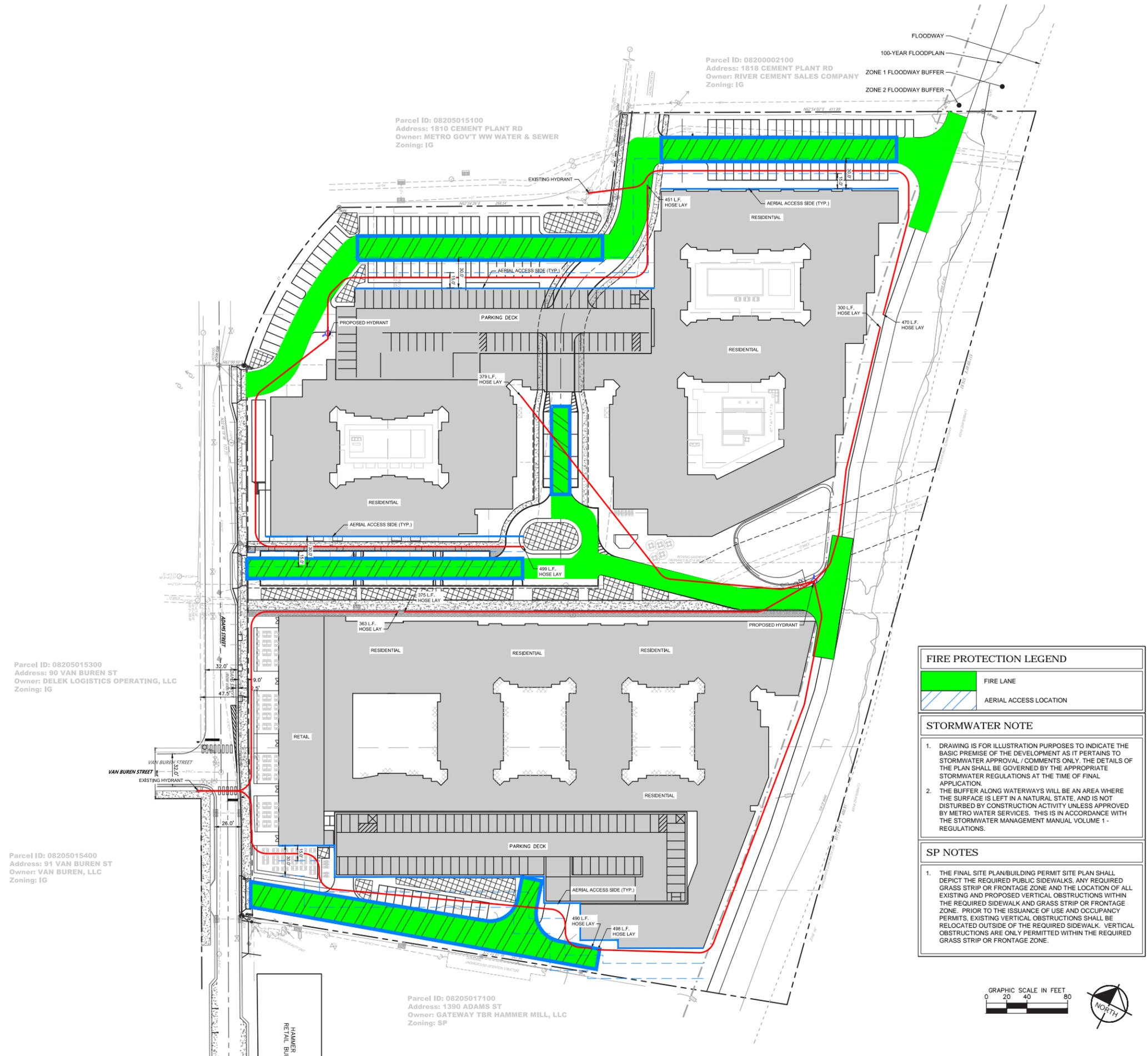
**SP NOTES**

1. THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.



# FIRE PROTECTION PLAN

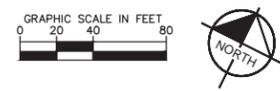
MODERA GERMANTOWN  
#2020SP-009-001



FIRE PROTECTION LEGEND	
	FIRE LANE
	AERIAL ACCESS LOCATION

- STORMWATER NOTE**
- DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT AS IT PERTAINS TO STORMWATER APPROVAL / COMMENTS ONLY. THE DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE STORMWATER REGULATIONS AT THE TIME OF FINAL APPLICATION.
  - THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY UNLESS APPROVED BY METRO WATER SERVICES. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 - REGULATIONS.

- SP NOTES**
- THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.





# TRAFFIC SIGNAL





 **MILL CREEK**  
RESIDENTIAL